



Central Coast Council

Ordinary Meeting

Attachments

Monday, 14 May, 2018

Central Coast Council
Attachments to the
Ordinary Meeting
to be held in the Council Chamber,
Level 1, 49 Mann Street, Gosford
On Monday, 14 May 2018 ,
Commencing At 6.30pm

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GENERAL REPORTS

3.1 Exhibition of the draft Delivery Program and Operational Plan 2018-19 and draft Resourcing Strategy

Attachment 1: Draft Delivery Program and Operational Plan 2018-19 (Distributed under separate cover)	
Attachment 2: Draft Resourcing Strategy (Distributed under separate cover)	

Item No: 3.1
Title: DA 51671/2017 - Child Care Centre 612 The Entrance Road, Wamberal
Department: Environment and Planning



18 December 2017 Ordinary Council Meeting

Author: Jane Doyle, Development Assistant
 Authoriser: Scott Cox, Director Environment and Planning

Summary

An application has been received for a Child Care Centre located at 612 The Entrance Road, Wamberal. The application has been examined having regard to the matters for consideration detailed in section 79C of the *Environmental Planning and Assessment Act 1979* and other statutory requirements with the issues requiring attention and consideration being addressed in the report.

Applicant	R M Bateman
Owner	Mario Reznik
Application No	DA 51671/2017
Description of Land	Lot: 70 DP: 1210387 612 The Entrance Road Wamberal
Proposed Development	Child Care Centre
Site Area	1,291m ²
Zoning	R2 Low Density Residential
Existing Use	Vacant Land
Employment Generation	Yes
Estimated Value	\$1,100,000

Recommendation

- 1 *That Council grant consent subject to the conditions detailed in the schedule attached to the report and having regard to the matters for consideration detailed in Section 79C of the Environmental Planning and Assessment Act 1979 and other relevant issues.*
- 2 *That Council advise those who made written submissions of its decision.*

Moved: **Councillor Best**
Seconded: **Councillor Burke**

Resolved

833/17 ***That Council defer this item to the Ordinary Meeting on 26 March 2018 to***

further review this development application.

834/17 ***That the officers report back on the previous traffic calming measures that were considered for Old Gosford Road.***

835/17 ***That Council reopen the application for public submissions for a period of 14 days.***

836/17 ***That Council specifically address the DCP issues relating to the separate entry and exit driveways and if they are a plan or a policy.***

For:

Unanimous

Precis:

Proposed Development	Child Care Centre
Permissibility and Zoning	The subject site is zoned R2 Low Density Residential under <i>Gosford Local Environmental Plan 2014</i> (GLEP) The proposed development is defined as a Child Care Centre which is permissible with consent in the zone.
Relevant Legislation	<ul style="list-style-type: none"> • Environmental Planning & Assessment Act 1979 - Section 79C • Local Government Act 1993 - Section 89 • State Environmental Planning Policy No 55 - Remediation of Land (SEPP 55) • State Environmental Planning Policy No 71 - Coastal Protection (SEPP 71) • State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017 (SEPP Education and Child Care) • Gosford Local Environmental Plan 2014 (GLEP) • Gosford Development Control Plan 2013 (GDGP)
Current Use	The site is a vacant parcel of land
Integrated Development	No
Submissions	92
Councillor Representation	Nil

Variations to Policies

Policy	Clause	Variance
GDCP 2013 Chapter 3.9 Child Care	CI 3.9.4.2 – Maximum site coverage	4.75%
	CI 3.9.4.2 – Maximum land gradient	Variable departure between 2% - 80%. It is noted that 86% of the site is compliant
	CI 3.9.5 – Traffic & Parking	Separate entry/exit not provided, a wider single entry/exit proposed
	CI 3.9.10 – Building Appearance – maximum slope of land	Variable departure between 2% - 80%. It is noted that 86% of the site is compliant and the building is designed in response to the slope of the site.

The Site

The site is located on the south eastern corner of The Entrance Road at the intersection with Old Gosford Road as shown edged in blue in Figure 1 below. The site is an irregular shaped allotment and is bound by RMS Road reserve to the north, west and south (known as Lot 71 DP1210387 – shown edged in red).

The subject land benefits from an easement which creates a right of access over the adjoining RMS land to the south being Lot 71. The right of access has been drafted so that it extinguishes upon Lot 71 becoming dedicated as public road, which would have the effect of the subject land (Lot 70) having a frontage to Old Gosford Road. At the date of writing this report, Lot 71 has not been dedicated as public road and so access remains reliant on the terms of the easement.

The site is currently a vacant parcel of land and there is no significant vegetation on site. The site is not identified as being "bushfire prone land" on Council's bushfire prone land maps.

Council records indicate that prior to being used by RMS during the Central Coast Highway construction, the site was used for rural residential purposes.

Subject Site



Figure 1 – Locality Plan

Surrounding Development

The site is bound by street frontage at all boundaries with the exception to the eastern boundary. A single storey medium density residential development exists on the adjoining property to the east at No. 93 Old Gosford Road, Wamberal.

Single residential dwellings exist to the south of the site and large lot rural residential developments exist on the opposite side of The Entrance Road (Central Coast Highway) to the north.

The Proposed Development

The development application proposes the construction of a Child Care Centre for 80 children and is to employ more than 10 teaching staff. The development proposes a 27 space basement carpark that provides parking for up to 13 staff and 14 customers including an accessible car space. The basement level will also include waste storage and the pedestrian entry to the child care centre.

The child care centre is proposed to operate during the following trading hours;

- Monday to Friday: 7.00am - 6.00pm

Special events will be conducted off site in alternate venues.

Consultation

Submissions from the public

The application was advertised from 24/03/2017 to 21/04/2017 in accordance with Chapter 7.3 Notification of Development Proposals of the Gosford Development Control Plan 2013 (GDGP). A total of 92 public submissions were received in relation to the application (89 of which were in the form of standardised proforma letters). The matters raised in the submissions are summarised and addressed below.

- *Traffic, Safety, and Parking concerns including:*
 - Close proximity to the intersection of the Central Coast Highway and impact on the functioning of the intersection.
 - Additional vehicle movements will generate traffic conflicts and congestion including truck movements for waste collection.
 - Driveway design is inadequate
 - Lack of convenient and safe parking for residents, staff and visitors in the street.
 - Traffic Management Plan submitted with this application is ambiguous Event parking has not been adequately addressed.
 - No safe and convenient pedestrian crossing in this location and the information submitted fails to provide information regarding pedestrian crossing Old Gosford Road.
 - Noise will impact on the amenity of the surrounding properties.
 - The location of the bus stops is a potential conflict.
 - Mechanical roller door at the entry to the car park is prone to failure and will cause additional traffic congestion.
 - Health risk to young children from ultra-fine particulates (UFPs) from proximity to main road

Comment:

The subject application has been assessed by Council's Traffic Engineer and Roads & Maritime Services (RMS) who raise no concerns in terms of traffic generation and safety. Both the RMS and Council's Traffic Engineer are in support of the proposed development (as amended) subject to conditions.

The applicants' Traffic Study identifies that the traffic movements associated with the child care centre are within the limits of the road network.

The proposed development plans have been amended to lower the driveway grade and basement level. This will provide for improved accessibility and will be consistent with the requirements of the Chapter 7.1 Car Parking of the GDGP as it relates to grade. Further, conditions are proposed which require street network improvements such as a road blister and timed parking restrictions to improve site distances, calm traffic and support pedestrian safety.

Parking proposed in the application meets Council's requirements as contained in Chapter 7.1 Car Parking of the GDCP, and applicable Australian Standards for size, turning, manoeuvrability, and access.

As the proposal provides adequate on-site parking, the assessment of the proposal does not rely on the availability of on street parking. Direct access to the child care centre is available from within the basement carpark. Pedestrian access from the street will only be available via the driveway, however this is unlikely to be relied upon given the adequate onsite parking.

Traffic calming measures and pedestrian footpaths are available within the street network to support pedestrian access, however given the nature of the proposed use and the convenience of onsite parking, customers are unlikely to rely on pedestrian access. Within the basement a separate pedestrian area has been identified.

Event parking is not proposed as part of this application and the applicant has identified that off-site facilities will be used for special events and end of year concerts etc.

The roller shutter door proposed at the entrance to the child care centre is for security purposes and will not be in operation during the trading hours of the child care centre.

Council's Traffic Engineer and RMS are supportive of the proposal as it will not generate undue noise impacts on the surrounding properties. Noise associated with the entry/exit of vehicles, parking and manoeuvre will be contained within the basement floor level. The bus stop is located east of the site access point, within 50m of the eastern boundary. The site access provides dual entry/exit therefore all vehicles will leave the site in a forward direction. As the bus stop is located east of the site, and will not interfere with the access entry, adequate sight distances are provided to minimise potential conflict between bus movements and vehicles exiting the site.

The SEPP (Education and Child Care) provides controls which prohibit child centres in certain localities on the basis of potential health risks. The proposed location is not identified within the criteria which prohibit child centres and therefore the use is supported.

- *Inconsistent with the requirements of Chapter 3.9 of the GDCP 2013, particularly open space areas, setbacks and fence height.*

Comment:

The application proposes approximately 560m² of outdoor open space and play areas which is an average of 7m² per child, and therefore complies with the Child Care Planning Guideline (NSW Department Planning & Environment, 2017) requirements which is 7m² per child. There are no specific open space area requirements within Chapter 3.9 Child Care Centres of GDCP.

The minor variation of 4.75% to the maximum site coverage controls is addressed in following sections of this assessment.

The setbacks are considered appropriate and are addressed further in the detailed assessment against the applicable provisions of Chapter 3.9 Child Care Centres of GDCP.

The development proposes solid fences to surround the development to provide both security and acoustic treatment to limit noise levels from the site. The height of the fencing is predominantly 2.1m fronting the roads and 1.8m adjoining the residential development to the east. The proposal seeks to include a mix of masonry and perspex at the Old Gosford Road frontage, to provide variation while maintaining acoustic performance. The proposed fencing is considered appropriate and is addressed further in the detailed assessment against the applicable provisions in the various chapters of the GDCP.

- *Noise impact on the amenity of the adjoining neighbours.*

Comment:

The adjoining property to the east at No. 93 Old Gosford Road contains an existing single storey medium density development. There are two dwellings on the western portion of No. 93 which adjoin the development site. The dwellings are set back between 2.5m and 4.6m from the common boundary. The existing fence dividing the properties is approximately 1.4m high.

Unit 1, fronting Old Gosford Road, has informal open space areas within the setback to the common boundary. The master bedroom for this unit is located closest to the boundary, however does not include any window openings at this elevation. Primary living areas and open space areas for Unit 1 are orientated toward the south. Unit 10 is also located adjacent to the common boundary, with a frontage to The Entrance Road. The primary living and open space areas for Unit 10 are orientated to the north and the narrow parcel of land adjacent to the common boundary is used for ancillary open space. There are window openings in the western elevation of Unit 10, however these are narrow openings that are obscured by ancillary outbuildings on No. 93 and existing tree plantings (see figure 2).



Figure 2 –Units 1 and 10 as viewed from the subject site

The proposed basement is 2m from the boundary and the building is proposed to be a minimum of 4.09m from the boundary. Extensive landscaping is proposed within the setback

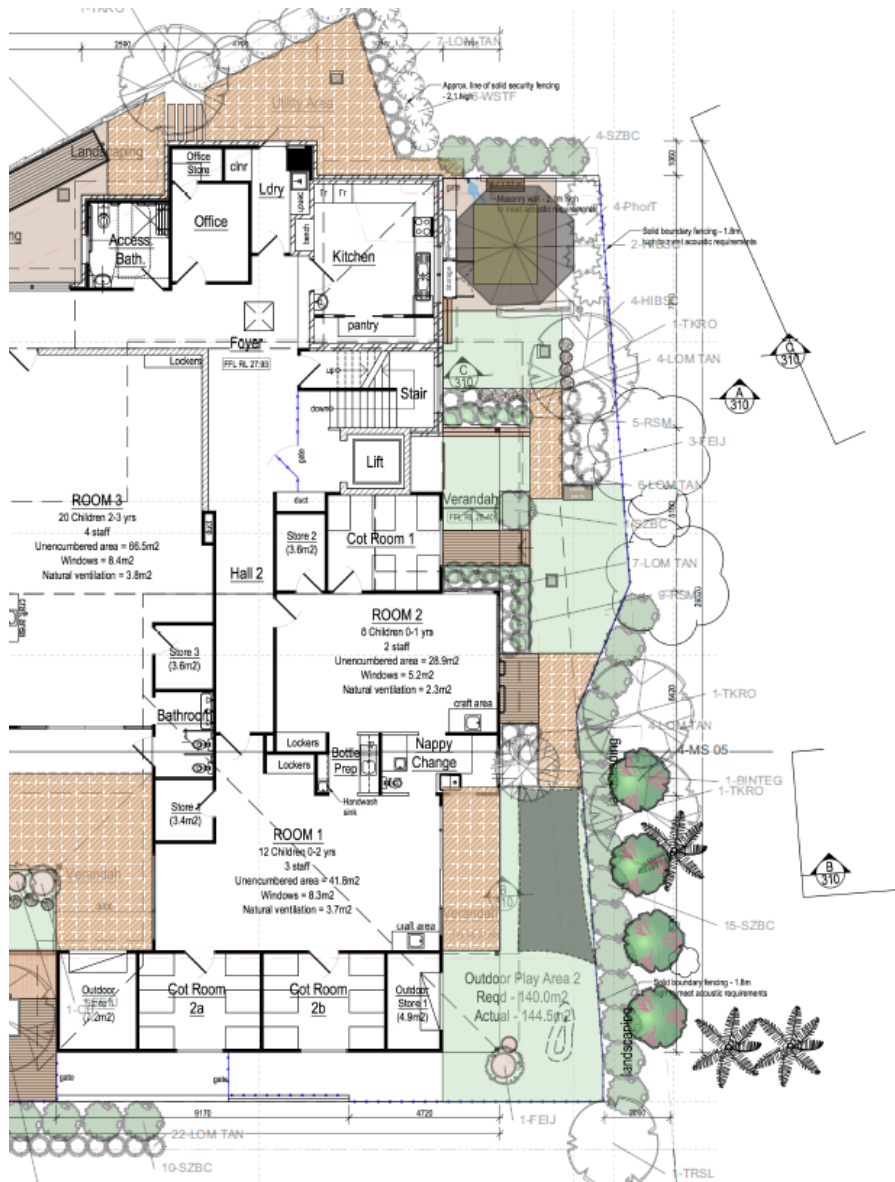


Figure 4 – Landscape plan identifies footprint of adjoining buildings.

The child care centre proposes outdoor play areas (associated with the 0-2 years infant rooms) adjacent to the common boundary with No. 93 Old Gosford Road. Boundary fencing, in addition to landscaping are proposed along this boundary. The Landscape Plan identifies that these play areas will comprise largely rubberised soft fall surfaces.

The Acoustic Report submitted as part of the application, has recommended that a 1.8m high fence be provided along this boundary to protect the amenity of the adjoining properties and to achieve compliance with the Environment Protection Authority (EPA) Industrial Noise Policy criteria (see figure 5).

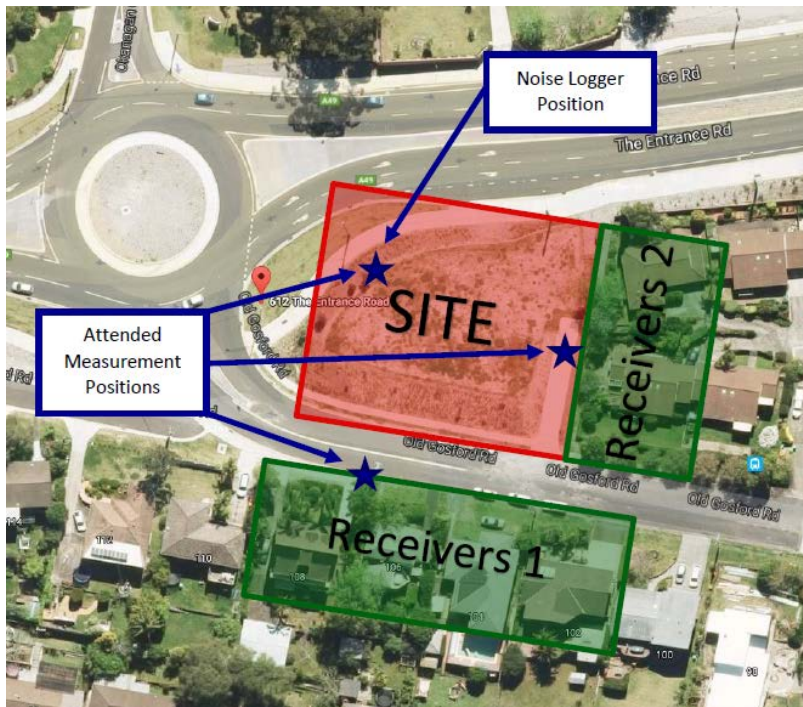


Figure 5 – Extract of Acoustic Report showing noise receptors

This Acoustic Report has undertaken an assessment of noise receptors as shown in Figure 5, and includes consideration of the impact on the adjoining property from noise intrusion from The Entrance Road and Old Gosford Road. The Report identifies that the installation of the 1.8m high fence will maintain noise levels at a reasonable level and not detrimentally affect the amenity of No. 93.

In addition to the acoustic fencing it is considered that the design and operation of the child care centre will minimise noise intrusion in this location. The design of the child care centre will ensure that the areas of most active play occur where they have the least impact on the adjoining property. This is achieved by allocating the play areas for the older children in a space on site that is largely enclosed by the building, and with an orientation to the south, as a means of protection for No. 93 Old Gosford Road. This area is over 15m from the shared boundary with No. 93 Old Gosford Road.

It is noted that children in the 0 - 2yrs age group are indoors for longer periods than older children as they are sleeping more frequently and for longer periods. The glass sliding doors will further minimise noise impacts of internal play time. As such the potential impact on the acoustic privacy of the adjoining properties is considered to be minimal. It is considered that as a consequence of the design elements, the acoustic recommendations and assessment, the proposed landscape treatment of the boundary and the anticipated operation measures (sleep periods, indoor play etc), the proposed child care centre will not impact on the amenity of the adjoining properties.

- *Claim that the noise assessment is inadequate*

Comment:

The application is supported by an acoustic report prepared by PKA Acoustic Consulting and dated 05/12/2016 which considers the impact of noise on the centre and the impact of the use on the neighbours to the east.

Council's Environmental Health Officer has reviewed the Acoustic Report and the methodology for assessing the potential noise disturbance. The report undertook an assessment of the intrusiveness of potential noise, as well as amenity impacts. The report relies upon adopted internal and external noise criteria taken from the EPA's Road Noise Policy. The assessment identified that the proposed child care centre would exceed background noise levels established by the noise loggers and most sensitive receivers. The Report recommends the installation of a 1.8m high solid fence to comply with the criteria.

The Report is of a professional standard to rely upon the recommendations proposed. The recommendations of the Acoustic Report will form conditions of consent.

Council's Environmental Health Officer supports the noise attenuation recommendations of the report and is satisfied with the subject development.

- *Overshadowing concerns*

Given the orientation of the allotment, shadow from the proposed development will only impact on the adjoining properties to the east in the afternoon period, from 1pm onwards during mid winter.

The western elevation of Unit 1 does not have any window openings therefore the bedroom will remain unaffected by the shadow cast at this time. The windows of unit 10 western façade are identified as being within low use rooms (bedroom/bathroom).

The primary open space areas for both Unit 1 and 10 will be unaffected by shadow cast by the proposed child care centre and will retain a minimum of 3 hours of sunlight during the winter period in accordance with the GDCP.

The outdoor area of unit 10 along the shared boundary is secondary open space/utility area and currently contains vegetation which currently shades this area and the side of unit 10.

The building footprint has been moved further east to increase separation between the proposal and the common boundary. The building (above basement) setback to the boundary ranges from 4.09m at the closest point to greater than 4.9m. The height of the proposed building and proposed setbacks, have been considered, and the development will not cast unreasonable shadows on the property to the east, with less shadowing cast by this development compared to that which would be cast by a compliant two storey residential

development built 900mm from the boundary. Shadow diagrams have therefore not been submitted or requested to be provided.

- *Amenity impact caused by bulk and scale of the development*

Comment:

The development utilises the natural site slope to minimise bulk, and has an appropriate mix of materials and roof forms to add visual interest to the design. Additionally, the landscape treatment of the site includes mature plantings and heavy screening to soften the presentation of the building to the street frontages, and adjoining properties. The proposed development is compatible with the surrounding locality.

- *Absence of commercial demand for this type of facility and impact on the existing centres in this location.*

Comment:

The child care centre is a permissible land use. The commercial viability or demand for a child care centre is not a matter for consideration under s79C of the *Environmental Planning & Assessment Act 1979* (EP&A Act). In addition, the SEPP (Education & Child Care) precludes Council from assessing the application on this basis of competition.

- *Insufficient information in relation to the proposed signage for the child care centre*

Comment:

The application does not propose advertising signage in association with the proposed child care centre. Any future signage will need to be assessed on the basis of the merits of a separate development application or be in accordance with the State Environmental Planning Policy (Exempt and Complying Development Codes) 2008 for signage.

Submissions from Public Authorities

The application was referred to the following public authorities:

NSW Roads and Maritime Services (RMS)

The RMS reviewed the application and raised no objection to the proposed development subject to the following being included in the conditions of consent:

- Suitable crash barrier be installed along the Central Coast Highway frontage of the site, to minimise potential risk of harm to site users in the event an errant vehicle enters the site. The barrier is to be at the full cost of the developer and no cost to Roads and Maritime or Council, and to Council's requirements. Note: Consultation has been undertaken with The Red Bus, which services this area.

- They maintain that the bus bay, whilst not signposted as a bus stop, is required as it is used for the school bus service. Therefore the barrier is to be installed along the property boundary.

Appropriate conditions are proposed in this regard.

Internal Consultation

The application was internally referred to the following officers:

Internal Referral Body	Comments
Building	Supported, subject to conditions
Engineering	Supported, subject to conditions
Engineering Traffic/Transport	Supported, subject to conditions
Environmental Health	Supported, subject to conditions
Waste Service (Garbage)	Supported, subject to conditions
Water and Sewer Assessment	Supported, subject to conditions

Engineering Traffic/Transport Summary

The engineering aspects to the proposal are satisfactory, subject to the imposition of specific engineering conditions of consent.

Council's engineers have assessed the traffic impact assessment prepared by Northern Transport Planning and Engineering dated November 2016 and has determined that the proposed development will not have an adverse impact upon the surrounding road network in terms of traffic generation, and carparking.

Austrroads 2010 Guide to Road Design Part 4A for 50km/h speed environment requires a minimum of 69m sight distance for right turn from the driveway, the subject proposal exceeds this requirement by providing 69.5m and is therefore compliant with the applicable standard (i.e. AS2890:1:2004). Notwithstanding this, additional measures are conditioned to be introduced into the street network to improve sight distances and increase safety. It is proposed that the development provide a road blister and restrict an on street parking space during the hours of 8am to 6pm Monday to Friday to improve sight distances in this location.

Environmental Health

Council's Environmental Health officer has assessed the proposal for a Child Care Centre development on the subject premises, in terms of site contamination and acoustic impacts.

Information submitted by the Applicant indicates that historically the land was subdivided and sold. The development proposes excavation to create the outdoor play areas and these will be covered with synthetic surfaces and drainage management systems.

According to Regulation 25 of the *Education and Services National Regulations 2016* additional site contamination investigations are required.

Council's Environmental Health Officer supports the proposal subject to conditions being imposed that require contamination investigations and remediation works (if required) prior to the release of the construction certificate.

Environmental Health Officer supports the proposal subject to conditions relating to the implementation of the recommendations of the acoustic impact assessment and noise validation reporting following occupation to ensure noise goals have been achieved.

Assessment:

Having regard for the matters for consideration detailed in section 79C of the EP&A Act and other statutory requirements, Council's policies and section 149 Certificate details, the assessment has identified the following key issues, which are elaborated upon for Council's information.

Provisions of Relevant Instruments/Plans/Policies:

State Environmental Planning Policies (SEPP)

State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017

The development application was lodged on 9 March 2017 prior to the commencement of State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017 (SEPP Education and Child Care) which came into effect on the 1 September 2017.

Schedule 5 Savings and Transitional Provisions of the SEPP Education and Child Care, clause (1) states that the policy does not apply to the subject development as the application was lodged (made) but not finally determined before the commencement of the policy. Notwithstanding (1), clause (2) states that before determination of a development application referred to in (1) for the purpose of a centre-based child care facility, the consent authority (Council) is to take into consideration the regulatory requirements and the National Quality Framework Assessment Checklist set out in Part 4 of the Child Care Planning Guideline.

The table below provides a summary of Part 4 of the Child Care Planning Guideline and demonstrates how the proposed development meets the Education and Care Services National Regulations.

Regulation	Proposed	Compliance (Yes or No)
104. Fencing or barrier that encloses outdoor spaces	1.8m high (eastern boundary) and 2.1m high (remaining boundaries) fencing to secure the building and the outdoor play areas and provide acoustic protection as shown on architectural plans.	Yes
106. Laundry and Hygiene Facilities	Laundry/hygienic facilities are provided and located where they do not pose a risk to children. The laundry has capacity for a washing machine, dryer, laundry sinks and adequate storage for soiled items prior to cleaning.	Yes
107. Unencumbered indoor space - 3.25m ² per child	80 children, requires 260m ² indoor space Provides – 272.2m ² or 3.4m ² per child	Yes
108. Unencumbered outdoor space - 7m ² per child	The proposal includes some verandah areas that are not otherwise included as indoor space. Outdoor play area 1 for 2-5 years – 60 children - requires 420m ² – provides 424.8m ² Outdoor play area 2 for 0-2 years – 20 children - requires 140m ² – provides 144.5m ² Overall unencumbered outdoor open space provided is an average of 7m ² per child, and individual play areas each comply with the requirement.	Yes
109. Toilet and hygiene facilities	The location and design of the toilet, washing and drying facilities enables safe and convenient use by the children as shown on architectural plans. Separate staff and children's toilets are provided including an accessible bathroom. Children's toilets have direct access to activity rooms, as well as outdoor play areas. An internal viewing window schedule will be included in the construction certificate documentation.	Yes
110. Ventilation and natural light	Achieves natural ventilation and lighting as required by the standard. The relevant areas are shown on the ground floor plan. A raked ceiling is provided to Room 5, all other playroom ceilings are 2.75m.	Yes
111. Administration space	Adequate area is provided for the purposes of conducting the administrative functions, consulting with parents of children and conducting private conversations. A 9.7m ² office/office storage area is proposed adjacent to the main foyer, which meets the requirements of the guidelines.	Yes
112. Nappy change facilities	A defined nappy changing area is proposed for the 0-1 and 0-2 years rooms. Hand cleansing facilities for adults are provided in the immediate vicinity of the nappy change area.	Yes
113. Outdoor space – natural environment	Adequate natural outdoor spaces are provided to allow children to explore and play.	Yes

Regulation	Proposed	Compliance (Yes or No)
114. Outdoor space – shade	Shade is provided to at least 30% of the outdoor area, and distributed across the areas. No more than 60% of the outdoor area will be covered. Verandahs are provided as outdoor space with more than 1/3 rd of the perimeter being open with a minimum ceiling height of 2.1m which achieves the requirements of this regulation.	Yes
115. Premises designed to facilitate supervision	The proposal has been designed to eliminate areas of poor surveillance, and to promote effective supervision of all areas.	Yes

It is considered the proposed development generally meets the requirements of Part 4 of the Child Care Planning Guidelines and is capable of complying with the relevant requirements under the Education and Care Services National Regulations. Demonstration of compliance with the regulatory requirements will be required of the operator prior to the operation of the facility.

Part 3 Early Education and Care Facilities – Specific Development Controls

The proposal does not require consideration under Part 3 of the SEPP Education and Child Care as provided in Schedule 5 Savings and Transitional provisions, sub-clause 1 as stated above.

Notwithstanding this, the relevant provisions are addressed as follows:

Clause 22 - Concurrence of Regulatory Authority

The proposal achieves the requirements of Clause 107 and 108 of the regulations in relation to space requirements (both indoor and outdoor) and therefore does not require concurrence.

Clause 23

The proposed Child Care Centre is generally in keeping with the provisions of the Child Care Planning Guideline as required by the SEPP Education and Child Care.

Clause 25 - Centre-based child care facility—non-discretionary development standards

Development Standard	Required	Compliance
(a) Location	Any distance from an existing or proposed facility	Yes
(b) Indoor or outdoor space	Complies with Clauses 107 & 108 the Education and Care Services National Regulations	Yes
(c) Site area & dimensions	Located on a site of any size and any street frontage	Yes
(d) Colour of building materials	The development may be of any colour or colour scheme unless it is a State or local heritage item or in a heritage conservation area. The site is not a heritage item and external finishes are acceptable.	Yes

Clause 26 - Development Control Plans

The proposal is assessed against the provisions of Chapter 3.9 Child Care and Chapter 7.1 Car Parking of GDCP 2013 and despite minor non-compliances the proposed development is supported.

State Environmental Planning Policy No 55 - Remediation of Land

State Environmental Planning Policy No 55 - Remediation of Land (SEPP 55) requires that a consent authority must not consent to the carrying out of any development on land unless it has considered whether the land is contaminated. The site is not mapped as contaminated land within the contaminated land mapping.

Council records indicate that prior to being used by RMS during the Central Coast Highway construction, the site was used for rural residential purposes.

Council's Environmental Health Officer has reviewed the application for issues relating to contamination of land under SEPP 55 to determine if the site is suitable in the current state for the intended land use. The Environmental Health Officer provides the following comments on this matter;

Subclause (d) of regulation 25 of the Education and Care Services National Regulations 2016 states that one of the following must be provided during the application for service approval:

- *a soil assessment for the site of the proposed education and care service premises;*
- *if a soil assessment for the site of the proposed education and care service premises has previously been undertaken, a statement to that effect, specifying when the soil assessment was undertaken;*
- *a statement made by the applicant that states that, to the best of the applicant's knowledge the site history does not indicate that the site is likely to be contaminated in a way that poses an unacceptable risk to the health of children"*

None of the abovementioned documents were provided to Council as a part of the Development application.

Information provided by the applicant indicates that historically the land was subdivided and sold. Additionally, the application states that there will be excavation under most of the outdoor play areas and these areas will be covered with synthetic surfaces over drainage cells.

Due to the sensitivity of the proposed use of the site and requirements under Regulation 25 of the Education and Services National Regulations 2016 Environmental Health believes that further information is required regarding the potential contamination of the site.

The Environmental Health Team has assessed the application and has no objection provided that the Conditions of consent are adhered to.

As such it is considered that there is a low risk of contamination on the subject site, however it is prudent to require further investigations to be undertaken to ensure that the risk of any potential harm is removed. As such, conditions of consent are recommended which require the applicant to undertake contamination investigations and any remediation works prior to the issue of a Construction Certificate.

State Environmental Planning Policy No 71 - Coastal Protection

The provisions of State Environmental Planning Policy No 71 - Coastal Protection (SEPP 71) require Council consider the Aims and Objectives of SEPP 71 together with the matters for consideration listed in Clause 8 when determining an application within the Coastal Zone. The Coastal Zone is an area defined on maps issued by the NSW Department of Planning & Environment and the subject property falls within this zone.

The proposed development is considered to be consistent with the stated aims as it,

- is not considered to negatively effect the natural, cultural, recreational and economic attributes of the New South Wales coast,
- will not impact on public access to and along coastal foreshores,
- is located within a cleared site and will not impact Aboriginal cultural heritage or values,
- will not impact the visual amenity of the coast, or impact on the beach environment,
- will not impact on coastal vegetation,
- will not impact on the marine environment or rock platforms,
- is not inconsistent with the principles of ecologically sustainable development,
- is of a bulk, scale and size which is appropriate for the low density residential location,
- will not impact on coastal management approaches.

The proposed development is also considered to be consistent with the matters for consideration under clause 8 as it,

- is consistent with the aims of the policy,
- will not impact on public access to and along coastal foreshores,
- is suitable for the low density residential location,
- will not impact the amenity of the coast, or impact on the beach environment, and will not impact on views or overshadowing of the foreshore,
- will not impact the scenic qualities of the coast,
- is considered acceptable and in accordance with the Threatened Species Conservation Act, Fisheries Management Act and being on a cleared site will not impact on wildlife corridors,
- the development will not impact on coastal processes or hazards, and will not affect land based and water based coastal activities,
- is located within a cleared site and will not impact Aboriginal cultural heritage or values,
- will not impact on the water quality of coastal waterbodies,

- will not impact on heritage conservation values,
- in relation to cumulative impacts on the environment, the proposed facility is on cleared land,
- In relation to water and energy usage, the development is to be connected to services which are available.

Gosford Local Environmental Plan 2014

Zoning & Permissibility

The subject site is zoned R2 Low Density Residential under GLEP 2014 (see figure 6). The proposed development is defined as a centre-based child care facility which is a permissible use within the R2 Low Density Residential zone.

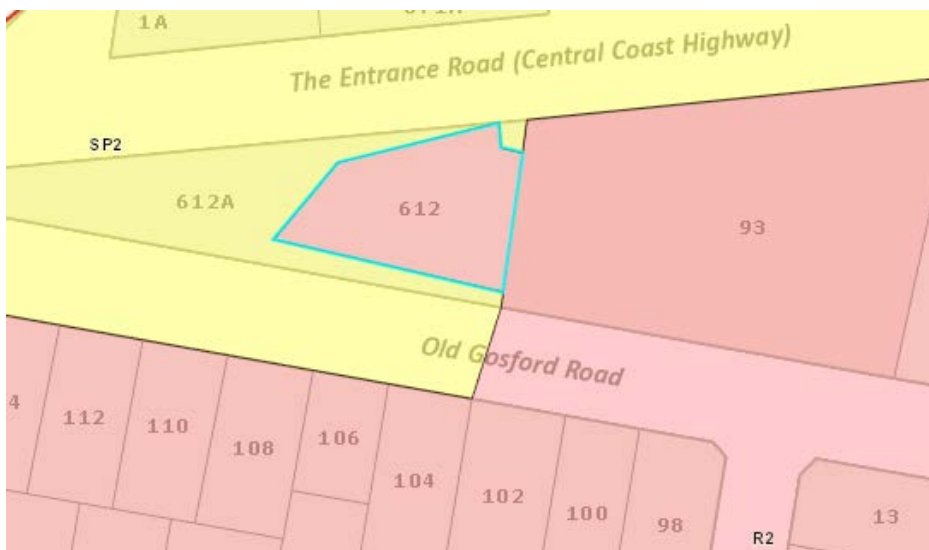


Figure 6 GLEP 2014 Zoning extract (site edged blue)

Under the provisions of GLEP 2014 **centre-based child care facility** means:

- (a) a building or place used for the education and care of children that provides any one or more of the following:
 - (i) long day care,
 - (ii) occasional child care,
 - (iii) out-of-school-hours care (including vacation care),
 - (iv) preschool care, or
- (b) an approved family day care venue (within the meaning of the Children (Education and Care Services) National Law (NSW)),

Note. An approved family day care venue is a place, other than a residence, where an approved family day care service (within the meaning of the Children (Education and Care Services) National Law (NSW)) is provided.

but does not include:

- (c) *a building or place used for home-based child care or school-based child care, or*
- (d) *an office of a family day care service (within the meanings of the Children (Education and Care Services) National Law (NSW)), or*
- (e) *a babysitting, playgroup or child-minding service that is organised informally by the parents of the children concerned, or*
- (f) *a child-minding service that is provided in connection with a recreational or commercial facility (such as a gymnasium) to care for children while the children's parents are using the facility, or*
- (g) *a service that is concerned primarily with providing lessons or coaching in, or providing for participation in, a cultural, recreational, religious or sporting activity, or providing private tutoring, or*
- (h) *a child-minding service that is provided by or in a health services facility, but only if the service is established, registered or licensed as part of the institution operating in the facility.*

The objectives of the R2 Low Density Residential zone are as follows:

- *To provide for the housing needs of the community within a low density residential environment*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents. To ensure that development is compatible with the desired future character of the zone.*
- *To encourage best practice in the design of low-density residential development.*
- *To promote ecologically, socially and economically sustainable development and the need for, and value of, biodiversity in Gosford.*
- *To ensure that non-residential land uses do not adversely affect residential amenity or place demands on services beyond the level reasonably required for low-density housing.*

In this instance, it is considered that the proposal is consistent with the stated objectives of the zone as the development will provide a service for the local residents. The development is of a scale and size which is consistent with the zoning provisions and can be built and managed so as to not adversely affect residential amenity of surrounding properties.

Principal Development Standards

The table below summarises the compliance of the proposal with the relevant development standards of GLEP 2014.

Development Standard	Required	Proposed	Compliance with Controls	Compliance with Objectives
Clause 4.3 - Height of buildings	8.5m	Approx. 7.58m	Yes	Yes
Clause 4.4 - Floor space ratio	0.5:1	0.49:1	Yes	Yes

The proposal complies with the principal development standards of the GLEP 2014.

Coastal Zone

The provisions of Clause 5.5 GLEP 2014 require Council to consider matters in relation to the Coastal Zone. The Coastal Zone is an area defined on maps issued by the NSW Department of Planning & Environment (DP&E) and the subject property falls within this zone.

The relevant matters identified within Clause 5.5(2) have been considered in the assessment of this application. It is considered that the proposed child care centre will not diminish or harm the coastal foreshore or water body within the locality. Water management practices will be employed to ensure that no impact on the water quality results as a consequence of this development. The location of the proposal will ensure that public access to the beach, coastal lagoon or similar are not impeded as a result of the child care centre. The subject site is not affected by coastal hazard.

The proposed works are therefore considered to be consistent with the stated aims and objectives of this clause.

Acid sulfate soils

This land has been identified as being affected by Class 5 Acid Sulfate Soils with no known occurrence locally. The site is approximately 250m from land with a higher risk of acid sulfate soil, and is approximately 22m higher in elevation. The matters contained in Clause 7.1 of GLEP 2014 have been considered. In this instance, the proposed works are not considered likely to lower the water table of nearby Class 1, 2, 3 or 4 land by more than a metre. As such the development is considered acceptable.

Gosford Development Control Plan 2013

Chapter 2.1 Character

The site is located within the Character Area: Wamberal 11: Scenic Buffers of the Character Statement for Wamberal.

The Applicant provides the following comments in relation to the proposed character:

The proposal is for a child care centre on a sloping site with two separate road frontages, one being a main road. Child care centres are a permitted use under the zoning of the land, and by their nature have a different appearance to traditional residential development.

The proposal has been designed in response to the slope of the site, the nature of surrounding development and the need to provide access from Old Gosford Road at a set point removed from the intersection with the Entrance Road. This has resulted in a development located over a basement parking level that maintains street presentations to two separate roads at different levels. The proposed levels of the development have been based on balancing the slope of the site, and the need to provide for a level development as it is undesirable to provide internal steps in a child care centre due to accessibility requirements and safety concerns for young children. Car parking has been provided in a basement level to enable reasonable access from the identified site entry point, to enable reasonable development of the site and to avoid large expanses of open car parking.

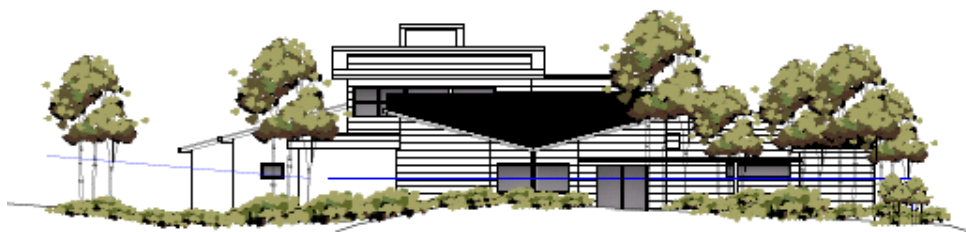
The proposal has been designed with a number of design elements and roof forms to provide a high level of articulation and visual interest, and to reduce the massing of the development given its non-residential nature. The proposed design provides a reasonable presentation to all boundaries, including the use of open verandahs and glazed areas. From The Entrance Road the proposal provides variations in building alignment and roof forms, and a mixture of cladding, painted masonry and metal roofing which is compatible with a main road location.

Extensive landscaping is also proposed between the building and The Entrance Road, which will soften the appearance of the development and will maintain the general leafy character of the area.

Comment:

The Applicant's comments in relation to character are supported and the assessment against the provisions of the Wamberal Open Woodland Foreshores Character Statement is a relevant consideration. The design responds to the constraints of the site and provides generous play spaces within the street setbacks to allow for landscaping and an open low intensity streetscape.

The presentation of the building to the Central Coast Highway and Old Gosford Road is appropriate for this location. See elevations below.



Concept Streetscape - West

Figure 7 Central Coast Highway Elevation

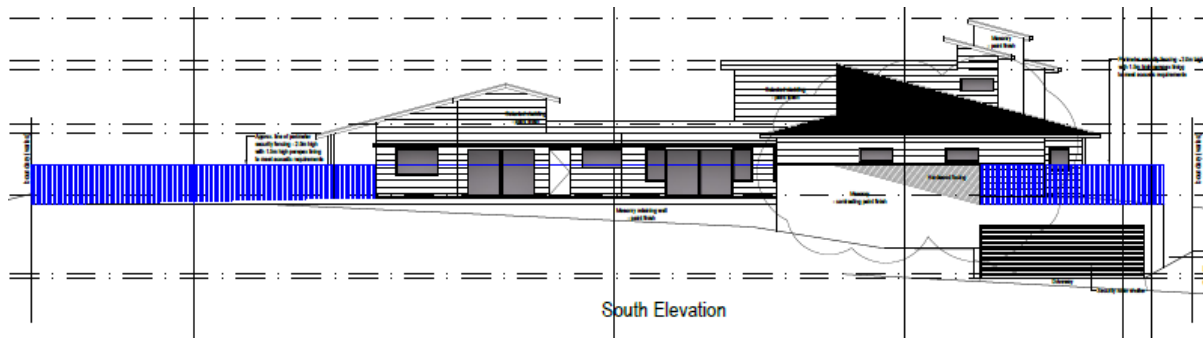


Figure 8 Old Gosford Road Elevation

The presentation of the child care centre at the basement entry (Old Gosford Road frontage) will be a taller element of the design, however garage doors are setback 6.9m to the road. This garage setback is in keeping with surrounding buildings. The Applicant has sought to minimise the bland nature of this façade by adding timber detail to the mix of materials, and this addresses the massing of the structure in this part of the development. The remainder of the street frontage to Old Gosford Road will be landscaped, open style security fencing.

The proposed solid security fencing treatment to the Central Coast Highway frontage is consistent with fencing along this streetscape.

The subject proposal is therefore considered to be in keeping with the requirements of the Character Statement of Chapter 2.1 Character of the GDCP.

Chapter 2.2 Scenic Quality

The visual and scenic impact has been assessed and the development is considered to be consistent with the objectives of protecting scenic quality. The development utilises the natural site slope to minimise bulk, and has an appropriate mix of materials and roof forms to add interest to the design. The proposed development is compatible with the surrounding locality.

Chapter 3.9 Child Care

The relevant provisions of Chapter 3.9 Child Care of GDCP have been considered in the assessment of application as summarised in the table below:

Development Standard	Description	Required	Proposed	Compliance with Controls	Compliance with Objectives
3.9.4.2 - Residential, Business and Industrial Zones	Max site coverage required	40%	41.9%	No - see comments below	Yes
	Max land gradient required	1 in 20 (5%)	8%	Variable non-compliance	Yes - see comments below

Development Standard	Description	Required	Proposed	Compliance with Controls	Compliance with Objectives
3.9.5 - Traffic and Parking	Min entry and exit driveways required	Separate entry/exit on collector road	Single entry/exit drive	No - see comments below	Yes - see comments below
	Min width of driveway required	3.0m	7.0m	Yes	Yes
3.9.6 - Visual and Acoustic Privacy	Max hours of operation required	Hours of operation within residential areas shall not extend outside the core hours of 7am to 7pm.	7am - 6pm Monday - Friday	Yes	Yes
3.9.7 - Design of Outdoor Play Areas	Max height of play equipment required	Equipment shall not be higher than the fence and should not be closer than two metres to a fence	No play equipment proposed adjacent to the fence.	Yes	Yes
	Min outdoor area shading required	50% of all outdoor areas should be shaded during the hours of 10.00 am to 3.00 pm	Approx. 70% shaded areas.	Yes	Yes
3.9.8 - Accessibility	Max width of pathways required	1200 - 1500mm	Access achieves the requirements	Yes	Yes
3.9.10 - Building Appearance	Max slope of land required	5%	Variable non-compliance	No - see comments below	Yes
3.9 - Appendix - Guidelines for Shade Planning and Design	Min head clearance height for fixed play equipment required			n/a	n/a
	Min distance of trees and shade structures from play equipment required			n/a	n/a

The proposal is generally in accordance with the relevant provisions of GDCP 2013 Chapter 3.9 with the exception of the following:

Clause 3.9.3 Location

Chapter 3.9 of GDCP 2013 seeks to ensure that child care centres are located in areas with high environmental quality, provide acoustic measures for neighbours, and are able to be accessed by a range of transport means.

The proposed child care centre achieves the intention of these controls of the GDCP in that it is suitably located relative to public transport, and adjacent to a generous road reserve owned by RMS. While the child care centre is located at an intersection with the Central Coast Highway, the access to the site is obtained from Old Gosford Road. The orientation and design of the child care centre has directed the play and sensitive areas towards the Old Gosford Road frontage to minimise exposure to the highway. The staff areas are located with the closest proximity to the highway.

Clause 3.9.4.2 Residential, Business and Industrial Zones

This clause seeks to ensure that child care centres minimise impacts on adjoining development and provide acceptable standards of design, open space, landscaping and parking.

The proposed child care centre design represents a minor numerical non-compliance with the GDCP requirements in relation to site coverage. While the grade and setbacks as proposed constitute a greater variation to the controls, it is considered that these variations can be supported. The proposal will achieve the objective of this Clause and has been designed to respond to a number of constraints on the property as a consequence of the irregular circumstances of the site.

The proposed building footprint has been designed to adapt to the irregular topography of the site. The design utilises the slope to include a basement car park that will minimise the site coverage and the height of the building. The site will exceed the maximum 5% grade standard, however this is as a consequence of the irregularity of the fall on site. At a distance of approx. 9.7m from the street boundary, in the south eastern corner, the fall represents approximately 8 – 9% grade. This portion, at this grade however represents only 14% of the total site area. The remainder of the site, (some 86%) has a grade of 2.7 – 6%.

The child care centre will be landscaped, and includes additional large canopy tree plantings, and dense screen plantings along the property boundaries. As a consequence it is considered that such a minor numerical variation to the site coverage control can be supported.

In relation to setbacks, the Clause 3.9.4.2 of the GDCP 2013 does not establish a numerical standard but requires consideration of the existing buildings within the streetscape.

The allotment is noted to have an irregular shape and the boundary is angled corresponding to the roundabout intersection with The Entrance Road. When the development is considered against the existing developments in the street, (as required by this clause) the development is considered to have a substantially similar setback to Old Gosford Road.

The area forward of the development is to be densely planted in accordance with the landscaping plan (see figure 9).

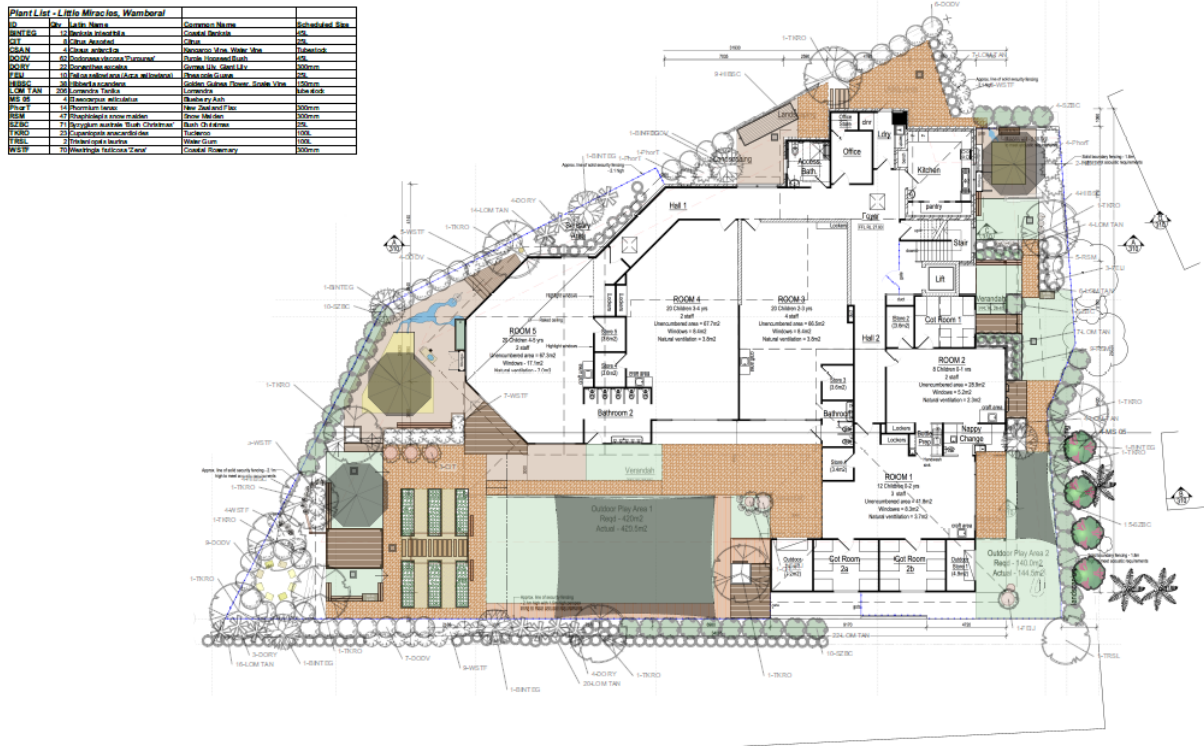


Figure 9 Extract of proposed landscaping plan

It is considered therefore that the development achieves the relevant objective to ensure minimal impact on the adjoining properties and achieving suitable design standards for landscaping, open space and parking.

Clause 3.9.4.4 General Requirements

This clause states that there is no minimum site area requirement for the establishment of a child care centre. However, the number and age of children who may attend the centre and number of staff are subject to the licensing requirements of the NSW Department of Community Services. The designer of the subject child care centre has certified that the proposal has been designed to comply with the relevant NSW Government legislation.

Clause 3.9.5 Traffic and Parking

This clause relating to traffic and parking seeks to ensure the development achieves safe movement of traffic entering and leaving the site, that there is adequate provision for staff parking and drop-off and pick-up parking. Further the clause seeks to promote pedestrian safety.

The Applicant has submitted a Traffic & Parking Assessment Report that demonstrates that safe and convenient access is available to the site. All vehicles will enter and leave the site in a forward direction, and swept path plans have been provided to demonstrate adequate space is provided within the parking area. The basement provides designated a pedestrian pathway which is not impacted by vehicles. Separate entry and exit driveways are not provided, as is promoted by the GDCP due to the proximity of the intersection.

The assessment by Councils traffic engineer has confirmed that the design is however acceptable and due to the constraints of the property the variation is acceptable.

Off street parking is provided in line with the requirements of Chapter 7.1 of GDCP, with no reliance on street parking to meet parking numbers.

The Applicant has also furnished additional information to confirm that special events will be held off site.

Clause 3.9.6 Visual and Acoustic Privacy

This clause seeks to maintain the privacy of neighbours in terms of overlooking and noise, protect the acoustic privacy of the children, and ensure that the hours of operation are reasonable particularly in residential areas. The existing fence provided on site is approximately 1.4m high. This fence will need to be replaced to provide acoustic protection and a height of 1.8m. This will increase both the acoustic and visual privacy of the adjoining units 1 and 10.

The proposed fence is shown on the submitted plans and will stand at the ground level of the proposed play areas. This will allow the landscaping proposed adjacent to the boundary to screen the fencing as it would be viewed from the neighbour's property.

The Environmental Health Team has assessed the proposal for a Child Care Centre development on the subject premises. In consideration of the GDCP, the team supports the proposal subject to conditions relating to the implementation of the recommendations of the acoustic impact assessment and noise validation reporting following occupation to ensure noise goals have been achieved. Conditions of consent are also proposed in relation to permitting operation of the centre between 7:00am and 6:00pm and that special events are to be held off site.

Clause 3.9.9 Landscaping

The landscaping requirement of Clause 3.9.9 seeks to retain trees where possible and provide for an attractive natural environment. The site is currently substantially cleared and so tree retention is not proposed. The Landscape Plan submitted provides a concept design for the proposal that densely plants the setback areas, with a variety of predominantly native species. This includes a range of trees, shrubs, grasses and groundcovers. This will achieve the requirements of the GDCP (see figure 6).

Clause 3.9.10 Building Appearance

The objectives of the GDCP are to ensure child care centres are purpose built, and maintain amenity of residents, integrating the building landscaping and car parking in a pleasant manner. The subject building has been designed to respond to the unique attributes and constraints of the site. This has led to the location of outdoor play areas away from the highway, and utilisation of the slope to provide for underground car parking. This basement parking approach means greater levels of cut and fill, but provides what is considered to be a better streetscape amenity outcome.

Chapter 6.3 Erosion Sedimentation Control

Chapter 6.3 applies to activities which may disturb the soil surface such as during development. Appropriate siltation controls are proposed to be conditioned.

Chapter 6.7 Water Cycle Management

Chapter 6.7 seeks to apply water sensitive urban design and integrated water cycle management approaches to development. The application is supported by a Concept Stormwater Management Plan prepared by Trehy Ingold & Neate and has been considered by Council's Development Engineer. The proposed Water Cycle Management Plan is supported and appropriate conditions are proposed to ensure implementation of the recommendations.

Chapter 7.1 Car Parking

Car parking rates established within Chapter 7.1 of GDCP for Child Care Centres are detailed in the table below:

Description	Required	Proposed	Compliance with Controls	Compliance with Objectives
1 space per person employed in connection with the use, plus a temporary stand area at the rate of 1 car for each 6 children (a minimum of 5 temporary stand spaces).	13 staff = 13 spaces + 80 Children = 14 spaces Total = 27 spaces required	27 spaces provided including: 13 staff and 14 customer spaces	Yes	Yes

The application does not propose a temporary pick up/set down area, however the car parking provided achieves the requirements of the GDCP. A turning area is proposed to facilitate easy maneuvering of vehicles within the basement, and is in an area separated from pedestrians. This area is in addition to the designated 'customer parking' area so as to minimise conflict within the carpark.

Chapter 7.2 Waste Management

In accordance with Chapter 7.2 Waste Management of GDCP, a Waste Management Plan has been submitted. A condition has been proposed.

Likely Impacts of the Development:**a) Built Environment**

A thorough assessment of the impacts of the proposed development on the built environment has been undertaken in terms of GDCP compliance and in terms of the submissions received.

b) Natural Environment

There will be no significant impact upon the natural environment as a result of the proposal.

Ecologically Sustainable Principles:

The proposal has been assessed having regard to ecologically sustainable development principles and is considered to be consistent with the principles.

The proposed development is considered to incorporate satisfactory stormwater drainage, erosion control and the landscaping of the site, and is therefore unlikely to have any significant adverse impacts on the environment and will not decrease environmental quality for future generations. The proposal does not result in the disturbance of any endangered flora or fauna habitats and is unlikely to significantly affect fluvial environments.

The location within residential setting promotes the capacity for drop off and pick up to be convenient to home and the location near the highway network and bus routes also minimises travel distances.

Climate Change

The potential impacts of climate change on the proposed development have been considered by Council as part of its assessment of the application.

This assessment has included consideration of such matters as potential rise in sea level; potential for more intense and/or frequent extreme weather conditions including storm events, bushfires, drought, flood and coastal erosion; as well as how the proposed development may cope, combat, withstand these potential impacts. The proposed development is considered satisfactory in relation to climate change.

Suitability of the Site for the Development:

The site is zoned R2 Low Density Residential which permits Child Care Centres among a range of other uses.

The site is located within an existing residential area on a vacant parcel of land. A review of Council's GIS Mapping data reveals the following potential constraints:

- Acid sulphate soils – there is considered to be low risk as, the GIS records indicate that there is no known occurrence of acid sulphate soils on the site or in the locality.
- Previous use may have resulted in potential contamination – this is considered to be of a low risk, given the proposed site excavation proposed, and the investigation and remediation conditions which are proposed to be imposed.

As such the site is considered suitable for this type of development subject to conditions of consent relating to contamination remediation.

Any Submission Made in Accordance with this Act or Regulations

Submissions received during notification of the proposal have been previously considered within this report.

The Public Interest: (S79c(1)(E)):

The approval of the application is considered to be in the public interest. The proposed development will ensure the orderly and economic development of this vacant parcel of land, provide a service to the community in the form of child care as well as additional employment opportunities.

Other Matters for Consideration:

Section 94 Contributions

The site is not subject to the provisions of any S94 development contribution plan. Therefore, no contributions are applicable.

Water and Sewer Assessment

A 307 Certificate is required, contributions will apply. Sewer is not available, the owner is responsible for extending the Council sewer main to service the property at their cost. The sewer main extension design is to be submitted to the Water Authority and approved before the issue of the Construction Certificate.

Conclusion:

This application has been assessed against the heads of consideration of Section 79C of the EP& A Act 1979 and all relevant instruments and policies. The constraints of the site have been assessed and it is considered that the site is suitable for the proposed development. Subject to the imposition of appropriate conditions, the proposed development is not expected to have an adverse social or economic impact. It is considered that the proposed development will complement the locality and meet the desired future character of the area.

Accordingly, the application is recommended for approval pursuant to Section 80 of the EP&A Act.

Attachments

- | | | |
|----------|-----------------------------|-----------|
| 1 | Draft Conditions of Consent | D12898704 |
| 2 | Development Plans | D12898708 |
| 3 | Landscape Plan | D12898707 |
| 4 | Planting Schedule | D12898706 |

Development Application: DA51671/2017

Proposal: Child Care Centre

Property Address: Lot: 70 DP: 1210387, No. 612 The Entrance Road, Wamberal

PROPOSED CONDITIONS OF CONSENT Part 1

1. PARAMETERS OF THIS CONSENT

1.1. Approved Plans and Supporting Documents

Implement the development substantially in accordance with the plans and supporting documents listed below as submitted by the applicant and to which is affixed a Council stamp "Development Consent" unless modified by any following condition.

Architectural Plans by Knight Mapleton Design Partners

Drawing	Description	Sheets	Issue	Date
16-1112	Site Plan	100	B	03/03/2017
16-1112	Site Analysis	110	B	03/03/2017
16-1112	Concept Cut & Fill	120	B	10/04/2017
16-1112	Carpark Level	200	B	03/03/2017
16-1112	Ground Floor Level	210	B	03/03/2017
16-1112	Upper Level / Roof Plan	220	B	03/03/2017
16-1112	Elevations / Section A-A	310	B	03/03/2017
16-1112	Elevations	300	B	03/03/2017

Landscape Plan by Grow & Co

Drawing	Description	Sheets	Issue	Date
LPP01	Landscape Planting Plan (REV)	1 of 1	B	07/07/2017
LMCP04	Playscape Concept Plan (REV)	1 of 1	A	11/16/16

Supporting Documentation

Title	Prepared by	Date
Waste Management Plan	Peter Banfield Trehy Ingold Neate	23/03/2017
Concept Stormwater Management Plan, Turning Path & Driveway Profile (Rev D), ESC, Roof Drainage (Sheets 1 - 5)	Trehy Ingold Neate	10/02/2017, 22/08/2017
Survey Ref No. C21886	Cahill & Cameron Surveyors	10/07/2017
Acoustic Impact Assessment Report Ref: Project 216156	PKA Acoustic Consulting	05/12/2016
Statement of Compliance	Knight Mapleton Design Partners	24/02/2017
Access & Mobility Statement	Knight Mapleton Design Partners	24/02/2017
Traffic Impact Study	Northern Transport Planning & Engineering Pty Ltd	Nov 2016

- 1.2. Carry out all building works in accordance with the Building Code of Australia.
- 1.3. Prior to the commencement of the operation of the child care facility obtain a Service Approval from the NSW Regulatory Authority for early childhood education.

2. PRIOR TO ISSUE OF ANY CONSTRUCTION CERTIFICATE

- 2.1. All conditions under this section must be met prior to the issue of any Construction Certificate.
- 2.2. No activity is to be carried out on site until any Construction Certificate has been issued, other than:
 - a. Site investigation for the preparation of the construction, and / or
 - b. Implementation of environmental protection measures, such as erosion control etc that are required by this consent.
- 2.3. Submit amendments to the approved plans to the accredited certifier pursuant to Clause 139 of the Environmental Planning Regulation 2000: Applications for construction certificates that must detail:
 - a. The approved plans are certified by the designer to be in accordance with the requirements of NSW Government Legislation for staffing number to student ratio.
 - b. The approved plans be amended as indicated in red to modify the location of the staff parking and provide pedestrian access adjacent to the northern basement wall. Pedestrian access should comply with AS2890.1.
- 2.4. Apply for and obtain from Council (Water Authority) a Section 307 Certificate of Compliance under the Water Management Act 2000. Conditions and contributions may apply to the Section 307 Certificate.

The 'Application for 307 Certificate under Section 305 Water Management Act 2000' form can be found on Council's website www.gosford.nsw.gov.au. Early application is recommended.
- 2.5. The Acoustic Consultant providing an Acoustic Design Review certifying that mechanical plant selection, construction specification and the recommendations of the Acoustic Impact Assessment Report by PKA Acoustic Consulting (Ref: Project 216156 dated 5 December 2016) have been documented and incorporated into the building design and specification prior to the release of the Construction Certificate.
- 2.6. Submit to council the following documents prepared in accordance with Managing Land Contamination - Planning Guidelines SEPP 55 - Remediation of Land (1998) and the National Environment Protection (Assessment of Site Contamination) Measure 1999, amended May 2013:
 1. Stage 1 Preliminary Contamination Investigation in accordance with Clause 3.5.2 of Managing Land Contamination - Planning Guidelines SEPP 55 - Remediation of Land (1998) to determine if the site is suitable in the current state for the intended land use.

2. Stage 2 Detailed Contamination Investigation in accordance with Clause 3.5.3 of Managing Land Contamination - Planning Guidelines SEPP 55 - Remediation of Land (1998) that defines the nature, extent and degree of potential contamination if positively identified in the Phase 1 Preliminary Contamination Investigation. This Stage 2 Investigation must also assess potential risks posed by contaminants to human health / natural environments and to obtain sufficient information to develop a Remedial Action Plan (RAP), if required.
 3. Stage 3 Remedial Action Plan in accordance with Clause 3.5.4 of Managing Land Contamination - Planning Guidelines SEPP 55 - Remediation of Land (1998) that set objectives and documents the process to remediate the site.
 4. Stage 4 Validation and Monitoring Report in accordance with Clause 3.5.2 of Managing Land Contamination - Planning Guidelines SEPP 55 - Remediation of Land (1998) that demonstrates whether the objectives stated in the Stage 3 Remedial Action Plan have been achieved.
- 2.7. Submit an application to Council under Section 138 of the *Roads Act, 1993*, for the approval of required works to be carried out within the road reserve.

Submit to Council Engineering plans for the required works within a public road that have been designed by a suitably qualified professional in accordance with Council's Civil Works Specification and Gosford DCP 2013 Chapter 6.3 - *Erosion Sedimentation Control*. The Engineering plans must be included with the Roads Act application for approval by Council.

Design the required works as follows:

- a. The provision of a Heavy-duty vehicle crossing that has a minimum width of 7.0 m and gradient of a maximum of 5% for the first 6.0metres transitioning to a maximum of 12.5% adjoining the basement floor level in accordance with As 2890.1:2004. The crossing shall also be constructed with 200mm thick concrete reinforced with 1 layer of SL72 steel fabric top and bottom.
- b. Heavy-duty layback to suit the width of the proposed heavy-duty vehicle crossing.
- c. The construction of a kerb blister island, associated chevron linemarking and sign posting within Old Gosford Road adjacent the site. The works shall also include the provision of "No Parking" restriction signage (8am to 6pm Monday to Friday) from the access crossing west along Old Gosford Road and a "Caution Driveway" (Curved Symbol W2-206-2) sign to be installed on the Old Gosford Road exit from the Central Coast highway roundabout. Note: The design engineering plans and specifications shall be endorsed and approved by the Local traffic Committee prior to the issue of the Road Act Approval.
- d. All redundant dish crossings and / or damaged kerb and gutter must be removed and replaced with new kerb and gutter.
- e. All redundant vehicular crossings are to be removed and footway formation reinstated.
- f. All redundant vehicular crossings are to be removed and the footway formation reinstated with turf and a minimum 1.2m wide reinforced (SL72 steel fabric, 100mm thick) concrete footpath in an approved location.

- g. The widening of the existing footpath in reinforced concrete adjoining the existing kerb west of the new access crossing to accommodate the temporary storage of garbage bins during collection days.
- h. A suitable crash barrier installed along the Central Coast Highway frontage of the site along the site's property boundary, to minimise potential risk of harm to site users in the event an errant vehicle enters the site. The barrier shall be at the full cost of the developer and no cost to Roads and Maritime or Council, and shall be to Council's requirements (designed by a certified Structural Engineer in accordance with RMS Guidelines, Austroads Specifications and Australian Standards).
- i. The piping of stormwater from within the site to Council's drainage system located in Old Gosford Road.
- j. Relocation of the speed limit sign westward of the proposed heavy-duty vehicle crossing.
- k. Erosion and sedimentation control plan.

The Roads Act application must be approved by Council.

A fee for the approval of engineering plans under the *Roads Act 1993* applies. The amount of this fee can be obtained by contacting Council's Customer Services on (02) 4325 8222.

- 2.8. Submit a dilapidation report to Council with the Roads Act application and / or Construction Certificate application. The report must document and provide photographs that clearly depict any existing damage to the road, kerb, gutter, footpath, driveways, street trees, street signs or any other Council assets in the vicinity of the development.
- 2.9. Pay a security deposit of \$3780 into Council's trust fund. The payment of the security deposit is required to cover the cost of repairing damage to Council's assets that may be caused as a result of the development. The security deposit will be refunded upon the completion of the project if no damage was caused to Council's assets as a result of the development.
- 2.10. Submit design details of the following engineering works within private property:
 - a. Driveways / ramps and car parking areas must be designed according to the requirements of *AS2890: Parking Facilities* for the geometric designs, and industry Standards for pavement designs. . Note: The driveway access way connecting to the vehicle access crossing shall include suitable entry/exit delineation by a combination of linemarking, pavement markers/rumble bars and appropriate signage.
 - b. A stormwater detention system must be designed in accordance with the Gosford DCP 2013 Chapter 6.7 - Water Cycle Management and Council's Civil Works Specification. The stormwater detention system must limit post development flows from the proposed development to less than or equal to predevelopment flows for all storms up to and including the 1% AEP storm event. A runoff routing method must be used. An on-site stormwater detention report including an operation and maintenance plan must accompany the design. On- site stormwater detention is not permitted within private courtyards, drainage easements, and/or secondary flowpaths.

- c. Nutrient/pollution control measures must be designed in accordance with Gosford DCP 2013 Chapter 6.7 - Water Cycle Management. A nutrient / pollution control report including an operation and maintenance plan must accompany the design.
- d. On-site stormwater retention measures must be designed in accordance with Council's DCP Chapter 6.7 - Water Cycle Management. A report detailing the method of stormwater harvesting, sizing of retention tanks for re-use on the site and an operation and maintenance plan must accompany the design.
- e. Piping of all stormwater from impervious areas within the site via an on-site stormwater detention structure to Council's drainage system located in Old Gosford Road.

These design details and any associated reports must be included in the construction certificate.

- 2.11. Submit details to the Principal Certifying Authority of the design of the fit out of the food premises. The design of the fit out of the food premises is to comply with the *Food Act 2003, Food Regulation 2010, Food Standards Code*, Australian Standard AS4674-2004: *Design, Construction and Fit-out of Food Premises* and Clause G1.2 of the Building Code of Australia (BCA). Details of compliance are to be included in the plans and specifications for the Construction Certificate.
- 2.12. Submit details to the Principal Certifying Authority of any proposed mechanical ventilation systems. The design of the mechanical ventilation is to comply with the relevant requirements of Clause F4.12 of the Building Code of Australia (BCA) and Australian Standard AS1668 Parts 1 & 2 (including exhaust air quantities and discharge location points). These details are to be included in the construction certificate.

3. PRIOR TO COMMENCEMENT OF ANY WORKS

- 3.1. All conditions under this section must be met prior to the commencement of any works.
- 3.2. Appoint a Principal Certifying Authority after the construction certificate for the building work has been issued.
 - a. The Principal Certifying Authority (if not Council) is to notify Council of their appointment and notify the person having the benefit of the development consent of any critical stage inspections and other inspections that are to be carried out in respect of the building work no later than two (2) days before the building work commences.
 - b. Submit to Council a *Notice of Commencement of Building Works* or *Notice of Commencement of Subdivision Works* form giving at least two (2) days notice of the intention to commence building or subdivision work. The forms can be found on Council's website www.gosford.nsw.gov.au
- 3.3. Do not commence site works until the sediment control measures have been installed in accordance with the approved plans / Gosford DCP 2013 Chapter 6.3 - *Erosion Sedimentation and Control*.
- 3.4. Erect a sign in a prominent position on any work site on which building, subdivision or demolition work is being carried out. The sign must indicate:

- a. The name, address and telephone number of the principal certifying authority for the work; and
- b. The name of the principal contractor and a telephone number at which that person may be contacted outside of working hours; and
- c. That unauthorised entry to the work site is prohibited.

Remove the sign when the work has been completed.

- 3.5. Submit both a Plumbing and Drainage Inspection Application, with the relevant fee, and a Plumbing and Drainage Notice of Work in accordance with the *Plumbing and Drainage Act 2011* (to be provided by licensed plumber). These documents can be found on Council's website at: www.gosford.nsw.gov.au.

[Contact Council prior to submitting these forms to confirm the relevant fees.](#)

- 3.6. Provide toilet facilities at a ratio of one toilet plus one additional toilet for every 20 persons employed at the site. Each toilet must:
 - a. Be a standard flushing toilet connected to a public sewer, or
 - b. Have an on-site effluent disposal system approved under the *Local Government Act 1993*, or
 - c. Be a temporary closet approved under the *Local Government Act 1993*.
- 3.7. Prevent public access to the construction site as required by Clause 298 of the Work Health and Safety Regulation 2011 when building work is not in progress or the site is unoccupied. Site fencing specifications are outlined under Australian Standard AS1725.1-2010 - Chain-link fabric fencing - Security fencing and gates. The use of barbed wire and/or electric fencing is not to form part of the protective fencing to construction sites.

A separate application made under the Roads Act 1993 will need to be lodged with Council if a hoarding or construction site fence must be erected on the road reserve or a public place.
- 3.8. Submit to the Principal Certifying Authority plans, details and specifications, prepared by a practising structural engineer, of any retaining wall in excess of 1 metre in height.
- 3.9. Establish a Tree Protection Zone (TPZ) 2m from the eastern boundary in the vicinity of the neighbors three (3) Liquidambar trees, and 1m from the boundary in the vicinity of other smaller adjoining properties trees. Erect a 1.8 metre high chain mesh fence along the Tree Protection Zone that must remain intact until construction is completed. Sign-post fences around Tree Protection Zones to warn of its purpose.
- 3.10. The owner must ensure that all parties/trades working on the site are fully aware of their responsibilities with respect to tree protection conditions.

4. DURING WORKS

- 4.1. All conditions under this section must be met during works.
- 4.2. Clearing of land, excavation, and / or earthworks, building works, and the delivery of building materials must only be carried out between the following hours:

Mondays to Fridays - 7:00am to 6:00pm

Saturdays - 8:00am to 4:00pm except as noted in Clause 'b'

- a. No work is permitted on Sundays and Public Holidays
- b. No work is permitted on:
 - Saturdays when a public holiday is adjacent to that weekend.
 - Construction industry awarded rostered days off.
 - Construction industry shutdown long weekends.

Clause b does not apply to works of a domestic residential nature as below:

- i. Minor renovation or refurbishments to single dwelling construction.
 - ii. Owner occupied renovations or refurbishments to single dwelling construction.
 - iii. Owner builder construction of single dwelling construction; and / or
 - iv. Any cottage constructions, single dwellings or housing estates consisting of predominantly unoccupied single dwellings.
- 4.3. Undertake and maintain Erosion and Siltation control measures in respect to any part of the land where the natural surface is disturbed or earthworks are carried out. The controls must comply with Gosford DCP 2013 Chapter 6.3 - *Erosion and Sedimentation Control*.
- 4.4. Keep a copy of the stamped approved plans on site for the duration of site works and make the plans available upon request to either the Principal Certifying Authority or an officer of Council.
- 4.5. Notify Council when plumbing and drainage work will be ready for inspection(s) and make the work accessible for inspection in accordance with the *Plumbing and Drainage Act 2011*.
- 4.6. Incorporate the following Crime Prevention Through Environmental Design (CPTED) principles and strategies to minimize the opportunity for crime:
- a. Provide adequate lighting to common areas as required under *AS1158: Lighting for roads and public spaces*.
 - b. Paint the ceiling of the car park white.
 - c. Design of landscaping, adjacent to mailboxes and footpaths, must not provide concealment opportunities for criminal activity.
 - d. Design the development to avoid foot holes or natural ladders so as to minimise unlawful access to the premises.
 - e. Provide signage within the development to identify all facilities, entry/exit points and direct movement within the development.

- 4.7. Cease all works if any Aboriginal objects or artefacts are uncovered during works. Immediately contact the NSW Office of Environment & Heritage and comply with any directions or requirements.
- 4.8. Compliance with all Demolition and Construction commitments within the Waste Management Plan dated 23 March 2017 by Trehy Ingold Neate.
- 4.9. Any imported soils to the subject site must be Virgin Excavated Natural Material (VENM) as defined in Schedule 1 of the Protection of the Environment Operations Act 1997.
- 4.10. All excavated material removed from the site must be classified in accordance with NSW EPA (1999) Environmental Guidelines: Assessment, Classification and Management of Liquid and Non-Liquid Wastes prior to disposal. All excavated material must be disposed of to an approved waste management facility.
- 4.11. Construct the works within the road reserve that required approval under the Roads Act. The works must be constructed in accordance with Council's Civil Works Specification and Gosford DCP 2013 Chapter 6.3 - Erosion Sedimentation Control.
- 4.12. Excavations for building are not to occur within the Tree Protection Zone. All care is to be taken not to damage tree roots. Cut with a saw (not rip) any tree roots that maybe found during works.

5. PRIOR TO ISSUE OF ANY OCCUPATION CERTIFICATE

- 5.1. All conditions under this section must be met prior to the issue of any Occupation Certificate.
- 5.2. Submit an application for the Occupation Certificate to the Principal Certifying Authority for approval.
- 5.3. Do not occupy the premises until the Occupation Certificate has been issued.
- 5.4. Submit a Certificate of Compliance for all plumbing and drainage work and a Sewer Service Diagram showing sanitary drainage work (to be provided by licensed plumber) in accordance with the *Plumbing and Drainage Act 2011*.
- 5.5. Construct, grade, drain, seal and line mark including directional arrows with impervious paving material the driveway, vehicle manoeuvring area and {27} car parking spaces as shown on the approved plan, in accordance with *AS2890.1-2004: Parking facilities - Off-street parking*.
- 5.6. Provide certification to the accredited certifier prior to occupation of the premises that the child care centre is constructed in accordance with the requirements of the Child Care Planning Guideline of State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017 and Education and Care Services National Regulations.
- 5.7. Complete works within the road reserve that required approval under the Roads Act. The works must be completed in accordance with Council's Civil Works Specification and Gosford DCP 2013 Chapter 6.3 - Erosion Sedimentation Control, and documentary evidence for the acceptance of such works must be obtained from the Roads Authority.

- 5.8. Rectify any damage not shown in the dilapidation report submitted to Council before site works had commenced. Any damage will be assumed to have been caused as a result of the site works undertaken and must be rectified at the applicant's expense.
- 5.9. Complete the internal engineering works within private property in accordance with the plans and details approved with the construction certificate.
- 5.10. Amend the Deposited Plan (DP) to:
- Include an Instrument under the *Conveyancing Act 1919* for the following restrictive covenants; with the council having the benefit of these covenants and having sole authority to release and modify. Wherever possible, the extent of land affected by these covenants must be defined by bearings and distances shown on the plan.
 - a. Create a 'Restriction as to User' over all lots containing an on-site stormwater detention system and/or a nutrient/pollution facility restricting any alteration to such facility or the erection of any structure over the facility or the placement of any obstruction over the facility.

And,

- Include an instrument under the *Conveyancing Act 1919* for the following positive covenants; with the council having the benefit of these covenants and having sole authority to release and modify. Contact council for wording of the covenant(s).
 - a. To ensure on any lot containing on-site stormwater detention system and / or a nutrient / pollution facility that:
 - (i) The facility will remain in place and fully operational.
 - (ii) The facility is maintained in accordance with the operational and maintenance plan so that it operates in a safe and efficient manner
 - (iii) Council's officers are permitted to enter the land to inspect and repair the facility at the owners cost.
 - (iv) Council is indemnified against all claims of compensation caused by the facility.

Submit, to the Principal Certifying Authority, copies of registered title documents showing the restrictive and positive covenants.

- 5.11. Provide certification from a mechanical engineer to the Principal Certifying Authority that the construction, installation and operation of the exhaust hood ventilation system meet the requirements of:

- *AS/NZS1668 Part 1-1998: The use of ventilation and air-conditioning in buildings - Fire and smoke control in multi-compartment building*

and / or

- *AS/NZS1668 Part 2-2012: The use of ventilation and air-conditioning in buildings - Mechanical ventilation in buildings*

- 5.12. Submit to Council a Food Premises registration form. The form can be found on Council's website www.gosford.nsw.gov.au

- 5.13. Obtain an inspection report from Council's Environmental Health Officer to confirm the final fit-out of the premises complies with the *Food Act 2003, Food Regulation 2010, Food Standards Code, Australian Standard AS4674-2004: Design, Construction and Fit-out of Food Premises* and Clause G1.2 of the Building Code of Australia (BCA).
- 5.14. Comply with all licensing/approval requirements of the NSW Regulatory Authority for early childhood education.
- 5.15. Complete landscaping in accordance with the approved Landscape Plan.

6. ONGOING OPERATION

- 6.1. Operate and maintain all external lights in accordance with the *AS4282-1997: Control of the obtrusive effects of outdoor lighting*.
- 6.2. Operate and maintain all external lights in accordance with the *AS4282-1997: Control of* Do not erect third party advertisement on or in conjunction with the proposed development. Third Party Advertising is defined as an advertising structure or sign displayed on a parcel of land or premises, which does not relate to the lawfully consented use of the property.
- 6.3. Restrict the hours of operation of the use to those times listed below:

Weekdays 7:00am to 6:00pm

Any variation to these hours is subject to the prior consent of Council.

Large special event activities (Christmas end of year concert, Easter hat parade, or the like, that are likely to generate additional parking demands) are to be held off site.

Small special events may occur onsite outside normal peak drop off/collection times. No reliance is to be given to on street parking for these events.
- 6.4. Restrict the maximum number of children attending the child care facility to eighty (80) children. Restrict the maximum number of staff employed at any one time with the proposed use to thirteen (13). Any increase in the number of children or staff will require separate application.
- 6.5. Compliance generally with on-going use details within the Waste Management Plan dated 23 March 2017.
- 6.6. Commercial waste bins to be wheeled to a suitable location at the kerbside on arrival of the Commercial waste contractor and immediately returned to the approved waste storage enclosure after servicing.
- 6.7. Commercial waste bins to be serviced at a suitable frequency to minimize odour and to ensure available waste storage capacity at all times.
- 6.8. An acoustic assessment including a review of operational noise emissions from the Child Care Centre must be undertaken by an accredited Acoustic Consultant at one (1) month after occupation in order to compare operational noise levels to the predicted noise levels (i.e. during peak usage periods, peak arrival and departure times and when the outdoor play

areas are in use) and identify any areas where further noise mitigation measures will be required.

A copy of the Report must be submitted to the Council's Environmental Health Team within fourteen (14) days of the review being undertaken.

- 6.9. Maintain the on-site stormwater detention facility in accordance with the operation and maintenance plan.
- 6.10. Maintain the nutrient / pollution control facilities in accordance with the operation and maintenance plan.
- 6.11. Maintain the linemarking of all car parking areas and spaces required by this consent.
- 6.12. Maintain all works associated with the approved Landscape Plan for a period of 12 months from the date of the issue of any Occupation Certificate to ensure the survival and establishment of the landscaping.
- 6.13. Maintain all works associated with the approved Landscape Plans for a period of 12 months from the date of the issue of any Occupation Certificate to ensure the survival and establishment of the landscaping.
- 6.14. Replace all damaged or missing areas of lawn and plantings at the completion of the landscaping maintenance period, including adjoining road reserve areas that are in a state of decline, to a healthy and vigorous condition in accordance with the approved detailed Landscape Plans and Development Consent Conditions.

7. PENALTIES

Failure to comply with this development consent and any condition of this consent may be a **criminal offence**. Failure to comply with other environmental laws may also be a **criminal offence**.

Where there is any breach Council may without any further warning:

- Issue Penalty Infringement Notices (On-the-spot fines);
- Issue notices and orders;
- Prosecute any person breaching this consent, and/or
- Seek injunctions/orders before the courts to retain and remedy any breach.

Warnings as to Potential Maximum Penalties

Maximum Penalties under NSW Environmental Laws include fines up to \$1.1 Million and/or custodial sentences for serious offences.

8. ADVICE

- 8.1. Ensure the proposed building or works comply with the requirements of the Disability Discrimination Act.

NOTE: The Disability Discrimination Act (DDA) is a Federal anti-discrimination law.

The DDA covers a wide range of areas including employment, education, sport and recreation, the provision of goods, services and facilities, accommodation and access to premises. The DDA seeks to stop discrimination against people with any form of disability including physical, intellectual, sensory, psychiatric, neurological, learning, disfigurement or presence in the body of a disease-causing organism. This development consent does not indicate nor confirm that the application complies with the requirements of the DDA.

- 8.2. The inspection fee for works associated with approvals under the Roads Act is calculated in accordance with Council's current fees and charges policy.
- 8.3. Payment of a maintenance bond may be required for civil engineering works associated with this development. This fee is calculated in accordance with Council's fees and charges.
- 8.4. Council must be notified immediately of any new information which comes to light during remediation, demolition or construction works which has the potential to alter previous conclusions about site contamination and remediation
- 8.5. Consult with public authorities who may have separate requirements in the following aspects:
 - a. *Australia Post* for the positioning and dimensions of mail boxes in new commercial and residential developments;
 - b. *Jemena Asset Management* for any change or alteration to the gas line infrastructure;
 - c. *Ausgrid* for any change or alteration to electricity infrastructure or encroachment within transmission line easements;
 - d. *Telstra, Optus* or other telecommunication carriers for access to their telecommunications infrastructure.
 - e. *Central Coast Council* in respect to the location of water, sewerage and drainage services.
- 8.6. Carry out all work under this Consent in accordance with SafeWork NSW requirements including the *Workplace Health and Safety Act 2011 No 10* and subordinate regulations, codes of practice and guidelines that control and regulate the development industry.
- 8.7. Dial Before You Dig

Underground assets may exist in the area that is subject to your application. In the interests of health and safety and in order to protect damage to third party assets please contact Dial Before You Dig at www.1100.com.au or telephone on 1100 before excavating or erecting structures. (This is the law in NSW). If alterations are required to the configuration, size, form or design of the development upon contacting the Dial Before You Dig service, an amendment to the development consent (or a new development application) may be necessary. Individuals owe asset owners a duty of care that must be observed when working in the vicinity of plant or assets. It is the individual's responsibility to anticipate and request the nominal location of plant or assets on the relevant property via contacting the Dial Before You Dig service in advance of any construction or planning activities.

Telecommunications Act 1997 (Commonwealth)

Telstra (and its authorised contractors) are the only companies that are permitted to conduct works on Telstra's network and assets. Any person interfering with a facility or installation owned by Telstra is committing an offence under the *Criminal Code Act 1995 (Cth)* and is liable for prosecution. Furthermore, damage to Telstra's infrastructure may result in interruption to the provision of essential services and significant costs. If you are aware of any works or proposed works which may affect or impact on Telstra's assets in any way, you are required to contact: Telstra's Network Integrity Team on phone number 1800 810 443.

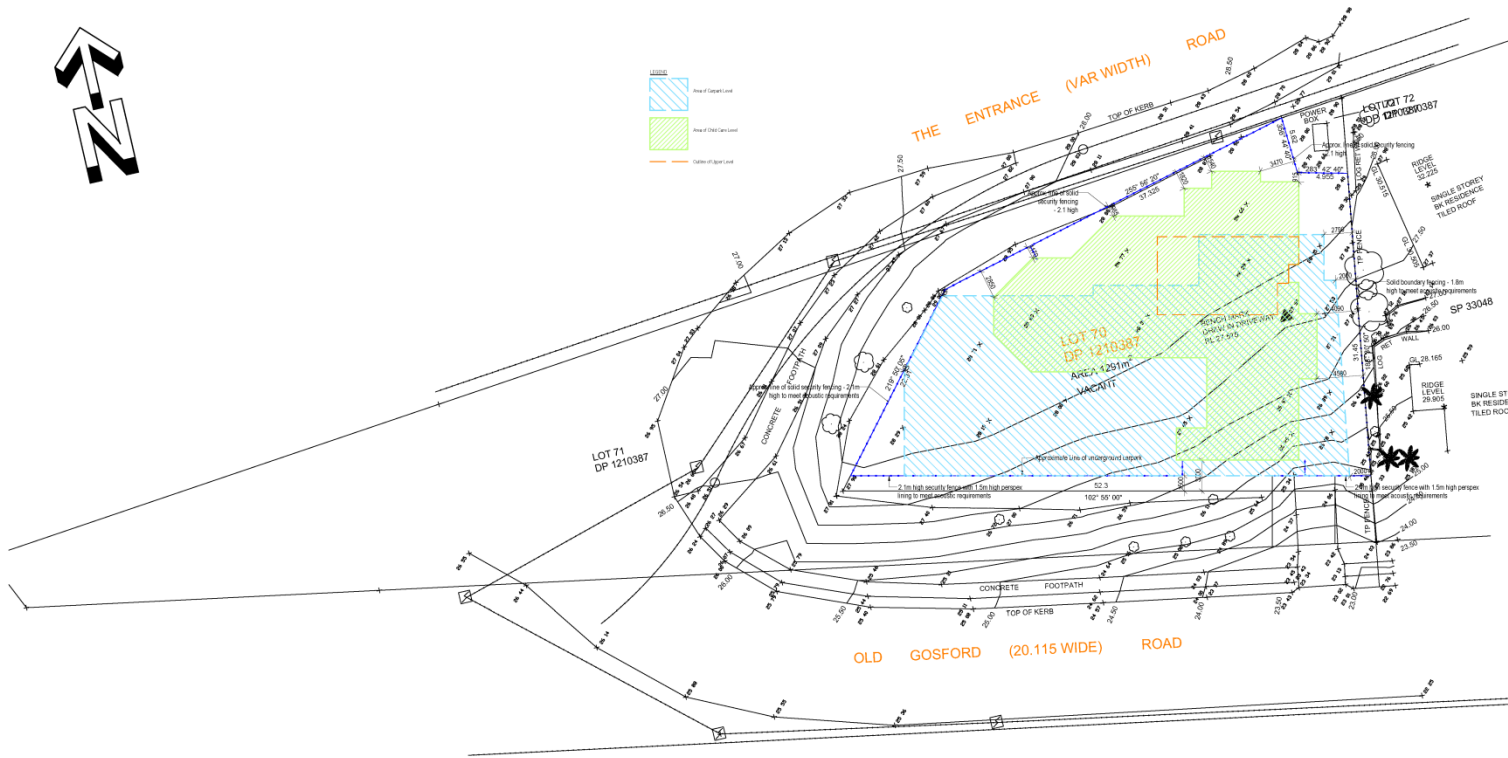
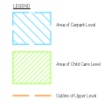
- 8.8. Separate application is required should the applicant require a new or upsized water supply connection to Council's water supply system.
- 8.9. Install and maintain backflow prevention device(s) in accordance with Council's *WS4.0 Backflow Prevention Containment* Policy. This policy can be found on Council's website at: www.gosford.nsw.gov.au

9. REVIEW OF DETERMINATION

- 9.1. Subject to provisions of Section 82A of the Act the applicant may make an application seeking a review of this determination, providing it is made in time for Council to determine the review within six (6) months of this determination.

10. RIGHT OF APPEAL

- 10.1. Section 97 of the Act confers on an applicant who is dissatisfied with the determination of a consent authority a right of appeal to the Land and Environment Court within six (6) months, from the date of determination.
- 10.2. To ascertain the date upon which the determination becomes effective refer to Section 83 of the Act.



CONTRACTOR MUST OBTAIN ALL NECESSARY PERMITS FROM THE LOCAL GOVERNMENT AND BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS. ANY DEVELOPMENT MUST BE IN ACCORDANCE WITH THE LOCAL GOVERNMENT'S DEVELOPMENT CONTROL BY-LAWS AND ANY OTHER APPLICABLE LEGISLATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR THE PROVISION OF ALL NECESSARY SERVICES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION OF ALL NECESSARY SERVICES AND FOR THE PROVISION OF ALL NECESSARY SERVICES.

Item	Description	Size	Unit
B	Development of site	21117	sqm
	Development of site		

Item	Description	Size	Unit
B	Development Application	3917	
A	Concept for the DA	4919	

Scale	Date	Issue
1:500 @ A1	AUG 14	CPS

Sheet No	Sheet Title	Sheet Size
100	B	1

Site Plan

Client: Child Care Centre

Address: Lot 70 / DP 1210387, 612 The Entrance Road, Wamberal, NSW

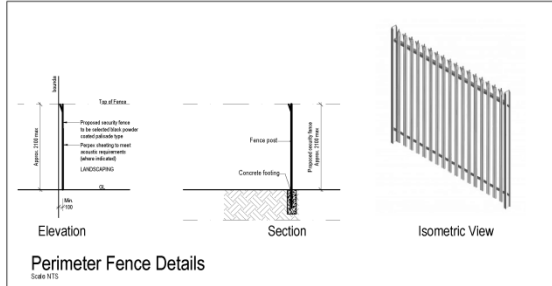
Project Name: Little Miracles

Site Authority: Central Coast Council

Designer: knight-maple design + plans sustainable building design

Address: PO BOX 304 UMBA BEACH NSW 2237, 358 OCEAN BEACH RD, UMBA BEACH, NSW, PH: 02 8542 3741, email: km@knightmaple.com.au, www: www.knightmaple.com.au

Accredited Building Designer No. 6137, Chartered member No. 649-00 of bda BUILDING DESIGNERS AUSTRALIA NSW

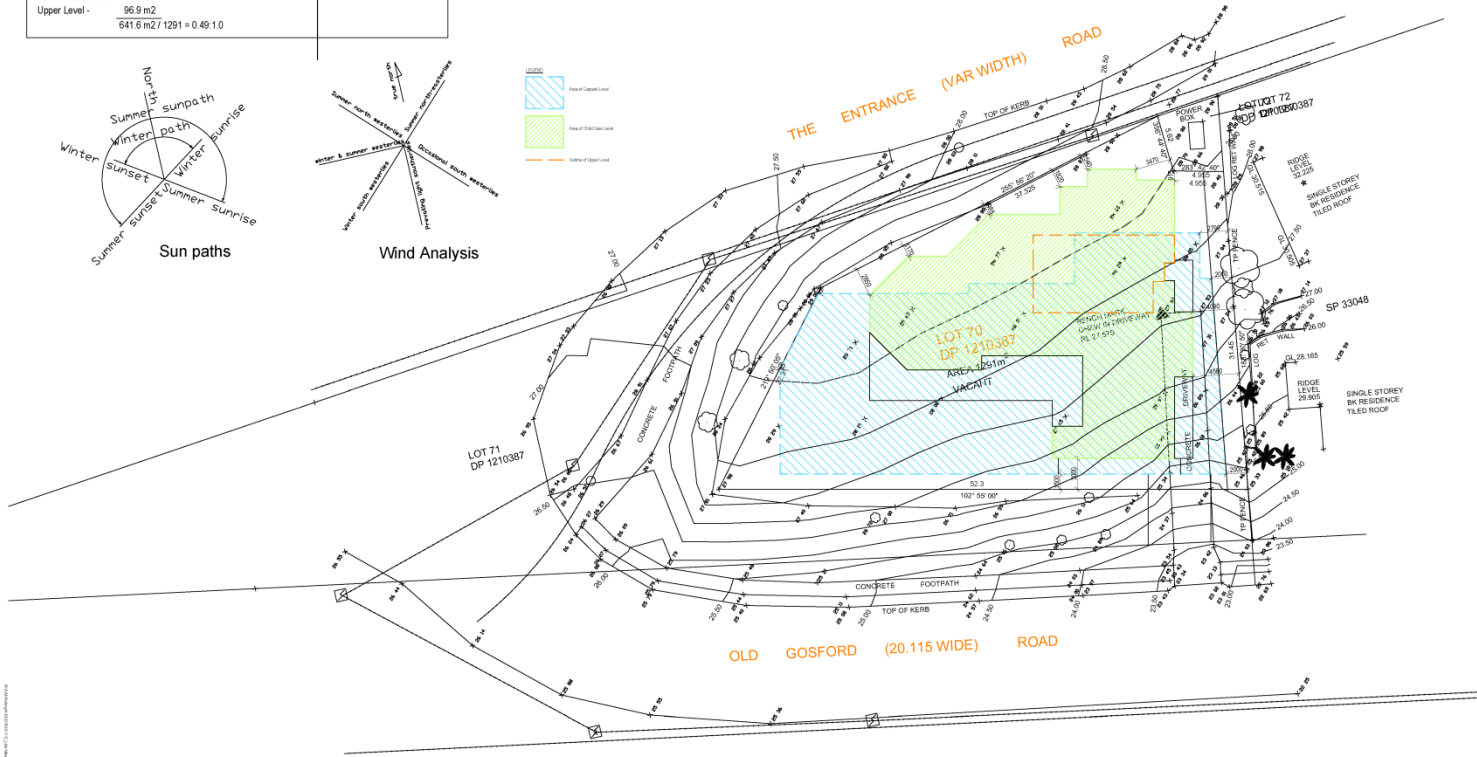
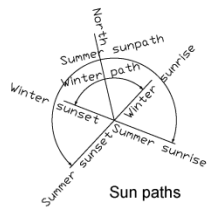


NOTE: ALL LEVELS INDICATED ARE TO AND FROM THE DATUM. ALL LEVELS AND DIMENSIONS ARE APPROXIMATE ONLY.

SCALE: 1:200 @ A1

NOT FOR CONSTRUCTION

SITE CALCULATIONS		
SITE AREA: 1291 m ²		
Building footprint -	532.1 m ² (41.2%)	CAR PARKING REQUIREMENTS Child Care Centre: - 1 space per 6 children = 13.33 spaces - 1 space per staff = 15.0 spaces Required = 29 spaces Proposed on site = 27 spaces
Verandahs -	115.0 m ² (8.9%)	
Carpark, footpath & driveway -	831.9 m ² (64.4%)	
Fenced outdoor play areas (Incl. verandahs) -	569.3 m ² (44.1%)	
Balance open space - (Incl. utility area)	189.6 m ² (14.7%)	
FSR -		
Foyer -	26.1 m ²	
Child Care Centre -	518.6 m ²	
Upper Level -	96.9 m ²	
	641.6 m ² / 1291 = 0.49:1.0	



NOTE: ALL LEVELS INDICATED ARE TO B.M.D. ELEVATIONS AND GRADE LEVELS SHOWN ARE APPROXIMATE ONLY.

SCALE: 1:200 @ A1

CONTRACTOR MUST OBTAIN ALL NECESSARY PERMITS FROM THE LOCAL COUNCIL AND STATE GOVERNMENT BEFORE COMMENCING ANY CONSTRUCTION TO BE APPROVED TO THE PROPOSED DEVELOPMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL COUNCIL AND STATE GOVERNMENT.

Sheet	Description	Date	By
8	Development submitted on site	2/1/11	DS
	Development issued		

Development Application 3/17	
Approved for the DA	4/9/18
Scale	Sheet
1:200 @ A1	AUG 14
CRS	CPB
Sheet no	Issue
110	B
Sheet	1

Site Analysis

Project: Child Care Centre

Site: Lot 79 / DP 1210387, 612 The Entrance Road, Wamberal

Client: Little Miracles

Local Authority: Central Coast Council

Design: **km** knight-mapleton design-mapleton sustainable building design


Address: PO BOX 304 UMBA BEACH NSW 2237, 338 OCEAN BEACH RD, UMBA BEACH, NSW 2412, 02 6642 3714


Accredited Building Designer No. 6137, Chartered member No. 645-00 of **bda** BUILDING DESIGNERS AUSTRALIA NSW

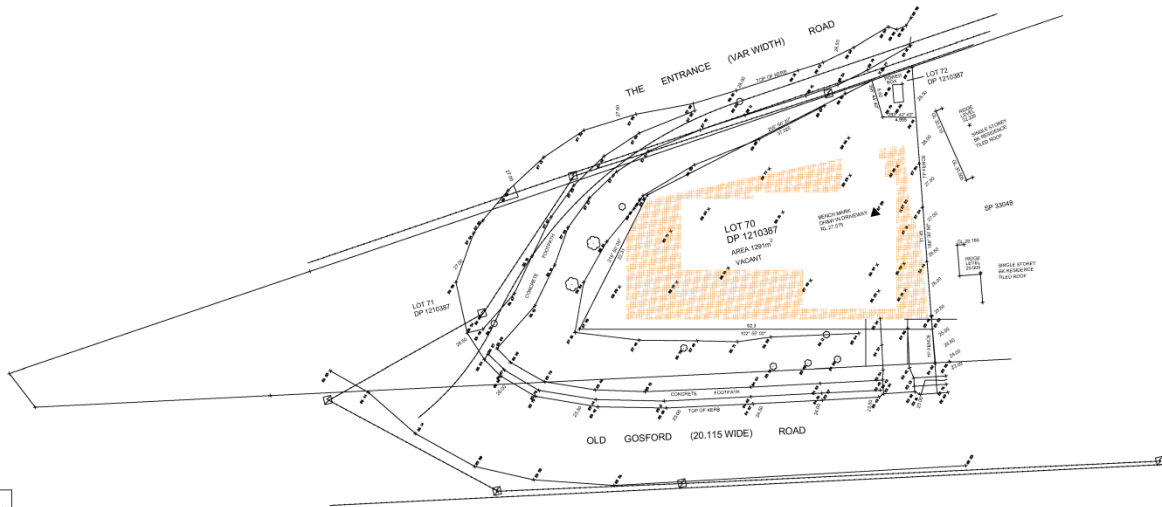
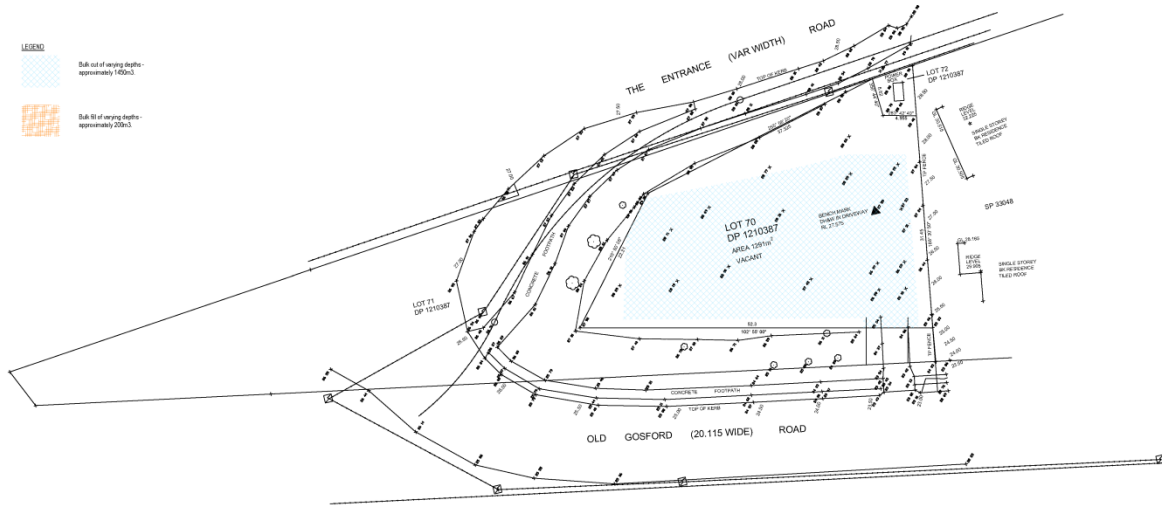
NOT FOR CONSTRUCTION



LEGEND

 Bulk fill of varying depths - approximately 1.60m

 Bulk fill of varying depths - approximately 2.0m



NOTE
LEVELS INDICATED ARE TO B.S.
LEVELS AND GRIDLINE LINES SHOWN ARE APPROXIMATE ONLY.

SCALE
1:300 @ A1

NOT FOR CONSTRUCTION

CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE PRIOR TO START AND NOTIFY ARCHITECT IMMEDIATELY IN WRITING OF ANY DISCREPANCIES. ANY DISCREPANCIES ARE TO BE REPORTED TO THE PROJECT SUPERVISOR FOR INVESTIGATION AND TO BE MADE IN ACCORDANCE TO SCALE MEASUREMENTS AND APPROXIMATE ONLY.

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Issue	Description	Date	By
1	Development issued on site	21/11/20	
2	Development issued on site		

Issue	Description	Date	By
1	Development Application	10/6/17	
2	Concept for the DA	No issue	

Scale	Date	Issue
1:300 @ A1	AUG 14	CPS

Project No	Drawing No	Issue	Sheet
120	B	1	

Project
Concept Cut & Fill Plan

Client
Child Care Centre

Site
Lot 70 / DP 1210387
612 The Entrance Road, Warburton

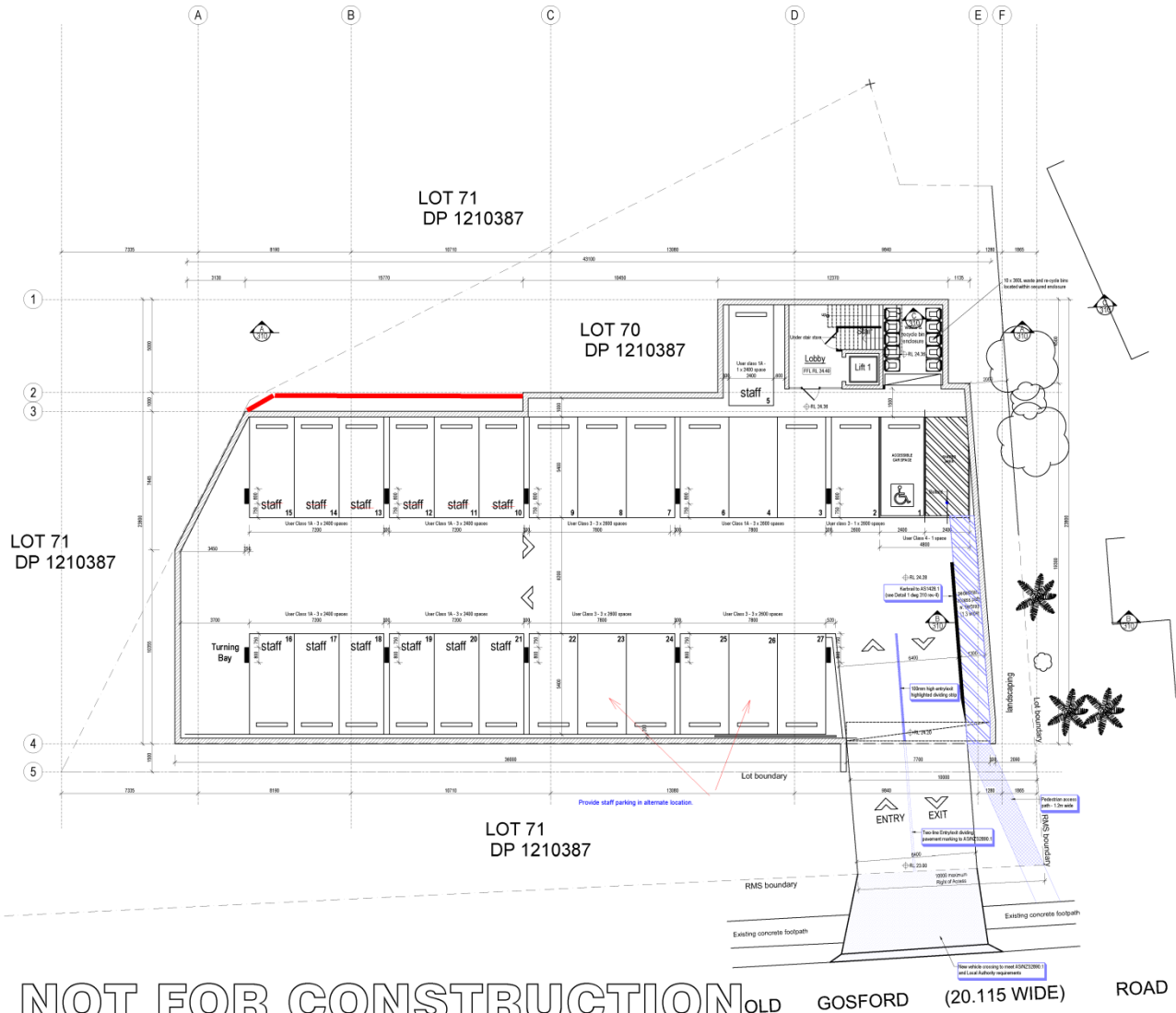
Client
Little Miracles

Client Authority
Central Coast Council

Architect
knight-mapleton
design-mapleton
sustainable
building design

PO BOX 304 UMBA BEACH NSW 2237
358 OCEAN BEACH RD, UMBA BEACH, NSW
PH: 02 4542 2714
email: km@knightmapleton.com.au
www: www.knightmapleton.com.au

Accredited Building Designer No. 657-00 of
BDA BUILDING DESIGNERS AUSTRALIA NSW



CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE PRIOR TO START AND NOTIFY ARCHITECT IMMEDIATELY IN WRITING OF ANY DISCREPANCIES. ANY DISCREPANCIES TO BE REPORTED TO ARCHITECT IMMEDIATELY. FOR DIMENSIONS TO BE USED FOR CONSTRUCTION, CONTRACTOR MUST VERIFY DIMENSIONS ON SITE PRIOR TO START AND NOTIFY ARCHITECT IMMEDIATELY IN WRITING OF ANY DISCREPANCIES. ANY DISCREPANCIES TO BE REPORTED TO ARCHITECT IMMEDIATELY.

Item	Description	Size	Qty
1	Survey for all services	20000	1
2	Excavate, remove 100mm concrete and rebar	21117	1
3	Reinforced concrete slab	7028	1

Item	Description	Size	Qty
1	Excavate, remove 100mm concrete and rebar	21117	1
2	Reinforced concrete slab	7028	1

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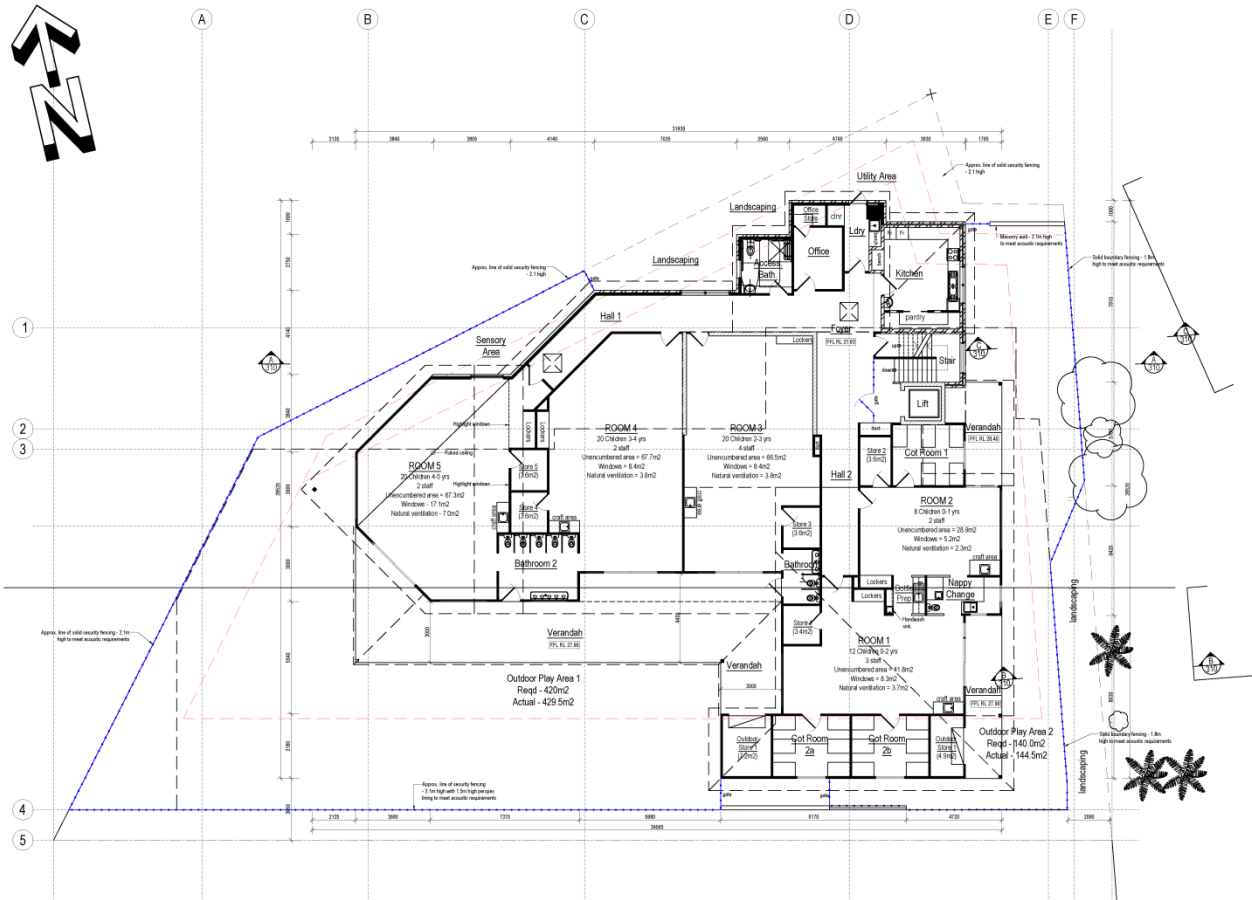
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Item	Description	Size	Qty
1	Excavate, remove 100mm concrete and rebar	21117	1
2	Reinforced concrete slab	7028	1

NOT FOR CONSTRUCTION

OLD GOSFORD (20.115 WIDE) ROAD

NOTE: ALL LEVELS INDICATED ARE TO FINISH. LEVELS AND DIMENSIONS ARE APPROXIMATE ONLY.
SCALE: 1:100 @ A1



NOTE:
 1. LEVELS INDICATED ARE TO HD
 2. LEVELS AND DIMENSIONS SHOWN ARE APPROXIMATE ONLY

SCALE
 1:100 @ A1

NOT FOR CONSTRUCTION

CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE PRIOR TO START AND NOTIFY ARCHITECT IMMEDIATELY IF ANY DISCREPANCIES ARE IDENTIFIED. ANY DISCREPANCIES ARE TO BE REPORTED TO THE PROJECT SUPERVISOR IMMEDIATELY. DIMENSIONS ARE TO FACE UNLESS OTHERWISE SPECIFIED. ALL DIMENSIONS TO FACE UNLESS OTHERWISE SPECIFIED.

THE ARCHITECT AND THE ENGINEER HAS CONDUCTED VISUAL ACCESSIBILITY TO CONFIRM ACCESSIBILITY AND THE COPIES USED OR APPROVED IN ANY WAY WITHOUT THE EXPRESS PERMISSION OF THE ARCHITECT OR ENGINEER.

Level	Description	Area	Notes
B	1. All outdoor play areas and verandahs, outdoor furniture	10,817	GS
	2. Outdoor play areas - 100mm x 100mm (actual) - 21,517	21,517	GS

E Development Application		3/3/17
A Concept for the DA		4/9/16
Scale		
Scale	Site	Block
1:100 @ A1	AUG 14	CPS
Sheet No	Drawing No	Issue
44/102	210	B 2

Drawing: **GROUND FLOOR LEVEL**

Project: **Child Care Centre**

Site: **Lot 70 / DP 1210387
612 The Entrance Road, Wamberal**

Client: **Little Miracles**

Issue Authority: **Central Coast Council**

knights-pleton
 design-partners
 architects



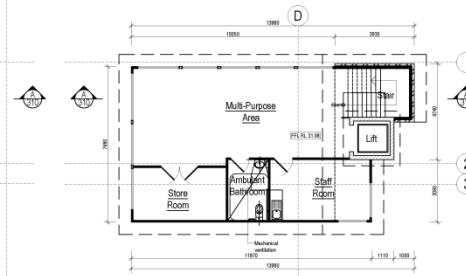
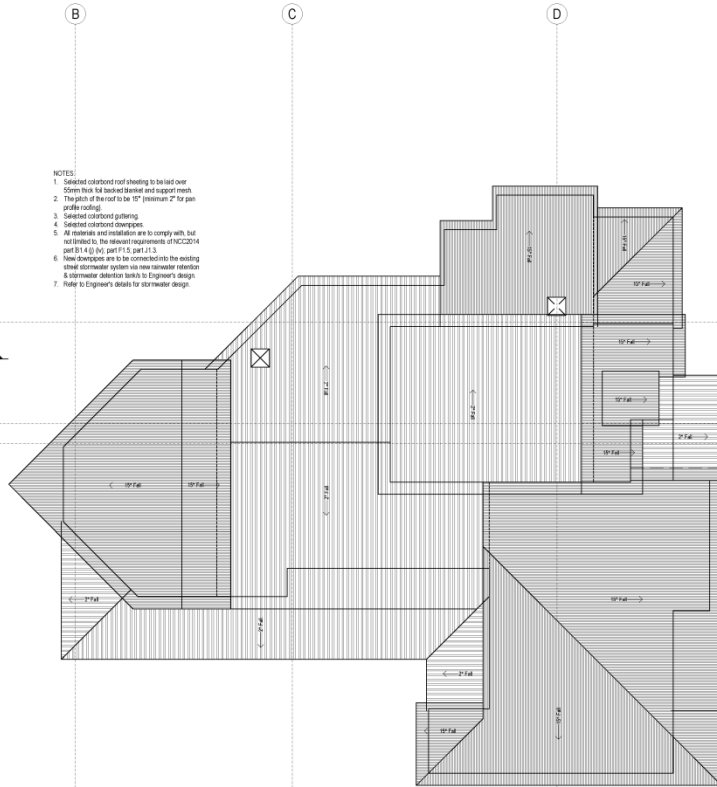
PO BOX 304 UMBA BEACH NSW 2237
 358 OCEAN BEACH RD, UMBA BEACH, NSW
 ph: 02 664 9734
 email: km@knightspleton.com.au
 web: www.knightspleton.com.au

Accredited Building Designer No. 6137
 Chartered member No. 649-00 of
bdm BUILDING DESIGNERS AUSTRALIA



NOTES

1. Selected coloured roof sheeting to be laid over 50mm thick foil backed batten and support mesh.
2. The pitch of the roof to be 10° (minimum 2° for gull profile roofing).
3. Selected coloured gutters.
4. Selected coloured downpipes.
5. All materials and installation to comply with, but not limited to, the relevant requirements of NCC2014 part 3.11.4.2 (for part 3.11.5, part 3.11.3).
6. New downpipes are to be connected into the existing street drainage system via new removable rainwater & stormwater detection tanks to Engineer's design.
7. Refer to Engineer's details for stair mezzanine design.



Upper Level

4
5

NOTE
LEVELS INDICATED ARE TO H.O.
LEVELS AND GROUND LINES SHOWN ARE APPROXIMATE ONLY.

SCALE
1:100 @ A1

NOT FOR CONSTRUCTION

CONTRACTOR MUST OBTAIN ALL NECESSARY PERMITS FROM THE LOCAL COUNCIL AND STATE GOVERNMENT AGENCIES. ANY DEVELOPMENT MUST BE APPROVED BY THE LOCAL COUNCIL AND STATE GOVERNMENT AGENCIES. THE CONTRACTOR MUST OBTAIN ALL NECESSARY PERMITS FROM THE LOCAL COUNCIL AND STATE GOVERNMENT AGENCIES. ANY DEVELOPMENT MUST BE APPROVED BY THE LOCAL COUNCIL AND STATE GOVERNMENT AGENCIES.

Sheet	Description	Date	By
B	Upper Level Floor Plan	21/11/17	GS

Development Application 30317	
Concept for the DA 4/9/18	
Scale	Sheet
1:100 @ A1	AUG 18
Project No	Issue
44110	B
Sheet	Sheet
	1

Project
Child Care Centre

Site
Lot 70 / DP 1210387
612 The Entrance Road, Wamberal

Client
Little Miracles

Role authority
Central Coast Council

knigh-t-maplion
design-partners
sustainable
building design

Km

PO BOX 394 UMBA BEACH NSW 2237
356 OCEAN BEACH RD, UMBA BEACH, NSW
PH: 02 8642 3741
email: km@knigh-tmaplion.com.au
www: www.knigh-tmaplion.com.au

Accredited Building Designer No. 657-00 of
Chartered number No. 657-00 of
bda BUILDING DESIGNERS AUSTRALIA NSW

EXTERNAL COLOUR SCHEDULE:
 ROOF
 GUTTERS, FASCIA & DOWNPIPES
 WALLS - CLADDING
 WALLS - MASONRY
 WINDOW FRAMES

- COLOURBOND 'BASALT'
 - COLOURBOND 'BASALT'
 - EQUAL TO DULUX 'CRUST'
 - EQUAL TO DULUX 'WAYWARD GREY'
 - WHITE

CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE PRIOR TO START AND NOTIFY ARCHITECT IMMEDIATELY IN WRITING OF ANY DISCREPANCIES. ANY DISCREPANCIES MUST BE REPORTED TO THE PROJECT SUPERVISOR IMMEDIATELY FOR RESOLUTION AND TO BE TAKEN INTO ACCOUNT PRIOR TO COMMENCEMENT OF WORK. ALL DIMENSIONS ARE TO FACE UNLESS SPECIFIED OTHERWISE.

Item	Description	Qty	Unit
B 1	Colour schedule add'd	15/17	qps
B 2	Replaced, lowered	2/117	qps

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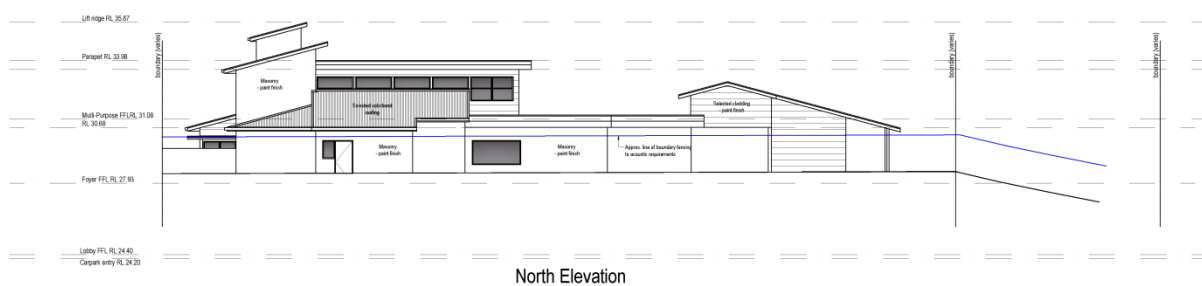
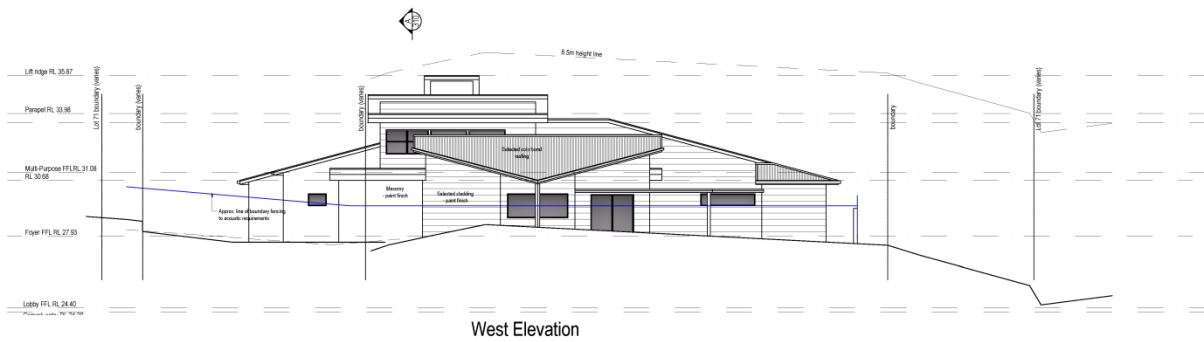
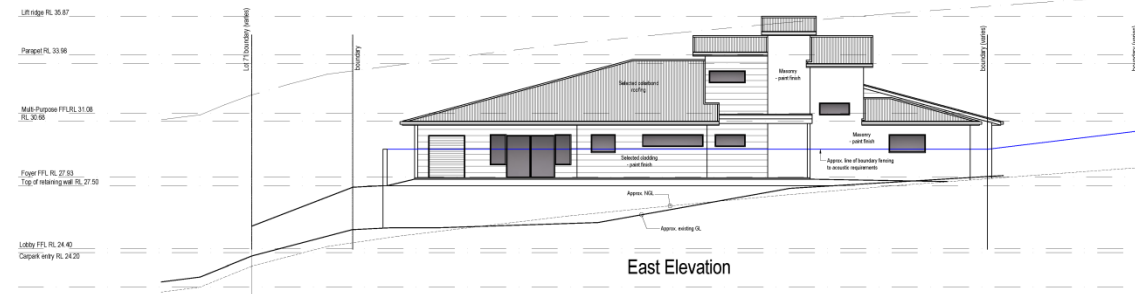
Development Application		3/2/17	
Concept for the DA		4/2/18	
Issue	Drawn for	Scale	Date
Scale	1:100 @ A1	Scale	AUG 14
Issue	300	Issue	B 2

Project Name:
 Child Care Centre

Address:
 Lot 70 / DP 1210387
 612 The Entrance Road, Warburton
 Little Miracles

Architect:
 Knight-Mapleton Architects
 Sustainable Building Design
 40 BOX 394 UMBA BEACH NSW 2237
 358 OCEAN BEACH RD, UMBA BEACH, NSW
 PH: 02 8642 3741
 EMAIL: km@knightmapleton.com.au
 WWW: www.knightmapleton.com.au

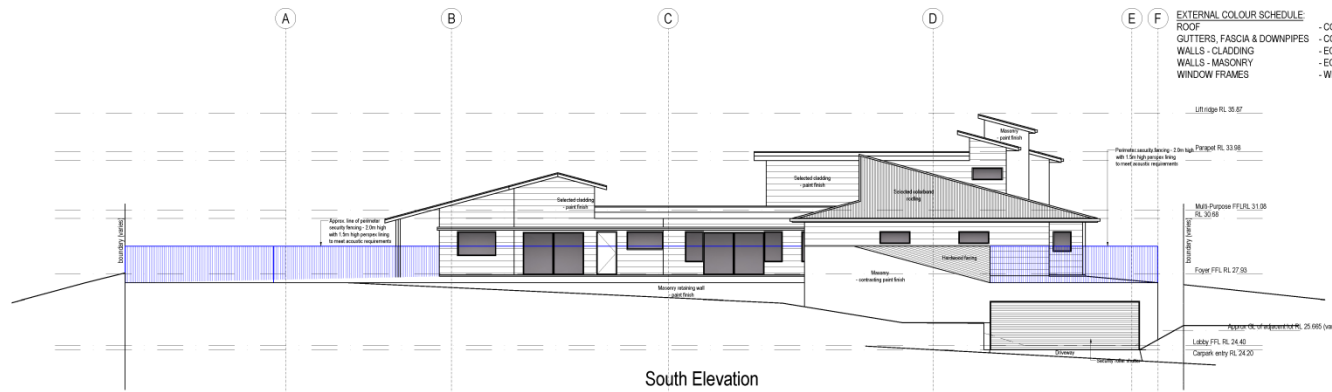
Accredited Building Designer No. 6137
 Chartered member No. 649-00 of
bdm BUILDING DESIGNERS AUSTRALIA NSW



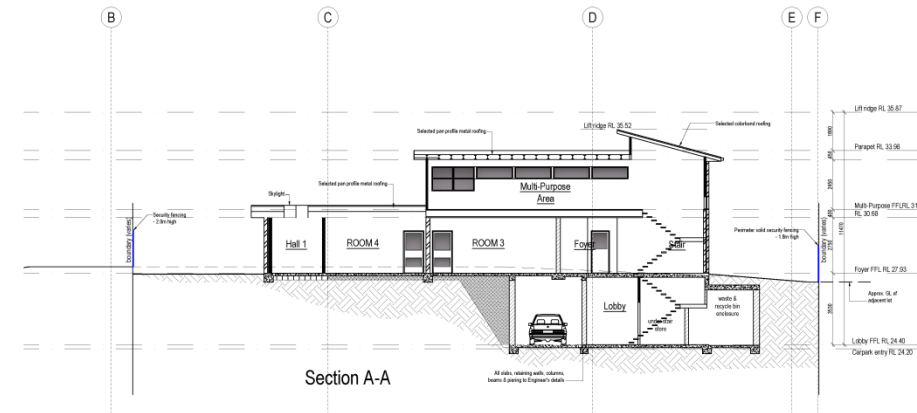
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 2. LEVELS AND GRID LINES SHOWN ARE APPROXIMATE ONLY.

SCALE:
 1:100 @ A1

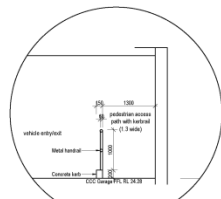
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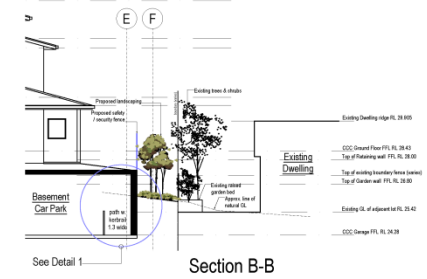
South Elevation



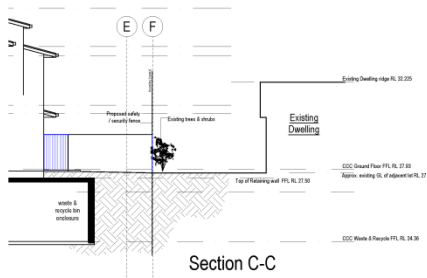
Section A-A



Detail 1
Pedestrian Access Path
(Scale 1:50)



Section B-B



Section C-C

EXTERNAL COLOUR SCHEDULE:
ROOF - COLOURBOND "BASALT"
GUTTERS, FASCIA & DOWNPIPES - COLOURBOND "BASALT"
WALLS - CLADDING - EQUAL TO DULUX "CRUST"
WALLS - MASONRY - EQUAL TO DULUX "WAYWARD GREY"
WINDOW FRAMES - WHITE

CONTRACTOR SHALL VERIFY ALL DIMENSIONS ON SITE PRIOR TO START AND REPORT ANY VARIATIONS TO THE ARCHITECT IMMEDIATELY. ANY DIMENSIONS ARE TO BE REPORTED TO THE ARCHITECT IMMEDIATELY FOR APPROVAL. DIMENSIONS ARE TO BE TAKEN IN ACCORDANCE TO AS/NZS 1546:2011. DIMENSIONS SHALL BE TAKEN TO THE FACE UNLESS OTHERWISE SPECIFIED.

REVISIONS:
THIS DRAWING AND THE INSTRUMENTS TO WHICH IT IS REFERRED ARE SUBJECT TO THE PROVISIONS OF THE INSTRUMENTS TO WHICH IT IS REFERRED. ANY CHANGES TO THIS DRAWING SHALL BE MADE IN ACCORDANCE WITH THE INSTRUMENTS TO WHICH IT IS REFERRED.

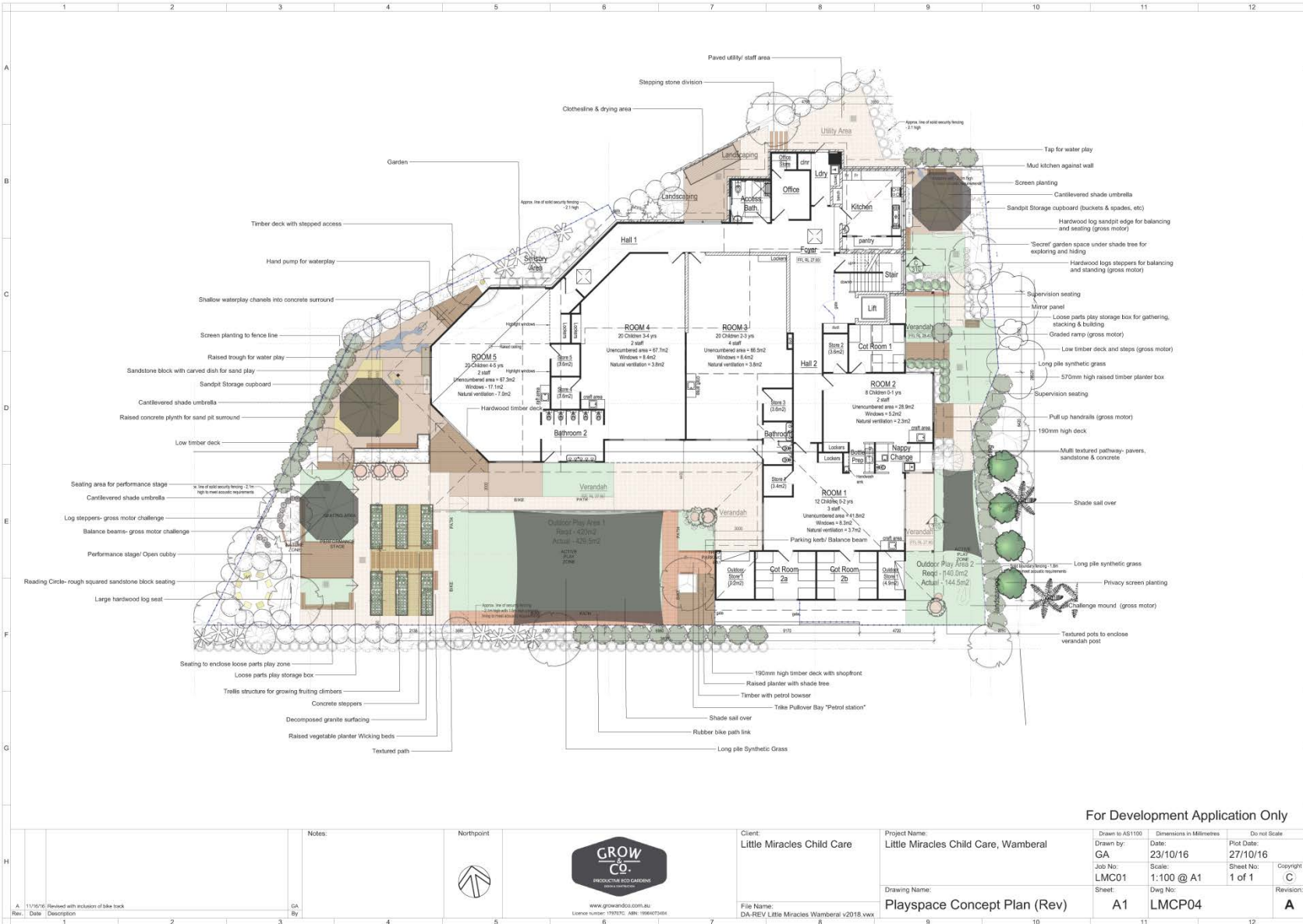
NO.	REVISION	DATE
B	1 Southern facade above vehicular entry altered, colour schedule added	12/4/17
	2 Section B-B added	13/1/17
	3 Relocated, lowered (200mm) external wall altered, lift microduct Section C-C added	21/1/17
	4 Detail 1 added	22/1/18

NOTE: LEVELS INDICATED ARE TO BE TO FINISH AND GROUND LINES SHOWN ARE APPROXIMATE ONLY.

SCALE: 1:100 @ A1, 1:50 @ A1

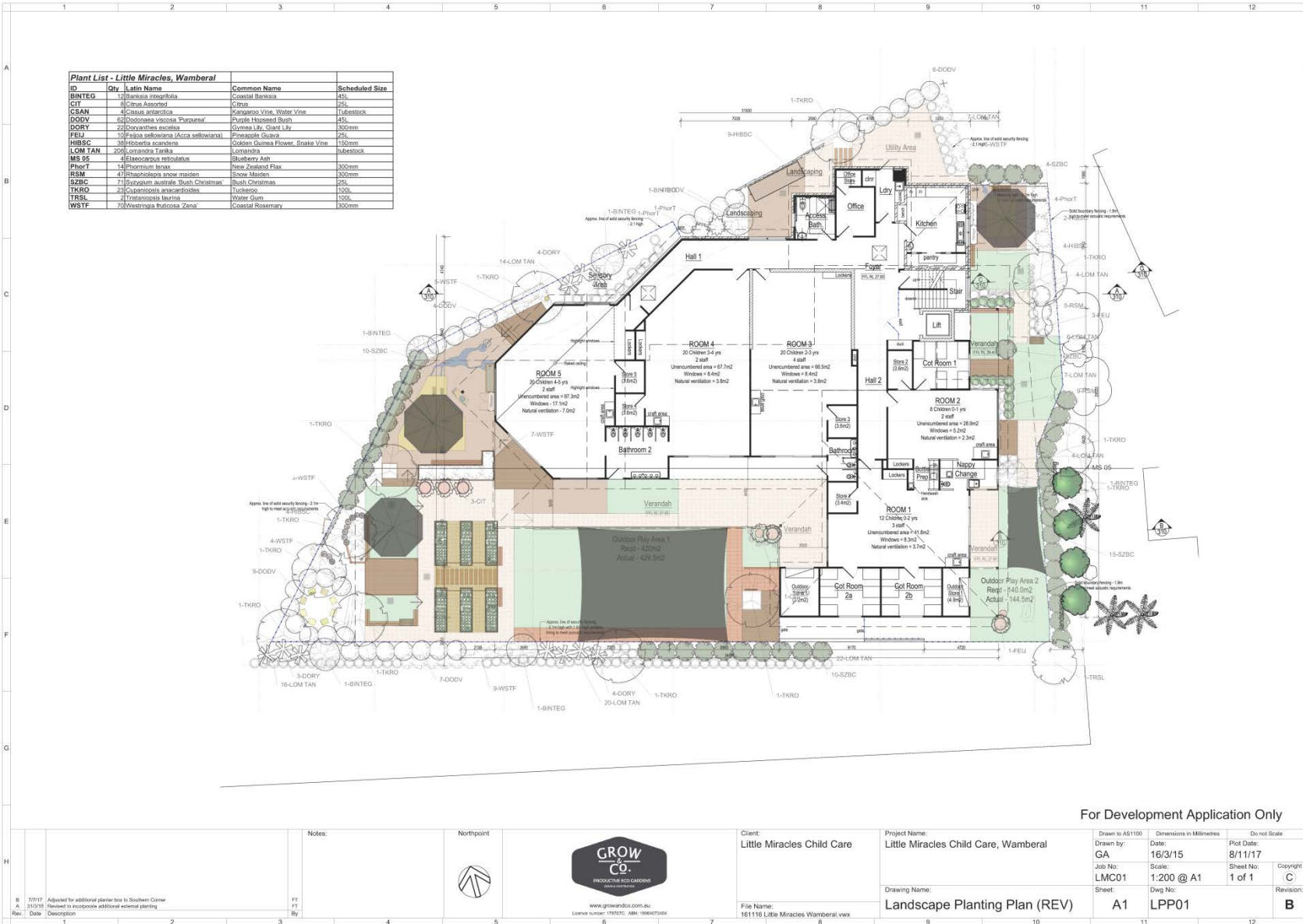
NOT FOR CONSTRUCTION

<p>Scale: 1:100 @ A1, 1:50 @ A1</p> <p>Project no: 310</p> <p>Sheet: B 4</p> <p>Project Name: Elevations Sections</p> <p>Client: Child Care Centre</p> <p>Address: Lot 70 / DP 1210387, 612 The Entrance Road, Wamberal</p> <p>Project: Little Miracles</p> <p>Local Authority: Central Coast Council</p> <p>Project: knight-planning architects sustainable building design</p> <p>Address: PO BOX 394 UMBA BEACH NSW 2237, 358 OCEAN BEACH RD, UMBA BEACH, NSW, ph: 02 6642714, email: km@knightplanning.com.au, www: www.knightplanning.com.au</p> <p>Accredited Building Designer No. 649-00 of bda BUILDING DESIGNERS AUSTRALIA NSW</p>			
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For Development Application Only

Notes:		Northpoint	Client: Little Miracles Child Care	Project Name: Little Miracles Child Care, Wamberal	Drawn to AS1100	Dimensions in Millimetres	Do not Scale									
			File Name: DA-REV Little Miracles Wamberal v2016.vwx	Drawing Name: Playspace Concept Plan (Rev)	Drawn by: GA Date: 23/10/16 Job No: LMC01 Scale: 1:100 @ A1 Sheet: A1	Plot Date: 27/10/16 Sheet No: 1 of 1 Day No: LMC04	Copyright: C Revision: A									
A	11/16/16	Revised with inclusion of bike track	GA	By	1	2	3	4	5	6	7	8	9	10	11	12



For Development Application Only

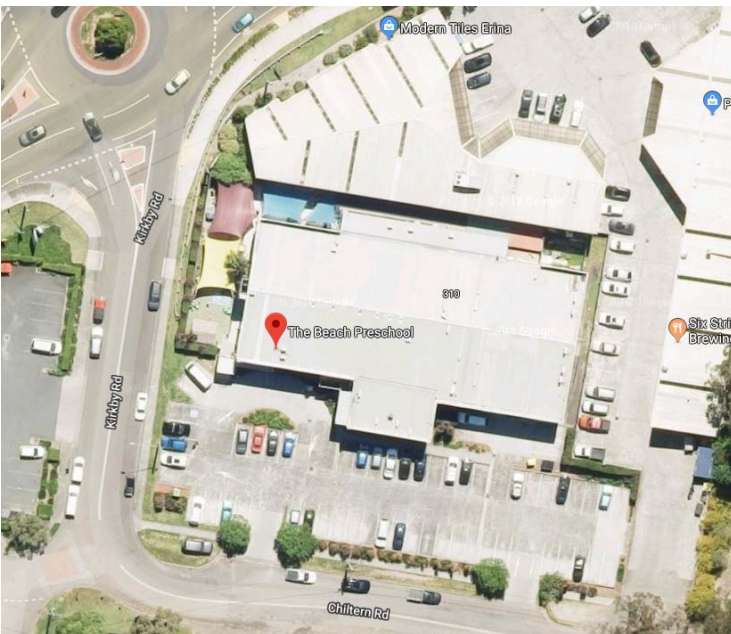
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B 7/7/17 Adjusted for additional planter box to Southern Corner A 31/3/15 Revised to incorporate additional external planting Ref: Date Description	FT FT By		File Name: 181155.Little Miracles Wamberal.vwx	Drawing Name: Landscape Planting Plan (REV)	Lead No:	Scale:	Sheet No:	Copyright
					LMC01	1:200 @ A1	1 of 1	C
					Sheet:	Dwg No:	Revision:	
					A1	LPP01		B

Attachment 4 - Survey of local Childcare Centres

Cnr Central Coast Hwy & Carlton Road, Erina NSW 2250



The Beach Preschool - 310 The Entrance Rd, Erina



Kariong childcare Centre – 46 Langdon Drive, Kariong



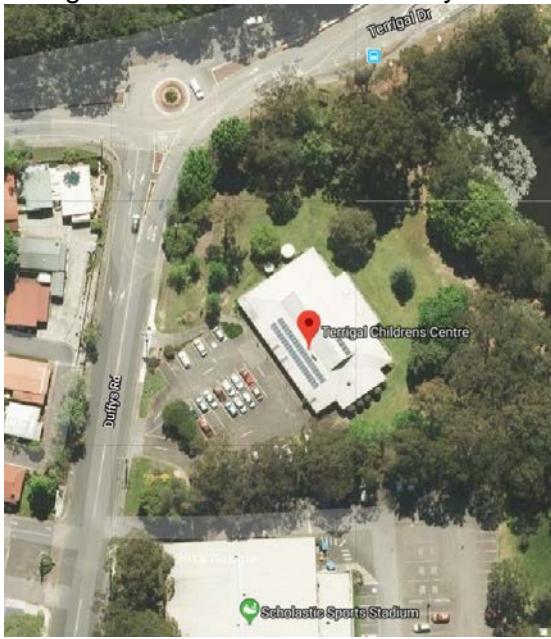
Niagara Park Childcare Centre – 24 Washington Ave



Umina Child Care Centre - 10 Australia Avenue, Umina



Terrigal Children's Centre - 4 Duffys Road, Terrigal



Alterations to Existing Childcare Centre - 20 Kincumber Street KINCUMBER NSW 2251



Child Care Centre - 16/16 Washington Avenue NIAGARA PARK NS



Child Care Centre - 359 Terrigal Drive ERINA NSW 2250





Child Care Centre - 97 Booker Bay Road BOOKER BAY NSW 2257



Child Care Centre - 433 Wards Hill Road EMPIRE BAY NSW 225



Child Care Centre - 586 Tumby Road WAMBERAL NSW 2260



Child Care Centre - 29 Waratah Street, East Gosford (Separate Entry/Exit)



Approved Ground Floor Child Care Centre (basement carpark) - 204 West Street, Umina Beach



Woodport Early learning Centre - 2-6 Woodport close, Green Point (Separate Entry/Exit)



Peninsula Child Care - 30 Edward Street, Woy Woy



Flutterbys Preschool – 68 Newling Street LISAROW (Separate Entry/Exit)



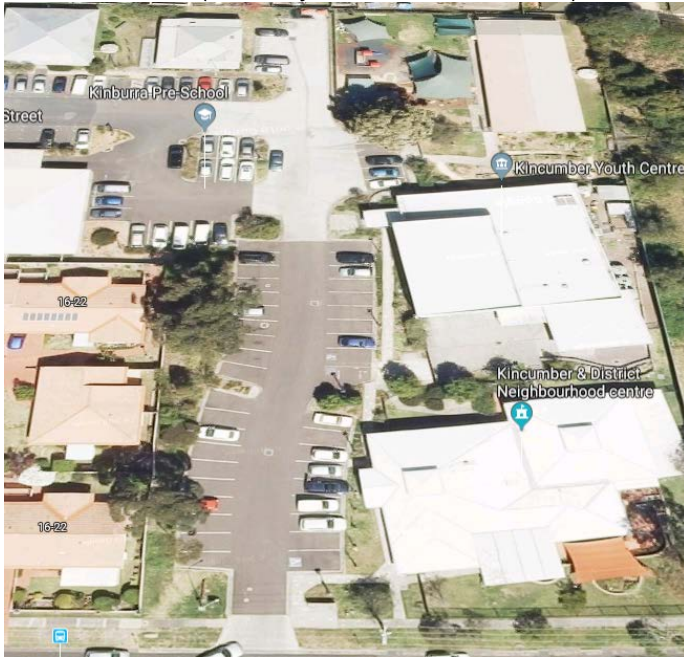
Lighthouse Preschool - 237-245 Ocean View Road ETTALONG BEACH.



15-17 Jessina Street KARIONG (Separate Entry/Exit)

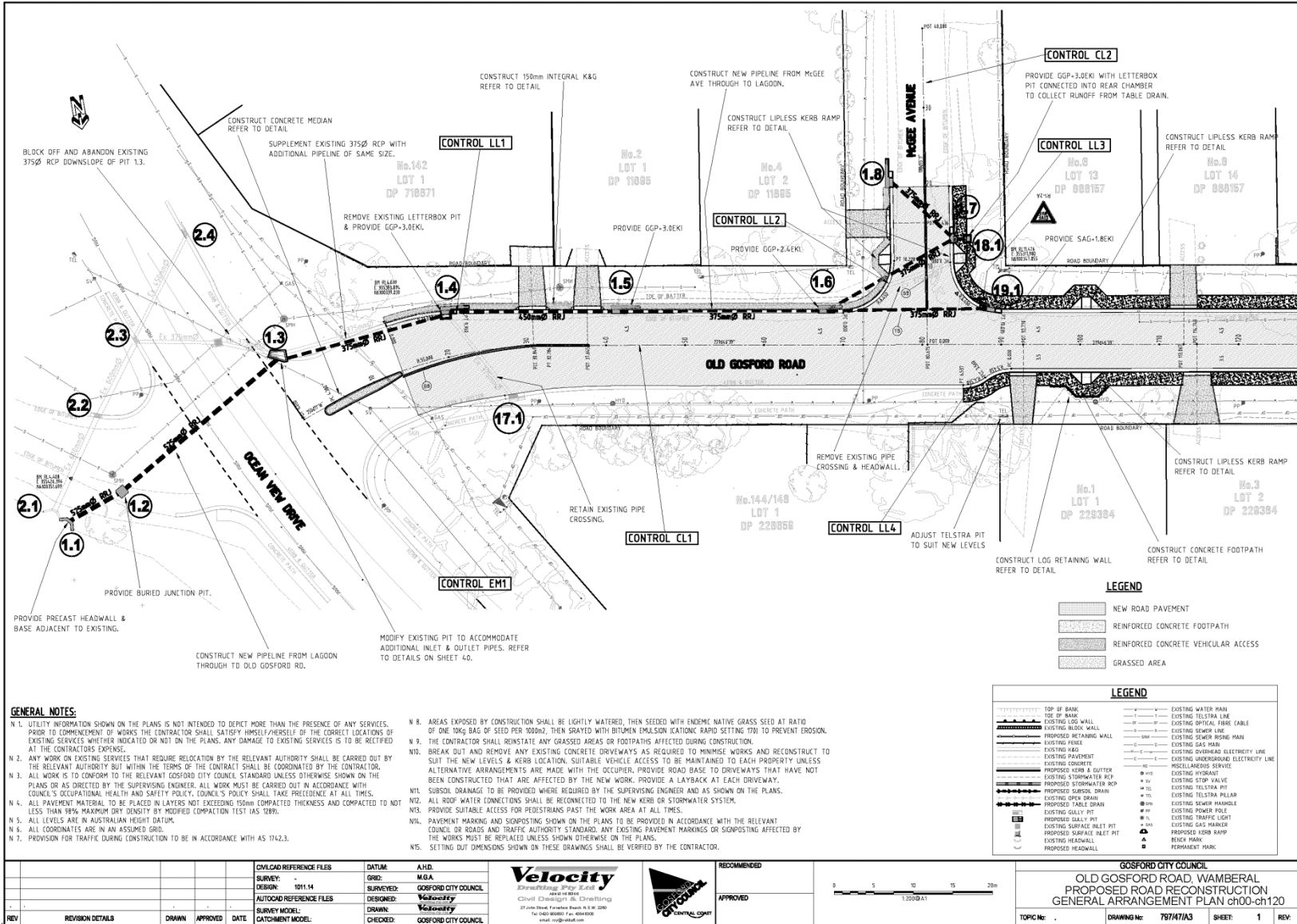


Michael Burns (formerly Kinburra Preschool) - 7/20-22 Kincumber Street KINCUMBER



Alkira Early Learning Centre Preschool - 1 Reads Road, WAMBERAL





GENERAL NOTES

- N 1. UTILITY INFORMATION SHOWN ON THE PLANS IS NOT INTENDED TO DEPICT MORE THAN THE PRESENCE OF ANY SERVICES. PRIOR TO COMMENCEMENT OF WORKS THE CONTRACTOR SHALL SATISFY HIMSELF/HERSELF OF THE CORRECT LOCATIONS OF EXISTING SERVICES WHETHER INDICATED OR NOT ON THE PLANS. ANY DAMAGE TO EXISTING SERVICES IS TO BE RECTIFIED AT THE CONTRACTORS EXPENSE.
- N 2. ANY WORK ON EXISTING SERVICES THAT REQUIRE RELOCATION BY THE RELEVANT AUTHORITY SHALL BE COORDINATED BY THE CONTRACTOR WITHIN THE TERMS OF THE CONTRACT SHALL BE COORDINATED BY THE CONTRACTOR.
- N 3. ALL WORK IS TO CONFORM TO THE RELEVANT GOSFORD CITY COUNCIL STANDARD UNLESS OTHERWISE SHOWN ON THE PLANS OR AS DIRECTED BY THE SUPERVISING ENGINEER. ALL WORK MUST BE CARRIED OUT IN ACCORDANCE WITH COUNCIL'S OCCUPATIONAL HEALTH AND SAFETY POLICY. COUNCIL'S POLICY SHALL TAKE PRECEDENCE AT ALL TIMES.
- N 4. ALL PAVEMENT MATERIAL TO BE PLACED IN LAYERS NOT EXCEEDING 150mm COMPACTED THICKNESS AND COMPACTED TO NOT LESS THAN 98% MAXIMUM DRY DENSITY BY MODIFIED COMPACTION TEST HAS 1089.
- N 5. ALL LEVELS ARE IN AUSTRALIAN HEIGHT DATUM.
- N 6. ALL COORDINATES ARE IN AN ASSUMED GRID.
- N 7. PROVISION FOR TRAFFIC DURING CONSTRUCTION TO BE IN ACCORDANCE WITH AS 1742.3.
- N 8. AREAS EXPOSED BY CONSTRUCTION SHALL BE LIGHTLY WATERED, THEN SEEDED WITH ENDEMIC NATIVE GRASS SEED AT RATIO OF ONE 10KG BAG OF SEED PER 1000m², THEN STRAYED WITH BITUMEN EMULSION (KATIONIC RAPID SETTING 170) TO PREVENT EROSION.
- N 9. THE CONTRACTOR SHALL REINSTATE ANY GRASSED AREAS OR FOOTPATHS AFFECTED DURING CONSTRUCTION.
- N 10. BREAK OUT AND REMOVE ANY EXISTING CONCRETE DRIVEWAYS AS REQUIRED TO MINIMISE WORKS AND RECONSTRUCT TO SUIT THE NEW LEVELS & KERB LOCATION. SUITABLE VEHICLE ACCESS TO BE MAINTAINED TO EACH PROPERTY UNLESS ALTERNATIVE ARRANGEMENTS ARE MADE WITH THE OCCUPIER, PROVIDE A LAYBACK AT EACH DRIVEWAY.
- N 11. SUBSOIL DRAINAGE TO BE PROVIDED WHERE REQUIRED BY THE SUPERVISING ENGINEER AND AS SHOWN ON THE PLANS.
- N 12. ALL ROOF WATER CONNECTIONS SHALL BE RECONNECTED TO THE NEW KERB OR STORMWATER SYSTEM.
- N 13. PROVIDE SUITABLE ACCESS FOR PEDESTRIANS PAST THE WORK AREA AT ALL TIMES.
- N 14. PAVEMENT MARKING AND SIGNPOSTING SHOWN ON THE PLANS TO BE PROVIDED IN ACCORDANCE WITH THE RELEVANT COUNCIL OR ROADS AND TRAFFIC AUTHORITY STANDARD. ANY EXISTING PAVEMENT MARKINGS OR SIGNPOSTING AFFECTED BY THE WORKS MUST BE REPLACED UNLESS SHOWN OTHERWISE ON THE PLANS.
- N 15. SETTING OUT DIMENSIONS SHOWN ON THESE DRAWINGS SHALL BE VERIFIED BY THE CONTRACTOR.

LEGEND

[Symbol]	NEW ROAD PAVEMENT	[Symbol]	EXISTING WATER MAIN
[Symbol]	REINFORCED CONCRETE FOOTPATH	[Symbol]	EXISTING TELSTRA LINE
[Symbol]	REINFORCED CONCRETE VEHICULAR ACCESS	[Symbol]	EXISTING OPTICAL FIBRE CABLE
[Symbol]	GRASSED AREA	[Symbol]	EXISTING SEWER LINE
[Symbol]		[Symbol]	EXISTING SEWER RIGID MAIN
[Symbol]		[Symbol]	EXISTING GAS MAIN
[Symbol]		[Symbol]	EXISTING OVERHEAD ELECTRICITY LINE
[Symbol]		[Symbol]	EXISTING UNDERGROUND ELECTRICITY LINE
[Symbol]		[Symbol]	MISCELLANEOUS SERVICE
[Symbol]		[Symbol]	EXISTING HYDRANT
[Symbol]		[Symbol]	EXISTING STOP VALVE
[Symbol]		[Symbol]	EXISTING TELSTRA PIT
[Symbol]		[Symbol]	EXISTING TELSTRA PILLAR
[Symbol]		[Symbol]	EXISTING SEWER MANHOLE
[Symbol]		[Symbol]	EXISTING POWER POLE
[Symbol]		[Symbol]	EXISTING TRAFFIC LIGHT
[Symbol]		[Symbol]	EXISTING GAS MANDER
[Symbol]		[Symbol]	PROPOSED KERB RAMP
[Symbol]		[Symbol]	BENCH MARK
[Symbol]		[Symbol]	PERMANENT MARK

CIVILCAD REFERENCE FILES	DATUM:	A.H.D.
SURVEY:	GRID:	MGA
DESIGN:	SURVEYED:	GOSFORD CITY COUNCIL
AUTOCAD REFERENCE FILES	DESIGNED:	Velocity
SURVEY MODEL:	DRAWN:	Velocity
DATCHMENT MODEL:	CHECKED:	GOSFORD CITY COUNCIL

Velocity
 27 JANA STREET, FORTITUDE BEACH, N.S.W. 2060
 CIVIL DESIGN & DRAFTING
 Tel: (02) 9550 6000 Fax: (02) 9550 6001
 Email: info@velocity.com.au

Velocity
 CIVIL CAD
 CONTROL POINT

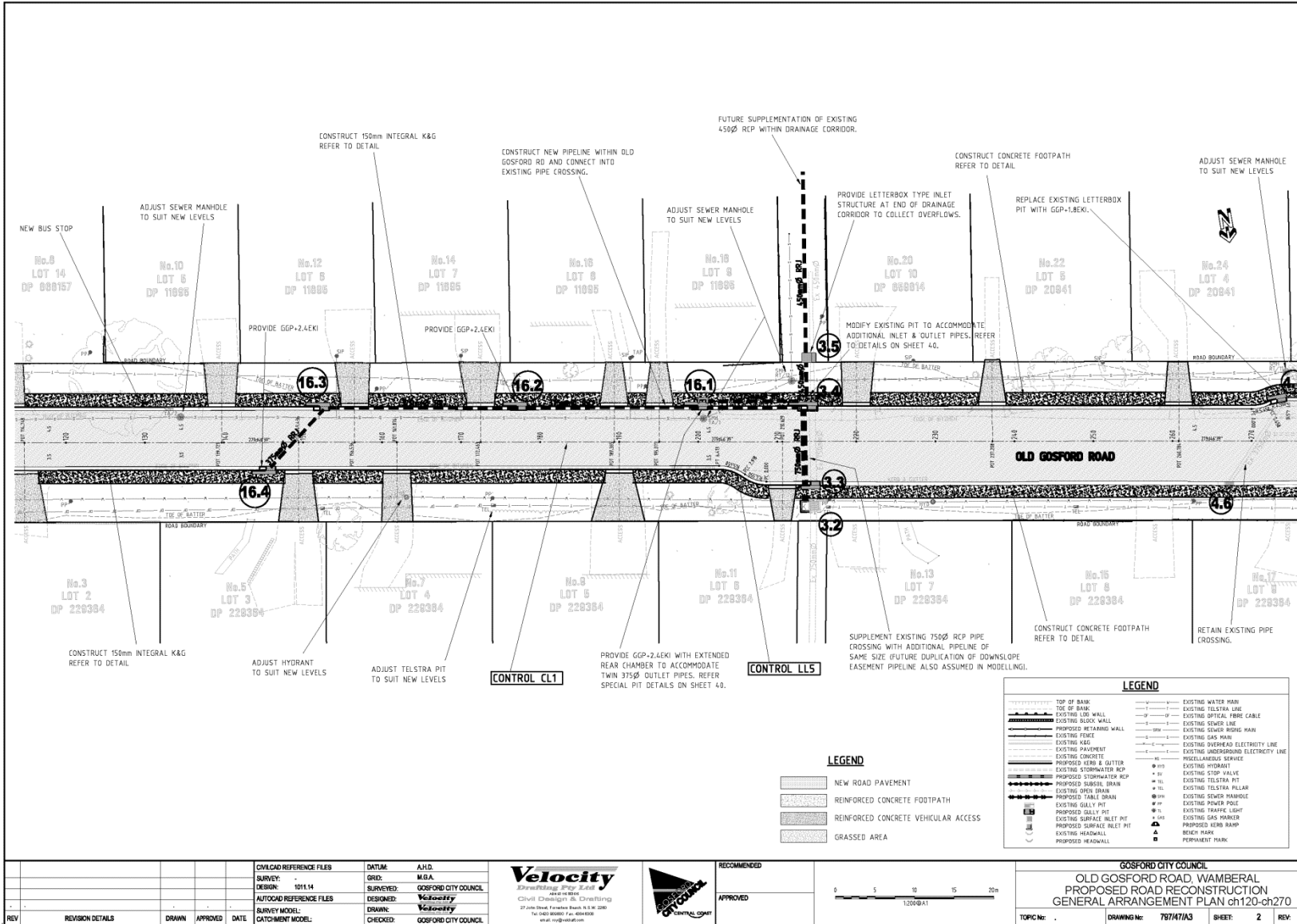
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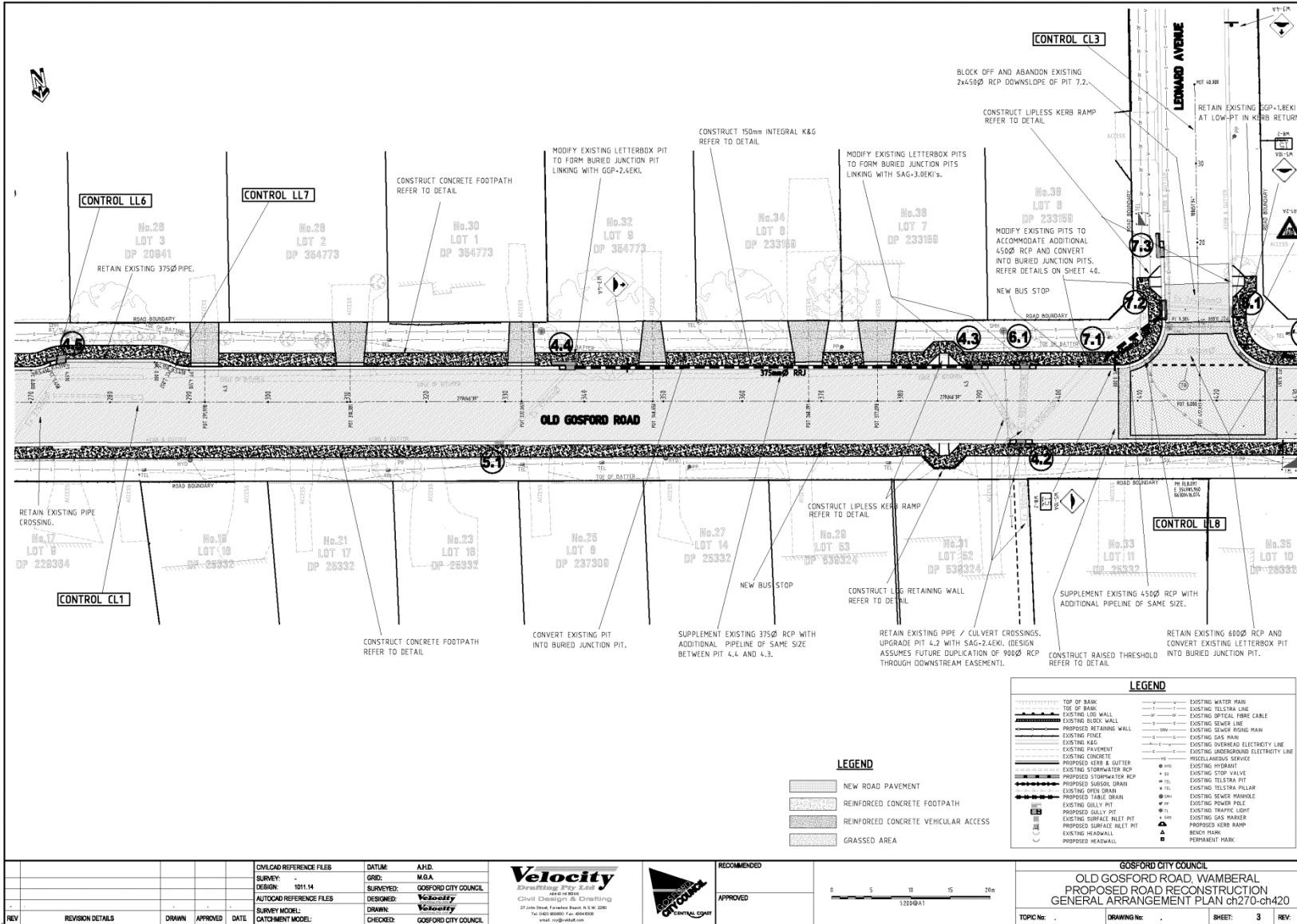
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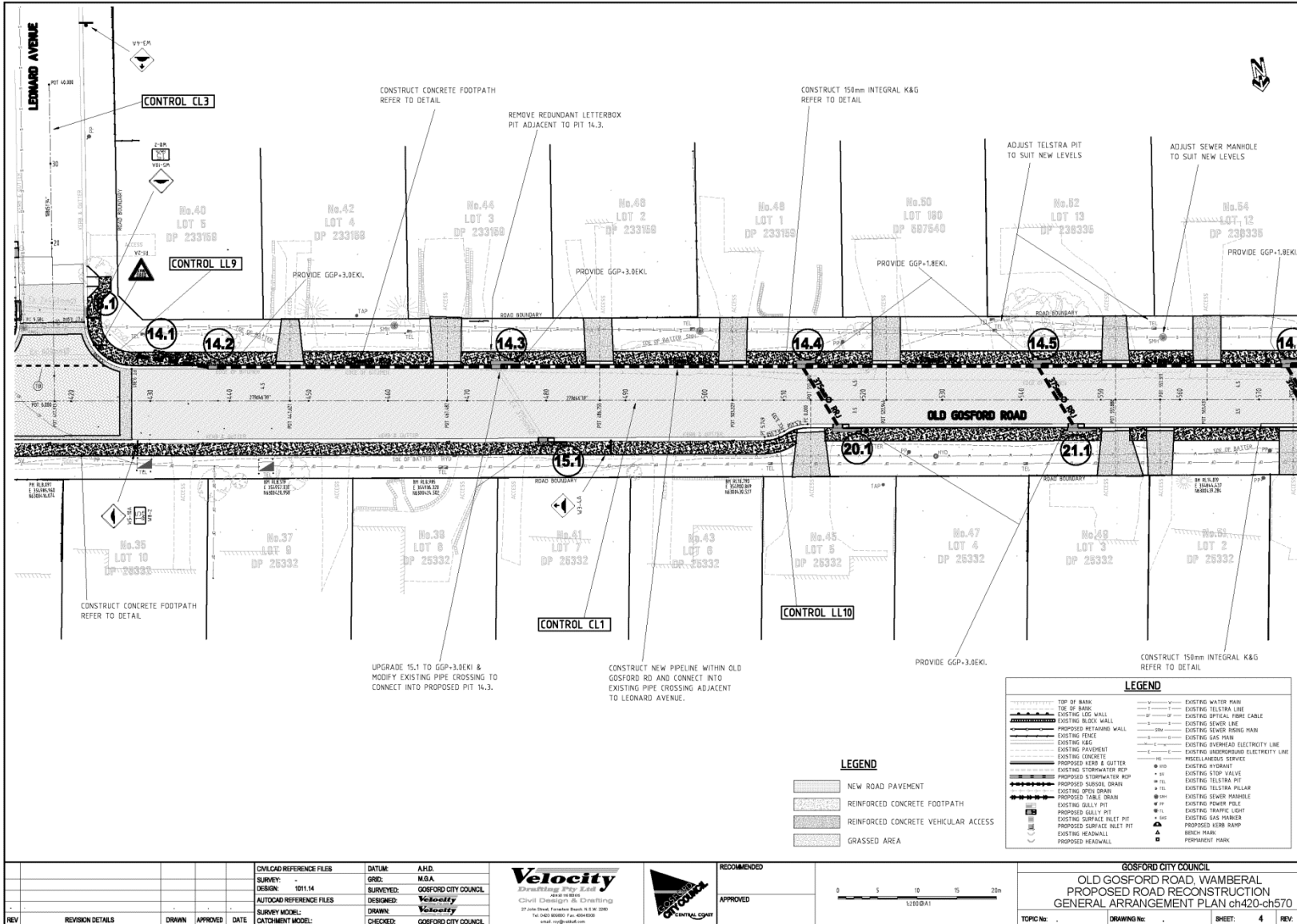
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GOSFORD CITY COUNCIL
 OLD GOSFORD ROAD, WAMBERAL
 PROPOSED ROAD RECONSTRUCTION
 GENERAL ARRANGEMENT PLAN ch00-ch120

TOPIC No: . DRAWING No: 797/47/A3 SHEET: 1 REV: .







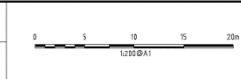
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SURVEY: 1011.14	GRID: MGA
AUTOCAD REFERENCE FILES	SURVEYED: GOSFORD CITY COUNCIL
SURVEY MODEL:	DESIGNED: Velocity
CATCHMENT MODEL:	DRAWN: Velocity
	CHECKED: GOSFORD CITY COUNCIL

Velocity
Engineering Pty Ltd
 Civil Design & Drafting
 27 JAMES STREET, FRENCH BAY, N.S.W. 2050
 Tel: (02) 9550 6100 Fax: (02) 9550 6101
 email: info@velocity.com.au

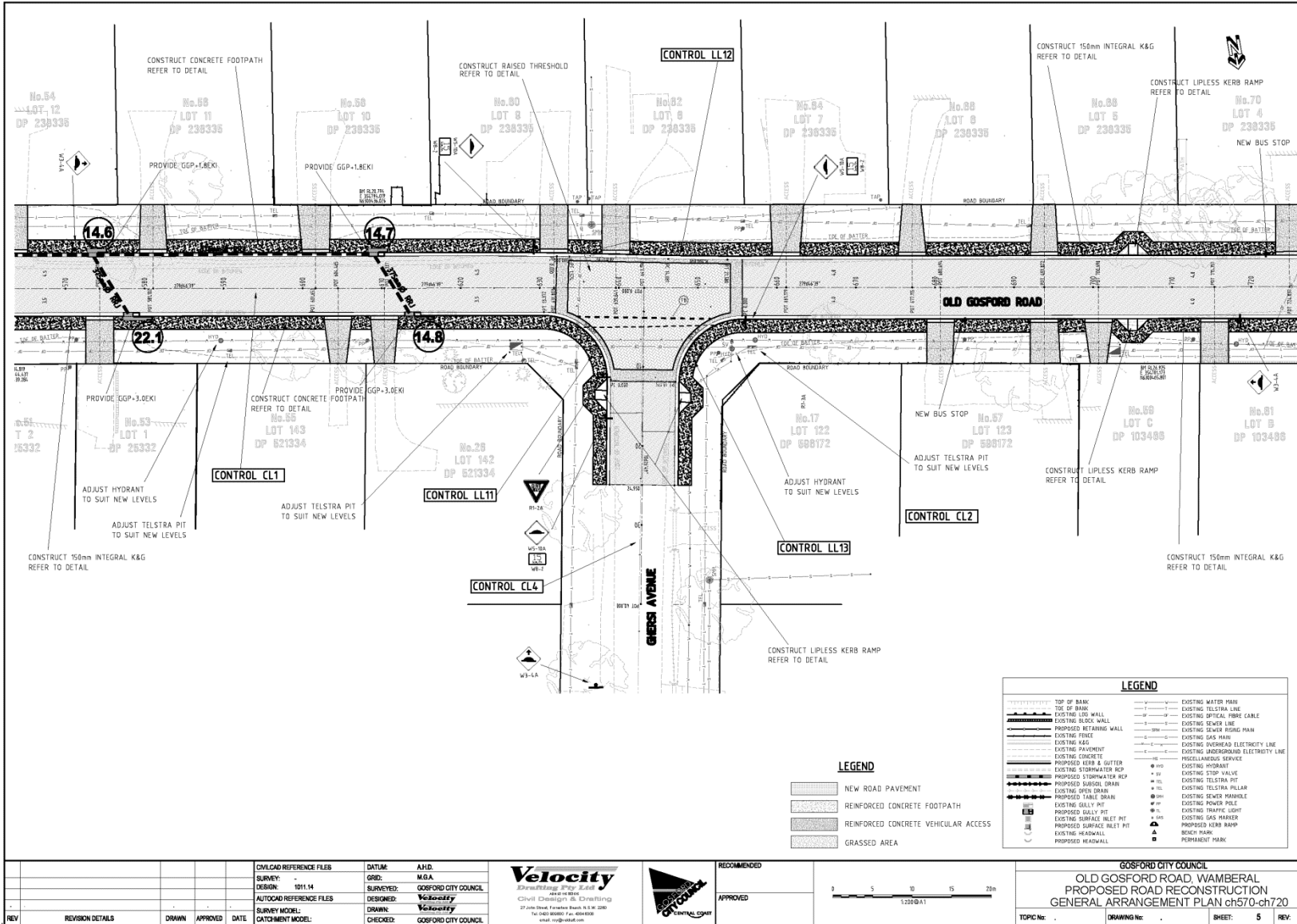


RECOMMENDED
 APPROVED



GOSFORD CITY COUNCIL
OLD GOSFORD ROAD, WAMBERAL
PROPOSED ROAD RECONSTRUCTION
GENERAL ARRANGEMENT PLAN gh420-gh570

TOPIC No: DRAWING No: SHEET: **4** REV:



LEGEND

	TOP OF BANK		EXISTING WATER MAIN
	TOP OF BANK		EXISTING TELSTRA LINE
	EXISTING LID WALL		EXISTING OPTICAL FIBRE CABLE
	EXISTING BLOCK WALL		EXISTING SEWER LINE
	PROPOSED RETAINING WALL		EXISTING SEWER RISING MAIN
	EXISTING PIPE		EXISTING GAS MAIN
	EXISTING K&G		EXISTING OVERHEAD ELECTRICITY LINE
	EXISTING PAVEMENT		EXISTING UNDERGROUND ELECTRICITY LINE
	EXISTING CONCRETE		MISCELLANEOUS SERVICE
	PROPOSED KERB & GUTTER		EXISTING HYDRANT
	EXISTING STORMWATER RCP		EXISTING STOP VALVE
	PROPOSED STORMWATER RCP		EXISTING TELSTRA PIT
	PROPOSED SIPHON DRAIN		EXISTING TELSTRA PILE
	EXISTING OPEN DRAIN		EXISTING SEWER MANHOLE
	PROPOSED TRENCH DRAIN		EXISTING POWER POLE
	EXISTING GULLY PIT		EXISTING TRAFFIC LIGHT
	PROPOSED GULLY PIT		EXISTING GAS MARKER
	EXISTING SURFACE INLET PIT		PROPOSED KERB RAMP
	PROPOSED SURFACE INLET PIT		BEACH PARK
	EXISTING HEADWALL		PERMANENT PARK
	PROPOSED HEADWALL		

LEGEND

	NEW ROAD PAVEMENT
	REINFORCED CONCRETE FOOTPATH
	REINFORCED CONCRETE VEHICULAR ACCESS
	GRASSED AREA

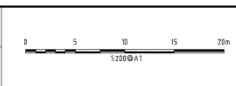
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DESIGN:	SURVEYED:	GOSFORD CITY COUNCIL
AUTOCAD REFERENCE FILES	DESIGNED:	Velocity
SURVEY MODEL:	DRAWN:	Velocity
CATCHMENT MODEL:	CHECKED:	GOSFORD CITY COUNCIL

Velocity
Shaping the Future
 27 JANA STREET, FORTITUDE BRANCH, R.D. 3, W. 2060
 Tel: (08) 8500 6000 Fax: (08) 8500 6000
 email: info@velocity.com.au

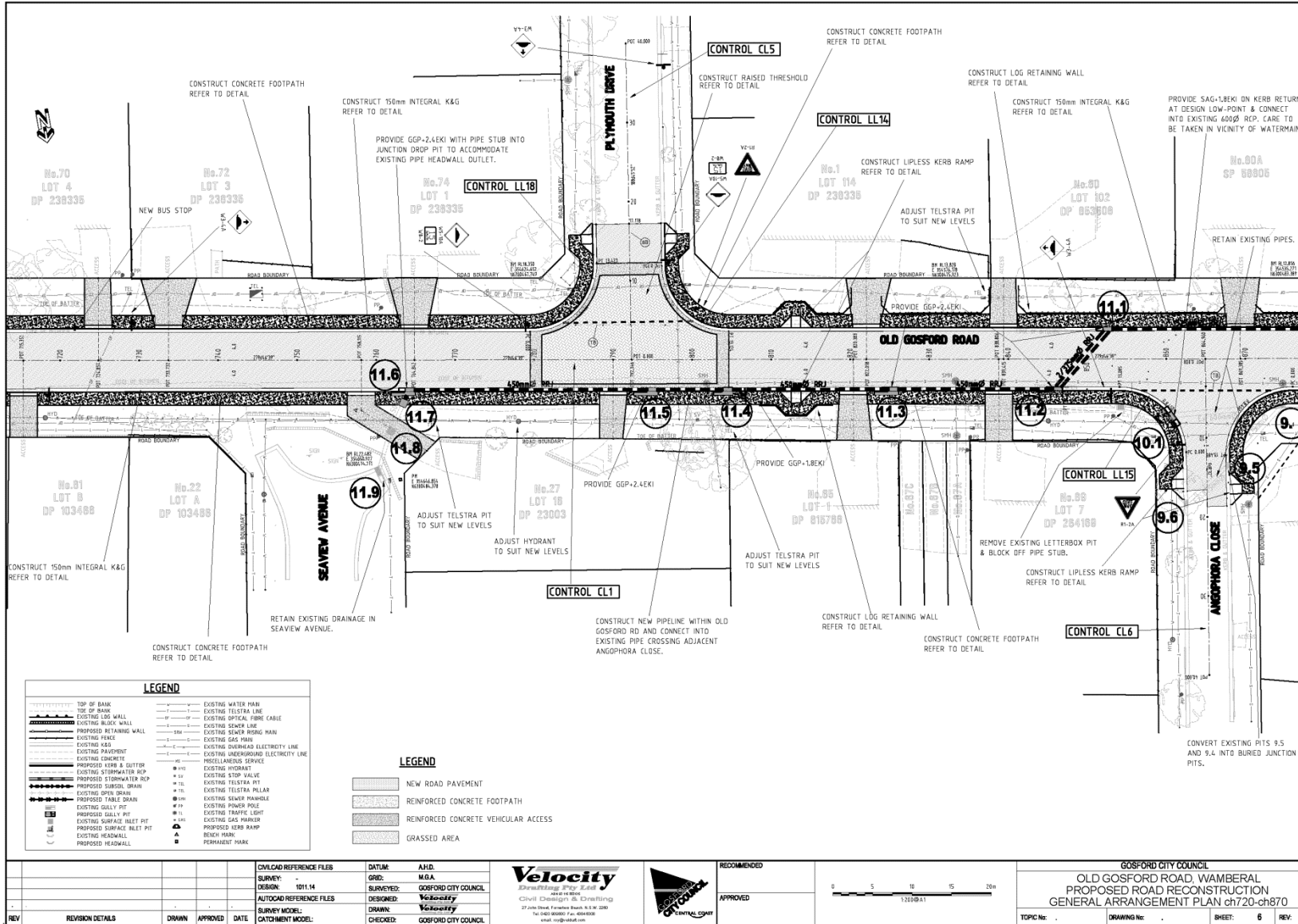


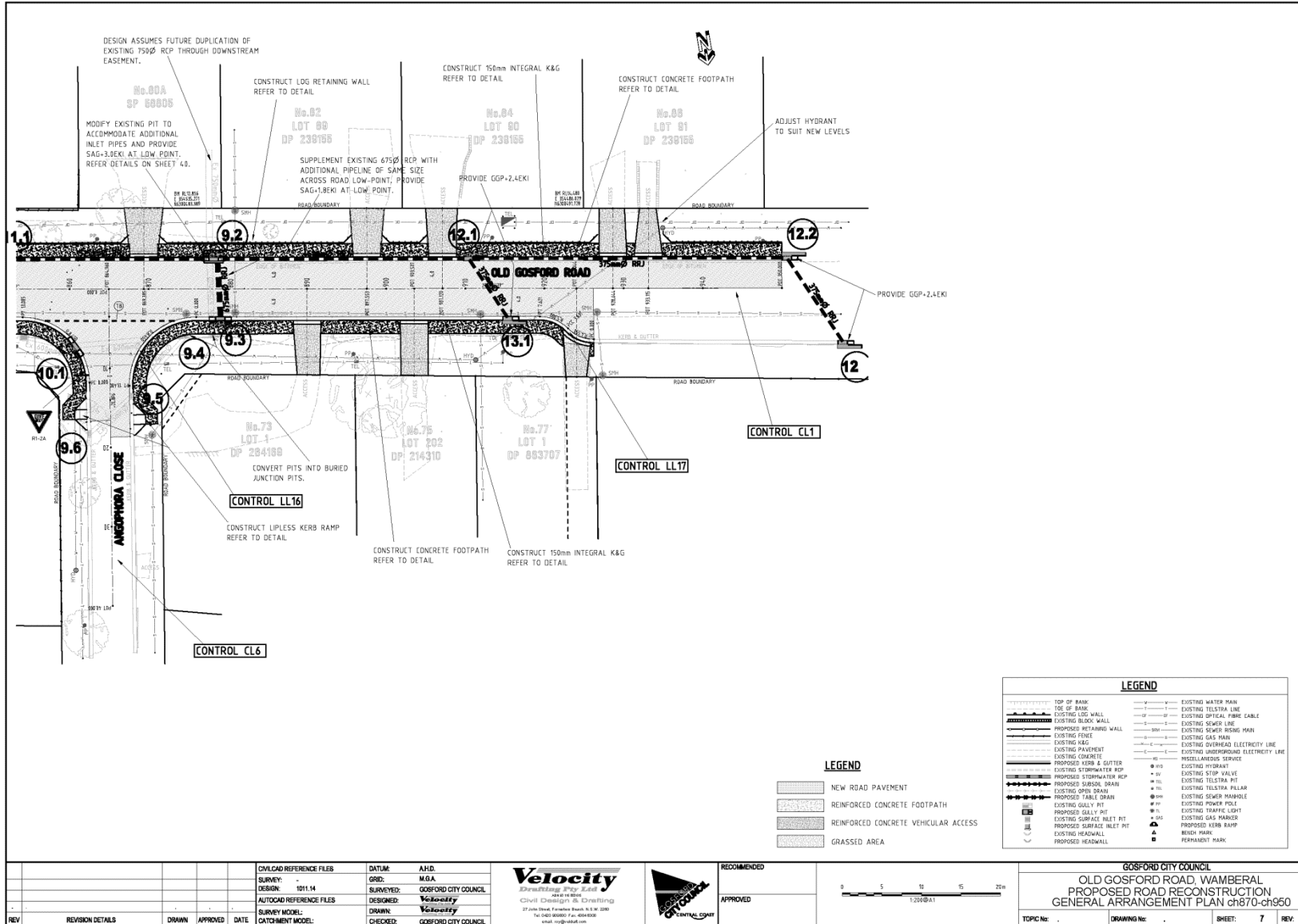
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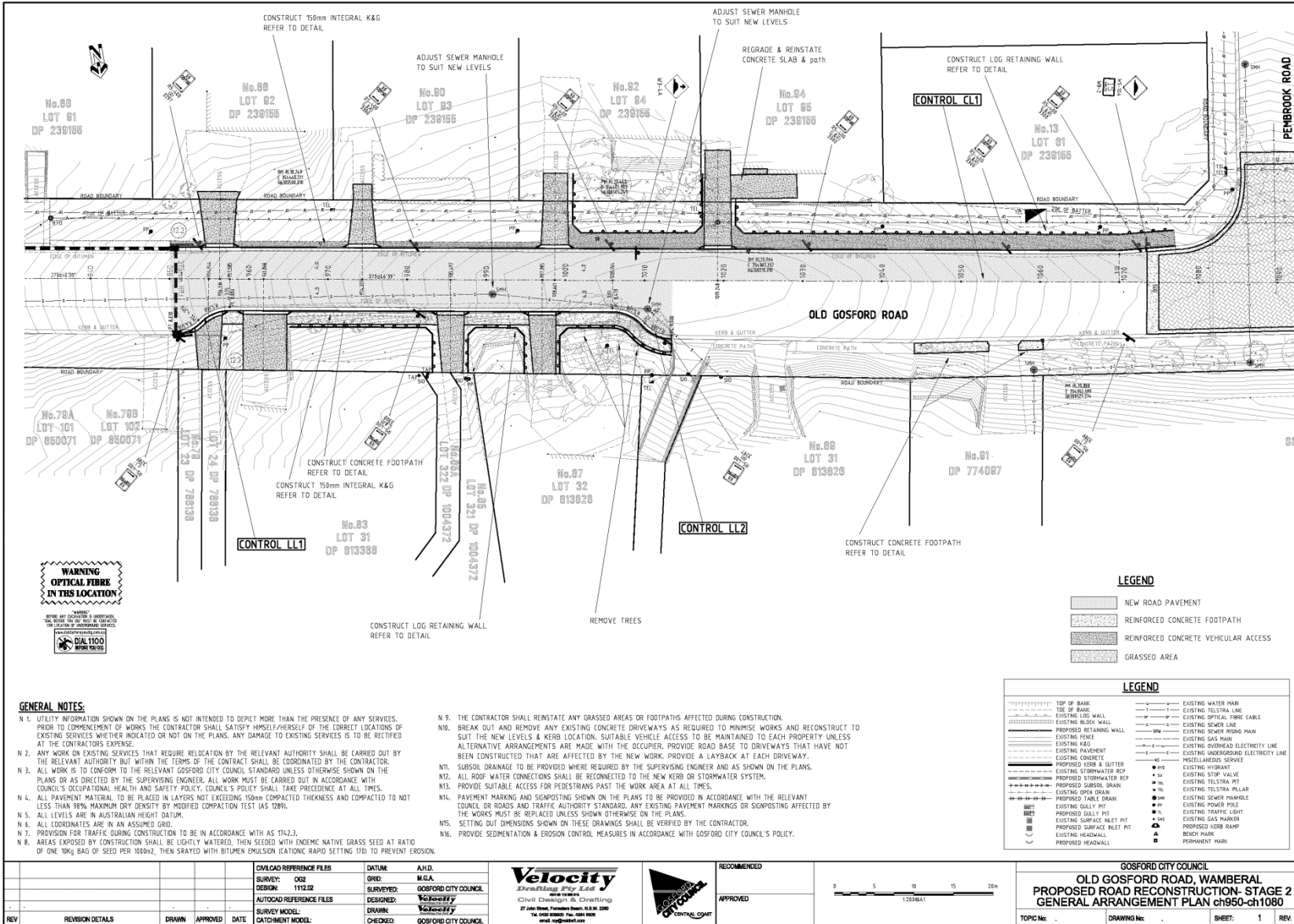


GOSFORD CITY COUNCIL
 OLD GOSFORD ROAD, WAMBERAL
 PROPOSED ROAD RECONSTRUCTION
 GENERAL ARRANGEMENT PLAN ch570-ch720

TOPIC No: DRAWING No: SHEET: 5 REV:







WARNING
OPTICAL FIBRE
IN THIS LOCATION

BEFORE ANY WORK IS UNDERTAKEN, YOU MUST CHECK THE LOCATION OF UNDERGROUND SERVICES.
DIAL 1100
FOR MORE INFORMATION

GENERAL NOTES:

- N 1. UTILITY INFORMATION SHOWN ON THE PLANS IS NOT INTENDED TO DEPICT MORE THAN THE PRESENCE OF ANY SERVICES. PRIOR TO COMMENCEMENT OF WORKS THE CONTRACTOR SHALL SATISFY HIMSELF/HERSELF OF THE CORRECT LOCATIONS OF EXISTING SERVICES WHETHER INDICATED OR NOT ON THE PLANS. ANY DAMAGE TO EXISTING SERVICES IS TO BE RECTIFIED AT THE CONTRACTORS EXPENSE.
- N 2. ANY WORK ON EXISTING SERVICES THAT REQUIRE RELOCATION BY THE RELEVANT AUTHORITY SHALL BE CARRIED OUT BY THE RELEVANT AUTHORITY BUT WITHIN THE TERMS OF THE CONTRACT SHALL BE COORDINATED BY THE CONTRACTOR.
- N 3. ALL WORK IS TO CONFORM TO THE RELEVANT GOSFORD CITY COUNCIL STANDARD UNLESS OTHERWISE SHOWN ON THE PLANS OR AS DIRECTED BY THE SUPERVISING ENGINEER. ALL WORK MUST BE CARRIED OUT IN ACCORDANCE WITH COUNCIL'S OCCUPATIONAL HEALTH AND SAFETY POLICY. COUNCIL'S POLICY SHALL TAKE PRECEDENCE AT ALL TIMES.
- N 4. ALL PAVEMENT MATERIAL TO BE PLACED IN LAYERS NOT EXCEEDING 150mm COMPACTED THICKNESS AND COMPACTED TO NOT LESS THAN 98% MAXIMUM DRY DENSITY BY MODIFIED COMPACTION TEST (AS 1091).
- N 5. ALL LEVELS ARE IN AUSTRALIAN HEIGHT DATUM.
- N 6. ALL COORDINATES ARE IN AN ASSUMED GRID.
- N 7. PROVISION FOR TRAFFIC DURING CONSTRUCTION TO BE IN ACCORDANCE WITH AS 1742.3.
- N 8. AREAS EXPOSED BY CONSTRUCTION SHALL BE LIGHTLY WATERED, THEN SEEDED WITH ENDEMIC NATIVE GRASS SEED AT RATIO OF ONE 10kg BAG OF SEED PER 100sqm, THEN SPRAYED WITH BITUMEN EMULSION (CATIONIC RAPID SETTING 170) TO PREVENT EROSION.

- N 9. THE CONTRACTOR SHALL REINSTATE ANY GRASSED AREAS OR FOOTPATHS AFFECTED DURING CONSTRUCTION.
- N 10. BREAK OUT AND REMOVE ANY EXISTING CONCRETE DRIVEWAYS AS REQUIRED TO MINIMISE WORKS AND RECONSTRUCT TO SUIT THE NEW LEVELS & KERB LOCATION. SUITABLE VEHICLE ACCESS TO BE MAINTAINED TO EACH PROPERTY UNLESS ALTERNATIVE ARRANGEMENTS ARE MADE WITH THE OCCUPYER. PROVIDE ROAD BASE TO DRIVEWAYS THAT HAVE NOT BEEN CONSTRUCTED THAT ARE AFFECTED BY THE NEW WORK. PROVIDE A LAYBACK AT EACH DRIVEWAY.
- N 11. SUBSOIL DRAINAGE TO BE PROVIDED WHERE REQUIRED BY THE SUPERVISING ENGINEER AND AS SHOWN ON THE PLANS.
- N 12. ALL ROOF WATER CONNECTIONS SHALL BE RECONNECTED TO THE NEW KERB OR STORMWATER SYSTEM.
- N 13. PROVIDE SUITABLE ACCESS FOR PEDESTRIANS PAST THE WORK AREA AT ALL TIMES.
- N 14. PAVEMENT MARKING AND SIGNPOSTING SHOWN ON THE PLANS TO BE PROVIDED IN ACCORDANCE WITH THE RELEVANT COUNCIL OR ROADS AND TRAFFIC AUTHORITY STANDARD. ANY EXISTING PAVEMENT MARKINGS OR SIGNPOSTING AFFECTED BY THE WORKS MUST BE REPLACED UNLESS SHOWN OTHERWISE ON THE PLANS.
- N 15. SETTING OUT DIMENSIONS SHOWN ON THESE DRAWINGS SHALL BE VERIFIED BY THE CONTRACTOR.
- N 16. PROVIDE SEDIMENTATION & EROSION CONTROL MEASURES IN ACCORDANCE WITH GOSFORD CITY COUNCIL'S POLICY.

LEGEND

- [Pattern] NEW ROAD PAVEMENT
- [Pattern] REINFORCED CONCRETE FOOTPATH
- [Pattern] REINFORCED CONCRETE VEHICULAR ACCESS
- [Pattern] GRASSED AREA

LEGEND

- [Symbol] TOP OF BANK
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- [Symbol] EXISTING SEWER LINE
- [Symbol] EXISTING GAS MAIN
- [Symbol] EXISTING UNDERGROUND ELECTRICITY LINE
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- [Symbol] EXISTING SEWER MANHOLE
- [Symbol] EXISTING POWER POLE
- [Symbol] EXISTING TRAFFIC LIGHT
- [Symbol] EXISTING GAS MANHOLE
- [Symbol] PROPOSED KERB RAMP
- [Symbol] PERMANENT MARK

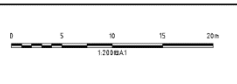
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DESIGN: 1112.02	SURVEYED:	GOSFORD CITY COUNCIL
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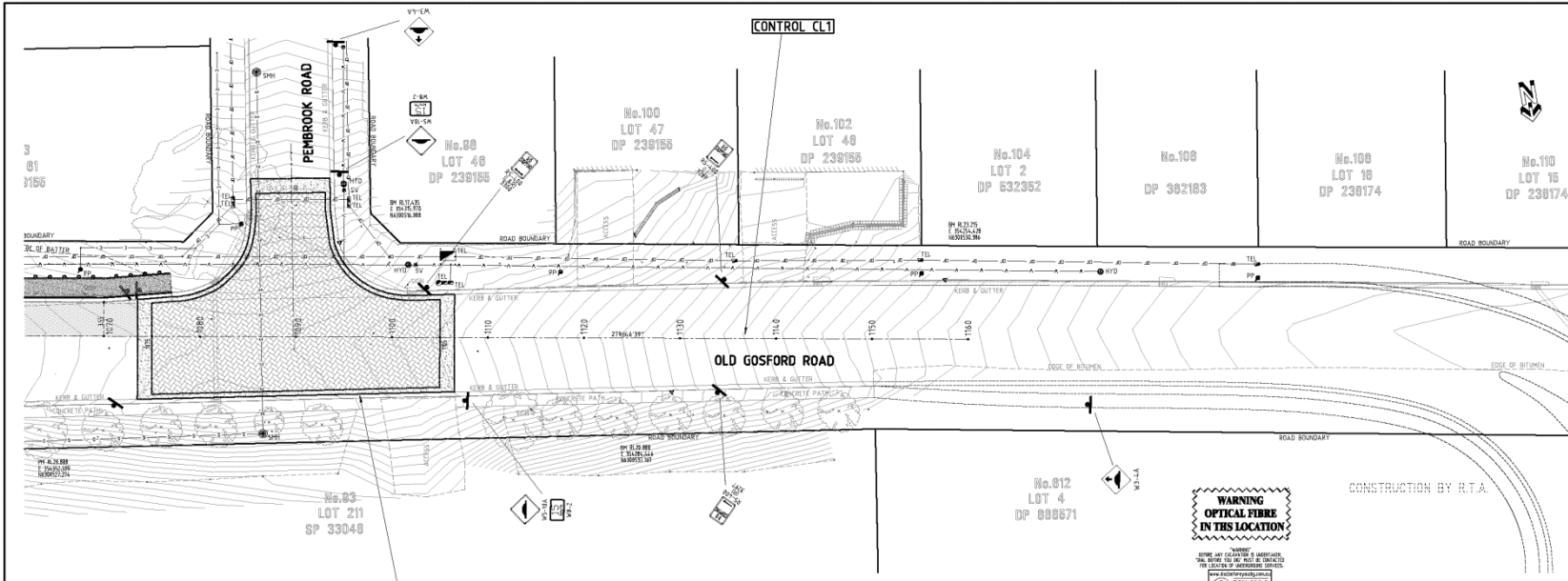


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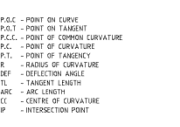
GOSFORD CITY COUNCIL
**OLD GOSFORD ROAD, WAMBERAL
PROPOSED ROAD RECONSTRUCTION- STAGE 2
GENERAL ARRANGEMENT PLAN ch950-ch1080**

TOPIC No: DRAWING No: SHEET: 1 REV:



CONSTRUCTION BY R.T.A.

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SURVEY LEGEND
 P.O.C. - POINT ON CURVE
 P.C. - POINT OF COMMON CURVATURE
 P.T. - POINT OF TANGENCY
 R - RADIUS OF CURVATURE
 L - LENGTH OF CURVE
 DEF - DEFLECTION ANGLE
 TL - TANGENT LENGTH
 ARC - ARC LENGTH
 C - CENTRE OF CURVATURE
 P - INTERSECTION POINT

LEGEND

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GOSFORD CITY COUNCIL
OLD GOSFORD ROAD, WAMBERAL
PROPOSED ROAD RECONSTRUCTION - STAGE 2
GENERAL ARRANGEMENT PLAN ch1080-ch1160

TOPIC No: DRAWING No: SHEET: 2 REV:



3 May 2018

Ms Lee Shearer
Coordinator General for the Central Coast,
PO Box 1148
GOSFORD NSW 2250

Dear Ms Shearer,

Submission of Central Coast Council on Gosford City Urban Design Implementation Framework:

- a) Setting the Scene and Early Recommendations and**
- b) Place Report 1 – The Civic Heart**

Thank you for the opportunity to provide feedback on behalf of Central Coast Council in relation to the following documents:

- Gosford Urban Design and Implementation Framework – Setting the Scene and Early Recommendations (The Background Report)
- Place Report 1 – The Civic Heart (The Report)

The revitalisation of the Gosford City Centre is a key objective of the Central Coast Council. The Council is supportive of the NSW Government's recent focus on supporting the revitalisation of Gosford not only as a City Centre but also as the Regional Capital of the Central Coast. Council had already commenced a number of initiatives consistent with those in the Urban Design Implementation Framework Reports to activate and enhance the Gosford City Centre.

Council supports the NSW Government initiative to engage the NSW Government Architects Office to provide valuable guidance on the future design and development of Gosford.

The Stage 1 reports outline a number of positive design principles and recommendations that promote a place based approach to planning for Gosford, however the Stage 1 Reports do not provide sufficient guidance and detail on matters relating to:

- future design and planning controls;
- movement and access, including parking;
- funding and implementation of key actions; and
- the responsibility for implementation across all levels of Government.

These issues are discussed further in the submission.

Gosford Urban Design Implementation Framework – Setting the Scene and Early Recommendations

The Gosford Urban Design and Implementation Framework – Setting the Scene and Early Recommendations (“Background Report”) provides an overview of previous work undertaken in the city centre, identifies key issues and outlines the UDIF consultation process. The Background Report also discusses initial recommendations responding to issues including place based planning, access and movement, governance and development controls and infrastructure funding.

The implementation of the recommendations has the potential to impact on Council resources significantly, particularly as the role of Council has not been clearly articulated, so it will be expected by the Council to ensure that council has a significant role and say as well as the funding to implement the recommendations.

Place 1 Report - The Civic Heart (Kibble Park)

Kibble Park was chosen for the first stage of the UDIF recommendations as it is geographically located in the centre of the city and is a central meeting place that brings the character of the bushland reserves into the heart of the city.

The potential is to create a centrally located, attractive and connected open space and town square.

There are nine design principles identified for the Civic Heart – Kibble Park (see Figure 1)

- 1 Create a green room – respite for the growing city
- 2 Relocate activity to the edge and activate where people move
- 3 Re-purpose library as a public building – possible Town Hall, meeting place, flexible exhibition space. Investigate upgrade or new café integrated with the park
- 4 Create a civic plaza and connect to Mann Street with active edges. Increase visual connection between Mann Street and the park. Streetscape upgrade to Mann Street
- 5 Establish through connection street at Watt Street
- 6 Align connection to new regional library. Explore secondary connections between the regional library, TAFE, Conservatory of Music and Council
- 7 Investigate the opportunity to retain public parking in part of future development behind an active edge to Henry Parry Drive.
- 8 Connect park to reserves with tree planting along Donnison Street
- 9 Locate development height to minimise overshadowing of Kibble Park



Figure 1 – Design Principles - Kibble Park

The majority of the design principles are consistent with current and previous planning work for the Gosford including Our City, Our Destiny, the Gosford Civic Improvement Plan and more recently the Draft Somersby to Erina Southern Growth Corridor Strategy. In terms of design principles 2 and 7,



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activation of public spaces and the provision of public car parking is highly supported, however additional detail on how these two principles will be implemented is requested.

The Gosford Place Report 1 - The Civic Heart expands on recommendations outlined in the Gosford Urban Design and Implementation Framework – Setting the Scene and Early Recommendations, relating to the simplifying and streamlining Council's Planning Controls by providing seven Built Form Principles for sites surrounding Kibble Park (see Figure 2 below), these are outlined below:

Built Form Principles

Principle 1: 2-3 storey podium height with a fine grain and active frontages to key places and streets

This Principle will result in reduced podium height to the east and south of Kibble Park when compared to Council's current podium height controls of 3-5 storeys. The reduced podium height promotes a pedestrian scale to development which is consistent with the Government Architects approach to fine grain development throughout the City Centre.

For development north and west of Kibble Park further clarification and discussion is required as this approach is inconsistent with the Council's current planning controls.

Principle 2: Ensure ground floor façade to the plaza buildings achieves sun throughout the day

This Principle promotes solar access to development south of Kibble Park along Donnison Street, i.e. the location of Council's future Regional Library. Council supports the design of any public buildings to maximise solar access within the forecourt and ground floor of any future development. The implementation of this Principle needs further collaboration between the Council and Government Architect to ensure that the future regional Library would comply with this principle.

Principle 3: Provide setback to upper levels above podium

This Principle recommends future development above the podium (i.e. tower) to be setback from site boundaries. The Principle provides no numerical information on how this will be achieved, however Council recognises that its existing controls do provide for front, rear and side setbacks and that this information should be considered to inform any new planning controls for the Gosford.



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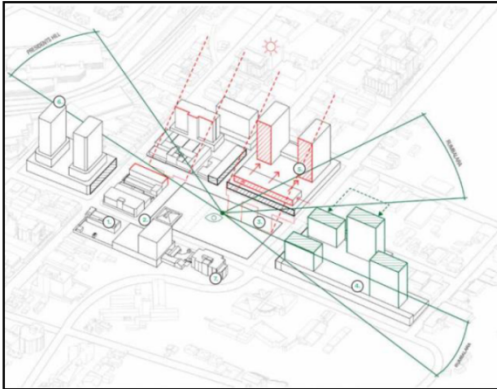
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Principle 4: Slender east/west tower forms, aligned to protect view corridors to Rumbalara ridgelines

This Principle promotes the use of slender tower typologies to maximise the view corridors to Rumbalara. The protection of view corridors is a key design issue that the community and Council supports and is a clear design objective in Council's current planning controls.

The Place Based Report outlines key view corridors that extend across multiple properties, that sit within the view corridor. The development of planning controls for land within the view corridors needs to be further developed to ensure Principle 4 can be achieved. Also further details are required on the planning controls for the Kibbleplex site which shows tower formations that have the potential to restrict views. Again, any inconsistencies with the required view corridors in Council's current planning controls need to be clarified and discussed with Council.



Principle 5: Slender north/south orientated tower forms to allow shadows to move quickly across Kibble Park

Maintaining acceptable and reasonable levels of solar access to Kibble Park is a priority of Council. Again, any inconsistencies with the solar access controls in Council's current planning controls need to be clarified and discussed with Council.

Principle 6: Protect view corridors to Presidents Hill from Kibble Park

This Principle promotes maintaining the view corridors to Presidents Hill. The protection of this view corridor is a key design issue that Council supports and is also a clear design objective in Council's current planning controls.

The implementation of this Principle relies on planning controls which have not been provided in the Stage 1 reports.



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Principle 7: Ensure solar access for 4 hours to 50% of the park between 9am and 3pm at the winter solstice

This Principle recognises that development promoted in Principle 4 and 5 will overshadow Kibble Park. The intent of the Principle is to allow for overshadowing where the impact is measurable and limited to hours where the park is most used.

This principle provides a measurable impact of solar access to Kibble Park. Council seeks further clarification on how this measured impact is consistent with *clause 8.5 of Gosford Local Environmental Plan 2014* to consider "whether the proposed development detrimentally overshadows Kibble Park, William Street Plaza, Burns Park and the waterfront open space adjoining The Broadwater".

Council seeks clarification on the rationale of this measurable standard and how the proposed solar access principle represents best practice and benchmarks against solar access provisions relating to significant open spaces and parks in other regional cities.

Planning Controls - Further Information/Clarification

The Reports provide no detail on any process or proposals to change Council's planning controls. There are a number of variations proposed to current planning controls and Council needs to be confident that it will take retain ownership in the development of any new controls as well as ensuring the community will have opportunity to respond.

It is also requested that DPE clarify whether traffic modelling and feasibility modelling has been undertaken as part of the UDIF process to facilitate any density increases above Council's current planning controls.

Place Making Initiatives

The State and regional level strategies and actions identifies the need to establish a place based strategy that is supported by the functions of place management to ensure the proper coordination and activation of key public domain spaces.

Whilst Council supports this approach there are further matters that require clarification especially in terms of appropriate initial and ongoing funding mechanisms including:

- Place based initiatives for Gosford should be conducted in line with regional place based activities to ensure cohesiveness.
- A Place based approach with a focus on public domain is a positive early recommendation. Clarity of the role and Council's responsibility needs clarity about the role of Place Managers though and where this role sits /who has responsibility. This role has a wide range of possible activities including project delivery, infrastructure coordination, marketing, tourism, business development etc.
- Based on previous experience, expectation setting with the community (local and broader) through a well-designed communication strategy will be needed. The local community have seen a number of plans from both council and state government over the years with little to no action resulting.



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Public Exhibition

When the Stage One UDIF documents were released on 9 April 2018 media releases contained statements which appeared to indicate that the public exhibition period for the Stage One documents closed on 18 June 2018.

On 16 April 2018 the "Go Gosford" website (set up by DPE and NSW Government Architects Office for the UDIF project) was amended to state that the Stage One documents were on exhibition until 27 April 2018. However a media release on the 19 April on the DPE website (extract below) could be interpreted to mean the Stage One documents are on exhibition until 18 June 2018.

"The bold plan for Gosford is articulated in the first of a series of Government Architect recommendations for the revitalisation of the city's centre, on public exhibition until 18 June."
Media Release DPE website 19 April 2018

DPE have clarified DPE that the deadline for comments on the first place-based report on the Gosford's Civic Heart and the UDIF Discussion Paper closed at 5pm on 27 April 2018, however Council has been granted an extension to this date to allow the matter to be reported to Council. DPE have also advised that is intended to have consultation on all the stages of the Government Architects work completed by 18 June 2018.

Due to the confusion within the community as to the close of exhibition date, Council requests that DPE extend the exhibition period for the Stage One documents until 18 June 2018 to enable adequate community input and that this date be confirmed in the relevant media and on the DPE and GoGosford website.

Again, thank you for the opportunity to provide feedback on Stage 1 of the UDIF. Central Coast Council looks forward to working collaboratively with the NSW Government to deliver a vibrant and sustainable Gosford CBD – the Regional Capital of the Central Coast.

Yours faithfully

Brian Glendenning
Acting CEO
Central Coast Council



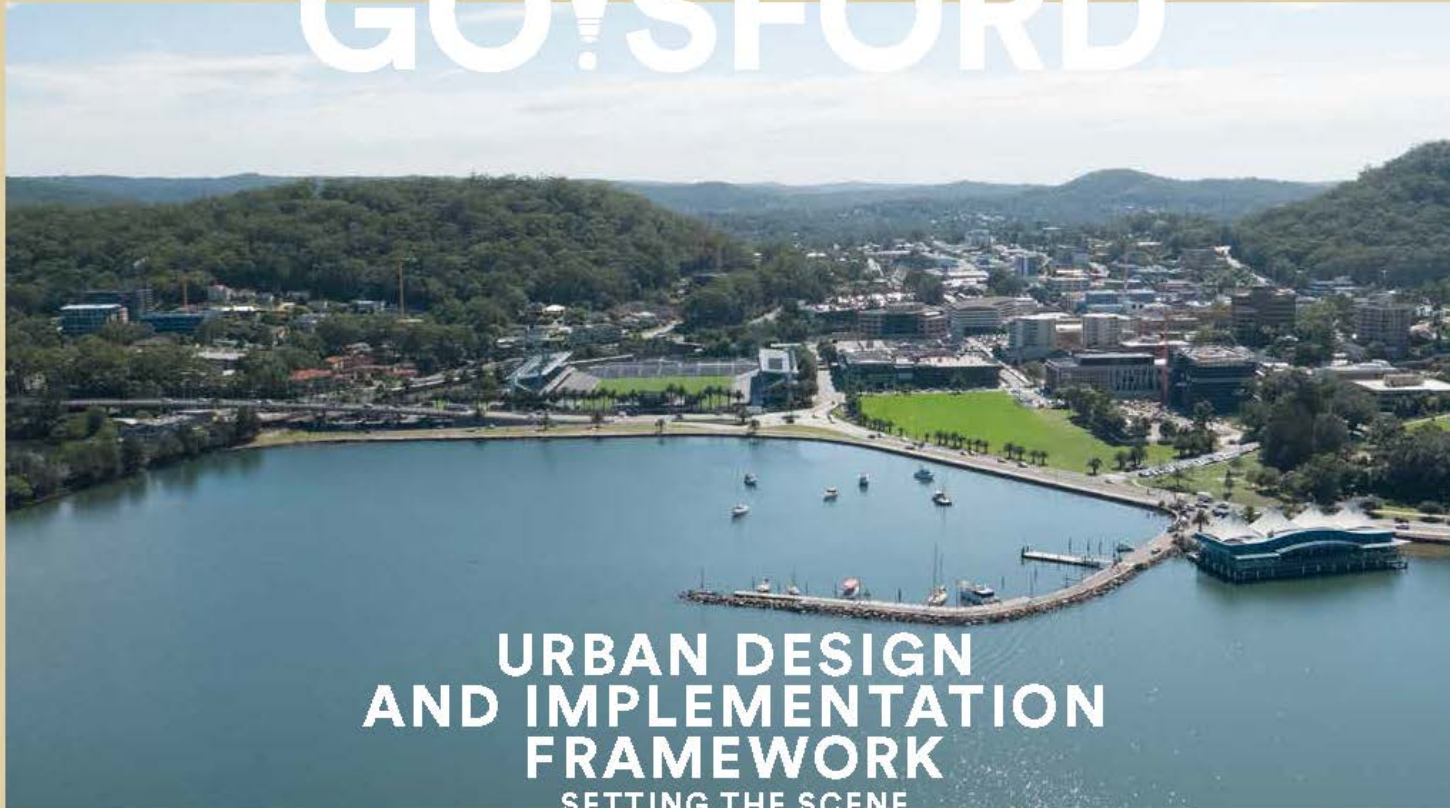
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FOR DISCUSSION
09 APRIL 2018

GO!SFORD



URBAN DESIGN AND IMPLEMENTATION FRAMEWORK

SETTING THE SCENE
AND EARLY RECOMMENDATIONS

CHROFI
TYRRELLSTUDIO

**GOVERNMENT
ARCHITECT
NEW SOUTH WALES**



Design objectives for NSW

Seven objectives define the key considerations in the design of the built environment.



Better fit
contextual, local and of its place



Better performance
sustainable, adaptable and durable



Better for community
inclusive, connected and diverse



Better for people
safe, comfortable and liveable



Better working
functional, efficient and fit for purpose



Better value
creating and adding value



Better look and feel
engaging, inviting and attractive

**G N
A S W**

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Government Architect NSW acknowledges the traditional custodians of the land and pays respect to Elders past, present and future. We honour Australian Aboriginal and Torres Strait Islander peoples' unique cultural and spiritual relationships to place and their rich contribution to our society. To that end, all our work seeks to uphold the idea that if we care for country, it will care for us.

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GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK

Collaborative roles

This Urban Design and Implementation Framework (UDIF) is a multidisciplinary collaboration with many inputs and involvement from other stakeholders.

Government Architect NSW (GANSW) has partnered with CHROFI, TYRRELLSTUDIO with support from HillDPA to deliver the Urban Design and Implementation Framework for Gosford.

GANSW provides independent and professional strategic advice and design leadership supporting the NSW Government in delivering good design, managing risk and fostering innovation to improve social, environmental and economic impact in the built environment.

CHROFI is an Australian award-winning strategic urban design practice with experience in sophisticated place making, strategic urban thinking, urban design studies and sustainability. Their work on numerous projects such as the Manly 2015 master plan, Turramurra Community Hub, and The Goods Line demonstrate their ability to work across high level master planning and strategic coordination to create high quality and unique place outcomes.

TYRRELLSTUDIO is an innovative and multi award winning Australian design firm. They have expertise in urban strategy, public domain design and green infrastructure. Working with GANSW, the Greater Sydney Green Grid, creates an overarching green structure for Greater Sydney, that the future city can be developed around a complete urban green infrastructure network that combines hydrological, ecological and recreational elements.

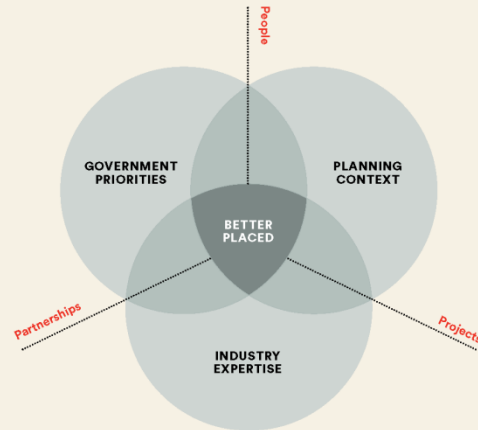
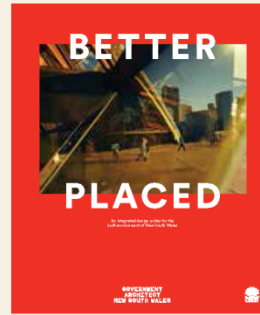
HillDPA is a multidisciplinary consulting firm with a highly skilled and experienced team of land economists, valuers and urban planners. The strengths of their practice are derived from the depth and breadth of their experience, their multidisciplinary expertise and their determination to ensure the highest level of professionalism and integrity.

Better Placed

GANSW's Better Placed advocates for a shared responsibility in realising good design processes and outcomes. The policy sits at the intersection of State-wide priorities, the mechanisms of planning and the expertise of industry and provides a framework for our collective aspirations, needs and expectations.

Better Placed recognises that specialist knowledge can be integrated within the process to create efficiencies and shared knowledge where skills intermesh, joint solutions emerge and perspectives change by working together.

Collaboration towards a shared vision considers a broad understanding of goals and values dependent on high levels of interdisciplinary expertise. By bringing together expertise, better built outcomes for Gosford will be achieved.



GANSW supports the Coordinator General and DPE team in establishing strong partnerships between state government agencies, industry and institutions to build an understanding of what it takes to create great places, and to disseminate new techniques and approaches to ensure Gosford's successful renewal is delivered.

The success of Gosford's revitalisation requires support and continual interaction across design, planning, and development at every scale. A continuing conversation and shared responsibility between the public and private sector in developing public space is paramount.

To ensure an effective process, GANSW assisted communities and experts to understand divergent aspirations and needs, to establish good design practices and how they will deliver benefit to Gosford. Participation is key to effective process and good design outcomes.

Stakeholder and community engagement, at the earliest stages of projects, creates shared ambition and embeds good process as the UDIF develops. This enables more considered and inclusive proposals, better suited to place and people.

The UDIF provides a consistent, forward-looking and long-term vision. This vision needs to be based on a clear understanding of the present situation and the factors that will influence future generations.



Central Coast Coordinator General foreword

As Coordinator General for the Central Coast my key focus is to make our shared vision of the *Central Coast Regional Plan 2036* a reality. A key priority to realising the vision is the revitalisation of Gosford.

It's time for Gosford to fulfil its role as the region's capital. Gosford brings together all the best characteristics of the Central Coast: water, natural beauty and an urban centre.

As a regional capital, Gosford will be the civic and cultural heart of the region, offering core business functions, professional and civic services and a consolidation of government investment. It needs to be a network of well-connected places to create a strong public domain as Gosford evolves.

Gosford has all the ingredients to be a successful regional capital and there is already change in the air. The reimagining of the city as an active urban centre must value its natural setting to become a vibrant destination and attractive regional capital.

The revitalisation of Gosford will entice investment to energise the city, attract new residents, businesses, tourism and culture. Gosford's renewal can help the Central Coast optimise its potential and improve people's quality of life, ensuring that the region is a desirable place to live, work and play.

I have requested the NSW Government Architect input to support the ongoing revitalisation of Gosford and to consult with the Central Coast community while doing this work.

The need for an Urban Design and Implementation Framework has arisen from the growing consensus that a vision needs a framework for implementation. The implementation of strategies and recommendations will enable the urban design outcomes to be delivered.

I am proud to launch the early recommendations from Government Architect for the revitalisation of Gosford City Centre.

I thank all stakeholders for their input into the process so far. This journey is going to take time and commitment so I urge all members of the community to maintain the energy and continue to actively contribute to the future of Gosford as we move through the revitalisation process.

Together we can renew Gosford as a regional capital with well-connected places a civic heart, desirable waterfront and great civic amenity. It will take time and require strong and clear stewardship so Gosford can realise its potential as a renewed urban centre, reinvigorating citizens with a shared sense of place.

[Lee Shearer APM](#)
[Coordinator General for the Central Coast](#)



Government Architect's foreword

Gosford has been shaped by a rich history and is defined by its natural setting and diverse communities with a strong local identity. The city has existing regional functions and attractions, including a train station, hospital, TAFE and local and state and government agencies. The investment in the hospital expansion, Australian Tax Office and Finance Building provide Gosford with opportunities for attracting knowledge jobs, and health, education and professional services. Gosford's population growth, together with an ageing local population and diverging family 'type', creates demand for a greater product mix of residential development, all of which impacts the built environment.

The revitalisation of Gosford City Centre takes a place-based approach that seeks to create a network of great, well-connected places – its civic heart, desirable waterfront and great amenity – to make Gosford a more attractive regional capital. The Urban Design and Implementation Framework (UDIF) is a high-level framework that clearly sets out the need for a future focus and early recommendations for the renewal of the city centre and priority places.

For Gosford to become a regional destination for commerce, culture and recreation it is important to create well-designed places with vibrant streets, attractive lifestyles, safe neighbourhoods, greener places, and better-connected transport systems and communities.

[Peter Poulet](#)
[Government Architect](#)

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

01. EXECUTIVE SUMMARY

Government Architect NSW (GANSW), supporting the Central Coast Coordinator General and in close collaboration with a consultancy team led by CHROFI, has initiated work to support the ongoing revitalisation of the regional city of Gosford.

The Urban Design and Implementation Framework (UDIF) will help to shape the continued development and renewal of the Gosford City Centre and support implementation of the Central Coast Regional Plan 2036 vision for Gosford. Capitalising on government and private sector investment in Gosford and taking a place based approach will strengthen its role as the regional capital and benefit current and future residents of the entire Central Coast.

The UDIF will build on previous work, plans and strategies undertaken for Gosford City Centre by the NSW Government and the former Gosford City Council (now Central Coast Council), including the 2008 Our City, Our Destiny Masterplan and the 2007 Revitalising Gosford City Centre Plan.



Gosford Urban Design and Implementation Framework

Image: Brooki Denny

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1.1 Introduction

The need for the UDIF has arisen from the growing consensus that a vision needs a framework for implementation. These strategies and recommendations will enable quality outcomes to be delivered.

Engaging with stakeholders and communities, undertaking urban design analysis and developing strategies and recommendations for collaborative implementation fosters confidence, development and investment. Done carefully, with focus on natural attributes, amenity, public domain and quality, it has the potential to transform quality of life, stimulate the economy, and enhance the environment.

The UDIF is a design-led methodology, balancing multiple and sometimes competing agendas. It is for all agencies, levels of government and industry to plug in to for action. It builds on the vision of the regional plan, which has consensus. We need to synthesise and ensure action starts now to deliver tangible outcomes for the community.

A future focus

A place-based narrative has been developed in response to the analysis, engagement and consultation that reflects the very strong landscape qualities of the city and region. This complements the strategic planning vision for the regional capital while providing tangible qualities for people to connect with and support.

This series of statements contained in Section Two provide a guide to the immediate focus while ensuring any work contributes to deliver the long-term vision.

The current context

There have been several ad hoc responses to support revitalisation in the city centre including sporadic development of new buildings, largely in the residential market concentrated around the fringe of the city centre itself.

There remains a run-down appearance in the buildings and vacant shops, with poor quality public domain and way finding, poor pedestrian amenity and poor connectivity with surrounding areas.

Previous plans have:

- not resolved issues relating to investment in public domain and the city's key public places
- led to individual developments but not significantly improved the public domain or overall attractiveness of the city
- not inspired confidence and investment in the Gosford City Centre.

However, there has been progress with the establishment of new government projects and jobs (Hospital, Australian Tax Office) within the city centre and commitments for a regional library and arts centre. This has built both momentum for change and boosted private sector investment confidence.

The Urban Design and Implementation Framework

The UDIF has identified a different approach to the revitalisation of Gosford City Centre, one that takes a place-based approach and prioritises interventions in terms of implementation and deliverability, with potential catalytic impact. This approach focuses on key public places where there is existing momentum in terms of public and private sector investment, identifying public focused initiatives in the overall city centre renewal.

Great places and cities don't happen by chance. They are designed and continue to be designed to manage their transformation over time.

The UDIF proposes an urban logic to assist with decision-making and outlining recommendations. It provides an implementation plan to guide rejuvenation, growth and foster good design and a high quality public realm.

In some cases, the UDIF presents options or scenarios to be tested further. This early release of the work to date is for exhibition and is designed to trigger discussion and inform ongoing work. It is designed to build confidence, and reveal potential partners and business and community champions for change and revitalisation.

The final UDIF will provide a series of recommendations regarding the design of places and spaces to support delivery of the vision for Gosford City Centre. It will also provide recommendations relating to governance, infrastructure and other matters that will be critical to the ongoing urban renewal and quality outcomes for Gosford.

Focus on public domain for great places

Throughout the engagement for this framework a consistent theme has been the need for a focus on the public domain. GANSW believes Gosford's future is contingent upon the creation of a network of great, well-connected places that attract further investment, particularly from the private sector.

Detailed urban design studies of the key places in the city centre will provide a framework and specific guidelines to help shape and guide the design of new projects. This approach will protect what is special and important while adding value and improving amenity across the city centre to inform and develop clear planning controls e.g. sunlight access to public domain at key times and views to the city's landscape setting, to enable greater certainty to both developers and the community.

Beyond place-based recommendations

Delivery of the overall vision for the city centre and the UDIF will require commitment to several actions, beyond the delivery of place-based recommendations. Cooperation and coordination across all levels of government will be required to achieve the desired outcomes. Future growth is dependent on strong leadership and governance, focused on coordinated and integrated delivery.

New governance and delivery arrangements are required to maintain momentum of revitalisation and to ensure it remains a priority for all levels of government. A coordinated governance role can help maximise opportunities and enable potential investment in cultural infrastructure, promote night-time activities, and soft infrastructure to draw visitors and tourists.

The UDIF also recognises a need for updated planning controls for the city centre to remove complexity and create the best possible framework for investment and development. Any amendments to planning controls need to be progressed quickly to avoid uncertainty. Absolutely key to success is supporting and securing commencement on approved development projects that contribute to delivering the vision for Gosford by bringing people and keeping them in the town centre.

Merit based assessment and design review processes early in the life of projects will assist in delivering better design processes and outcomes as well as streamlining the process.

To be successful, the design of key public places needs to immediately translate into state and local contributions plans that can be applied and negotiated with developers, as development occurs. The delivery of these actions needs to be coordinated and include short, medium and long-term priorities.

By providing an integrated design approach, we can collaboratively create the framework for the delivery of great places, spaces and buildings that attract and retain residents, jobs, tourists, and further investment.



Image: Jenae Woodall

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GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
1. Executive Summary

1.2 A future focus overview

THE SUCCESS OF THE REGION IS DEPENDENT ON THE ATTRACTIVENESS OF ITS CAPITAL.

Gosford is changing. A reimagining of the city as an active urban centre helps emphasise its distinctive natural setting and helps it to become a destination and attractive regional capital.

The city's revitalisation entices investment, optimises potential and improves quality of life. Gosford will be a desirable place to live, work and play.

Imagine if Gosford was characterised by its active waterfront, well-connected places, civic heart, and considered development with social, environmental and economic impact.

Gosford would realise its potential with lively streets, attractive lifestyles, safe neighbourhoods, greener places, and better-connected transport systems and communities.

Supporting the Regional Plan vision

The Regional Plan vision is for Gosford City Centre to become the capital of the Central Coast.

A place based narrative has been developed in response to the analysis, engagement and consultation that reflects the very strong landscape qualities of the city and region. This complements the strategic planning vision for the regional capital while providing tangible qualities for people to connect with and support.

The series of statements contained in A Future Focus (Section Two) provide a guide to the immediate focus while ensuring any work contributes to deliver the long-term vision.

The UDIF is not a design solution, but informs possible design, planning and development outcomes. It establishes a vision including a set of recommendations and implementation plan for the renewal of Gosford.

NARRATIVE:

It's time for Gosford to fulfill its role as the capital of the Central Coast. Gosford brings together all the best characteristics of the region: water, natural beauty and an urban centre.

Connect the people to its city and the city to its landscape.

Unlock the potential to create great places for people. An approach that puts the public domain and its people first.

Revitalising Gosford is not just one place or building, it is a network of well-connected places that create an attractive capital city with lively streets. An attractive city is a platform for investment with short-term returns and long-term growth.

Kibble Park is the civic heart of Gosford. It is a place for celebration, learning and exchange. It is a central meeting place the brings the character of the bushland hillsides into the heart of the city.

A place to arrive and to spend time, where the landscape, water and city centre come together.

The expansion of the hospital is a major investment in the future of Gosford. By connecting the hospital to the main street, it provides greater opportunities for innovation and learning.

A network of engaging places and connections create a strong public framework which ensures Gosford is resilient, diverse and adaptive as it develops.

Effective leadership, coordination and integration enables a common outlook and a clear understanding of the roles and responsibilities of key disciplines, agencies and consultants.

1.3 The study area

The UDIF helps connect the local community to its urban centre, and the centre to its breathtaking natural setting. It conceives of an attractive city to arrive and spend time. The UDIF makes visible and values the outcomes that matter most to people and place. There is a focus on the city centre and leveraging current investment.

The UDIF advocates for a considered approach that fosters investment with short-term returns and long-term growth without lessening the social, environmental and economic impact that will facilitate the area as a destination. It emphasises the role and value of design across the Framework's life cycle.

This is a commitment to creating long-term, systemic connections. This will build and share collective knowledge and generate unique insights that continually add value. The UDIF aims to establish alternative pathways. It will build strong, enduring and productive relationships across sectors for the public good, including the community, industry and government.

There are three key focus areas.

The Civic Heart

Geographically in the centre of the city, this study area focuses around the open space of Kibble Park. This open space is adjacent to the existing Gosford Library and its proposed re-location, multiple government and administrative functions, and significant private developments like the Imperial Centre and Bonython Tower.

Kibble Park is a central meeting place that can bring the character of the bushland reserves into the heart of the city. The focus on this area is to create a centrally located, attractive and connected open space and town square.

North City

The North City area includes the expanded Gosford hospital site, the schools west of the station, the station park and the station precinct. This latter comprises the crossing of the railway line and includes the eastern side of the station.

The hospital is a major investment in the future of Gosford. By connecting the hospital to a re-imagined rail corridor and main street, its potential is expanded to start a new generation of innovation in the northern end of Gosford.

The focus on this area is to provide a gateway precinct with pedestrian connections, improved walkability and an attractive public domain.

South City

The waterfront parklands bring the locals, the landscape and the water together. This area looks at the waterfront and its relation to Gosford City Park, the Leagues Club Park, Gosford stadium, the Australian Tax Office building, the Finance building and a potential Baker Street extension.

This is focused on providing a sense of place and a strong identity for Gosford. It will guide the opportunities to maximise the contribution of the next stage of development while ensuring public access and amenity. A place to arrive but also to spend time.



1.4 Building on previous work

Our City, Our Destiny

The result of extensive community consultation, 'Our City, Our Destiny' was a high-level master plan which articulated the community vision for the Gosford City Centre with five key precincts.

Our City Our Destiny forms the most comprehensive master plan to date for the city centre and was the basis for further detailed urban studies on transport, movement and street upgrades. Strategies and precincts identified in the master plan have built a strong base for the UDIF. The UDIF will update the master plan and provides recommendations to support implementation of the urban design framework.

What has happened since Our City, Our Destiny?

1. The early recommendations to start in the centre did not take place, and the waterfront development went out to market (The Landing). Waterfront development proposals failed to get off the ground as it was poorly received by the community, leading to the market process being cancelled. Community critique ranged from the scale of development proposed, blocking views to water from the city and privatisation of waterfront. Given the scale and design proposed, there was risk that the development may have soaked up the limited demand and activation away from the city.

Lessons Learnt

There is a need to test and define detailed public outcome with commercial feasibility and delivery scenarios. Socialise this with community before going to market to help manage community expectations as well as reduce development risk for proponents.

2. Market incentives (Bonus FSR) and general encouragement to submit Development Applications to improve market occurred. This included collecting Section 94 contributions to pay for public projects outlined in Our City Our Destiny.

A number of Development Applications submitted, but they have not resulted in many projects going to construction, resulting in limited on-ground change and Section 94 contributions. There have been signs of stagnation as people may have stopped investing in the short term with the hope of a big windfall when the market turns. In addition, research for this UDIF by HillPDA has evidenced that residential development does not directly strengthen Gosford's regional offer.

In general, it is not the approval process or development envelope capacity that is limiting growth within Gosford, but demand is limiting development. The solution isn't about improving market confidence

for developers to participate, but about increasing sales price to make the development feasible. Simply put, banks aren't sentimental, they need evidence of significant percentage of pre-sale at a price point high enough to finance the development.

Lessons Learnt

Establish city growth scenarios in line with demand forecast. In the short term, strengthening Gosford as a regional capital is unlikely to be driven by the market and S94 contributions. Hence forward public investment is needed to drive change.

Messaging needs to be managed regarding city growth. Both the UDIF and Our City Our Destiny reports highlight that the fundamental components that can help shape and make a significant regional city are there for the taking in Gosford but demand is likely to build slowly.

Government led renewal should focus on strengthening Gosford's offer as a regional centre in addition to providing housing and employment to meet the goal to make Gosford a regional capital.

'The 'life' of the city is not just from the people who live and work there but broad range of visitors who come from the region and beyond that come to satisfy different needs and desires.'

What is different about the UDIF?

The broad objectives, principles and city framework in the UDIF are essentially the same as Our City Our Destiny. What is different is a recognition of the physical scale of the city, market demand, Gosford's specific role as a regional capital and how this impacts on recommendations for implementation.

1. Physical Scale of the City

Gosford City is physically large. It has similar footprint to Sydney central business district with the length of Mann Street from Etna Street to the waterfront is equivalent to the distance between Sydney Town Hall to Circular Quay.

2. Market Demand

The projections for dwelling and employment is limited relative to scale of city and current planning envelope. The dwelling projections are 120 - 200 units per year. The commercial floor space projections are 40,000 sqm over 20 years. This is in line with Gosford City's share of Regional Plan targets.

3. Regional Capital

Regional capitals need to offer functions and attractions of type, scale and quality not available in a local centre, to draw users from the broader regional catchment.

Examples are:

- Public services: regional hospitals, court
- Educational: TAFE and University
- Cultural: museums, galleries, library
- Public space: regional parks, playgrounds, public squares, pools
- Professional services: regional scale and specialisation of services not available locally
- Retail, F&B & accommodation: regional scale and quality of retail, dining and night offering
- Events: regional scale sports, community and visitor events
- Transport: regional rail, bus, airport

1.5 Purpose of the document

The final UDIF will articulate a future focus for Gosford and provide recommendations and implementation strategies. The UDIF can be used to guide infrastructure investment and design decisions to rejuvenate the city centre, preserve the natural setting, invigorate places, and reposition Gosford as the regional focal point for commerce and high living standards.

The Framework will consist of:

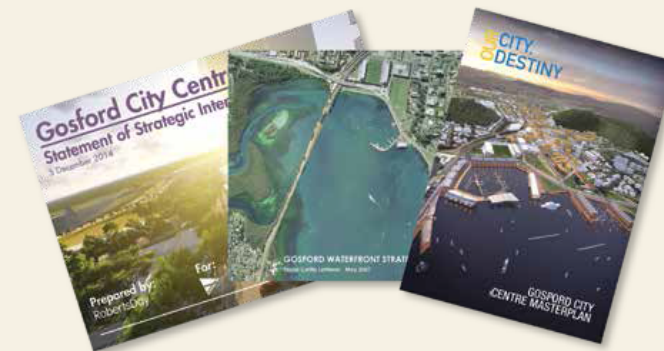
- A future focus: explaining Gosford's potential and envisioned future, and supporting the regional plan vision
- The Urban Logic: using the historical setting and citywide analysis of Gosford, together with an understanding of previous work, to identify the place-based approach and way forward
- The process to date: outlining what we heard (state/local government, industry, and community participants)
- The need to capture the extensive feedback we have received since the workshops and including this to better inform the decision process
- Early recommendations and an implementation framework with urban outcome evaluation criteria.

This document provides the extensive analysis and work to date including early recommendations for public comment.

The UDIF will achieve its ambition by:

- Putting people and place first and identifying how these can be considered in the overall design and renewal process.
- Acknowledging city-shaping is not just about one place or building, but rather a network of well-connected places with an active street life, walkability and connectivity.
- Outlining an integrated approach to ensure private and public needs, desires, and aspirations are effectively brought together into a shared ambition.
- Highlighting the potential impact and opportunities associated with government and private sector investment in Gosford, including public-sector investment projects such as the expansion of Gosford Hospital, the arrival of the Australian Tax Office, and the DFSI.
- Providing a spatial and urban logic to inform decision-making to better coordinate and integrate public and private sector projects to best leverage their potential.
- Identifying opportunities to foster better coordination, outcomes and integration.
- Identifying clusters of projects particularly those that both individually and collectively will have a catalytic effect and attract further investment. These projects can be progressed in stages in the short, medium and long term and encourage decision makers and project teams to think holistically when prioritising and funding infrastructure and other investment.

The UDIF builds on previous work



1.6 The process to date and what we heard

Our process is focused on people and place, to maximise opportunity and provide Gosford with a competitive advantage. It aims to make Gosford a more desirable regional capital that attracts people to live, work, visit and invest in the centre.

In bringing together many voices with different points of view, we fostered a shared understanding and created a common ambition better suited to people and place.

Concurrently, it was important to review the previous plans produced to understand their vision (section 6.2) and undertake urban analysis (section 3.4).

To build momentum and collective understanding, a series of workshops and surveys sought to identify commonly held aspirations and the strategic steps the community, industry and agencies will need to take to achieve them.

Design-led workshops are the fairest and fastest way of understanding the needs and objectives of different groups of people belonging to a community. These workshops, their methods and key findings informed the UDIF and will help to achieve a better, future Gosford.

SOME OF THE KEY ISSUES AND OPPORTUNITIES RAISED INCLUDE:

With significant public investment underway (hospital expansion, Australian Tax Office and Finance building, regional library, arts centre) there is potential for rejuvenated market confidence

There is great potential for Gosford to offer a high quality lifestyle with good links to Sydney, good transport and a strong sense of community

An inconsistent approach to planning and development over time

Public domain upgrades are needed to provide more trees, and improve parks and gardens, streetscapes, and public art to attract people and further investment

There is opportunity to rethink Gosford Station Interchange, upgrade Kibble Park and William St Plaza, connect the station to the Hospital, and to link the stadium to the water

An improved precinct transport network is needed to reduce car dependency in Gosford for better pedestrian access/footpaths, reduce impact from traffic, provide bike paths/hire, and better linkages over the station and rail line

There is the need to encourage a greater diversity of activity and people into the centre

We should encourage active usages, night-time economy, diverse users (youth, aging population) and uses, space for sports and outdoor recreation/family areas with shelter throughout the centre and along the water

Support Gosford as a cultural capital with diverse arts and music, with an active, public waterfront

Revitalisation of Gosford should support the development of a distinctive commercial centre which can compete with other centres, offering a greater variety of retail shops, food and beverage, events, but there has not been significant investment in the public domain

Many DAs have been lodged however only a small proportion are progressing



Image: Sally Dingo

Image: Sally Dingo

1.7 Summary of early recommendations

State and regional level strategies and actions

Foster a place based approach and focus on public domain

- Develop Place based strategies to focus activity and investment in and around the priority places
- Establish Place managers for Gosford's key places with a focus on the use and activation of places.

Improve regional access and movement

- Prioritise and focus transport investment to improve connections for people across the Central Coast into Gosford to support its role as a regional capital
- Prioritise place-based integrated transport plan with car parking strategy
- Fast track joint DPE/Central Coast Council car parking review and strategy (to be developed jointly with transport plan above)
- Support and plan for reduce journey times by rail to Sydney from the Central Coast and particularly Gosford

Leadership, coordination and integration

Leadership and coordination

- Build on the existing role of the Coordinator General to maintain momentum and ensure a holistic approach to Gosford
- Manage development by establishing a case management approach for major applications
- Coordinate and support delivery, including the coordination of activity around Government land and projects

Streamlining and de-risking the planning process

- Provide statutory weight to the urban design principles promoted in the UDIF
- Establish a case management approach to manage major applications
- Establish a Government Architect led or endorsed Design Review Panel for Gosford central business district
- Work with relevant agencies to fast track referral processes
- Implement simplified, flexible and streamlined planning controls with for Gosford central business district

Infrastructure requirements and developer contributions

- Prepare an updated schedule of infrastructure projects and public domain works to increase certainty about the provision of infrastructure
- Prepare a revised s94A plan (now s7.2 plan) based on the updated schedule of infrastructure
- Finalise the Central Coast Council review of the performance of the existing S94A plan and other approaches to developer contributions and provide transparency on where contributions are being spent
- Develop a plan to spend current funding on projects identified in the UDIF that will have a high impact and build confidence.

1.8 Next steps and process going forward

To ensure the community has a say in the preparation of the UDIF, it will be prepared in stages. The community will have the opportunity to comment on each of these stages and this feedback will inform a complete, final UDIF.

The NSW Government, Central Coast Council and a range of stakeholders will have a role in responding to the UDIF recommendations and delivering the ongoing revitalisation.

The stages in preparation of the UDIF are outlined in the adjacent table.

GOSFORD REVITALISATION

FROM APRIL 2018

- 1 Revitalising Gosford Discussion Paper
Gosford's Civic Heart: Place-based report #1
Discussion Paper & Place-based Report #1
For consultation
- 2 Gosford's Northern Precinct –
Hospital to Railway: Place-based Report #2
For consultation
- 3 Gosford's Waterfront Precinct:
Place-based report #3
For consultation
- 4 Consultation closes on all discussion
papers and final report
- 5 Government Architect NSW final report on
Gosford Revitalisation released for comment
- 6 Government response to final report from
Government Architect NSW

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

02. A FUTURE FOCUS

Gosford is changing. The city is being reimagined as an active urban centre that values its natural setting and becomes a vibrant destination and attractive regional capital.

The Urban Design and Implementation Framework (UDIF) is not a single design solution, but informs possible design, planning and development outcomes. The UDIF goes beyond determining development potential to help revitalise the city, strengthen and enhance its natural environment and ensure the longevity of place.

The UDIF establishes an ambition including a set of recommendations and implementation strategies for the renewal of Gosford. It advocates for the management of the city centre and its places. It informs the broader context and leverages current investment to establish future transport and soft infrastructure requirements, public domain and waterfront improvements, and development with consideration of social, environmental, and economic impact.

2.1 The narrative

The Central Coast Regional Plan 2036 shapes the development of the region. The UDIF provides a way forward to help deliver Gosford as the regional capital and a smart hub.

The revitalisation of the city centre will entice investment to energise the city, attract new residents, businesses, tourism, and culture. Gosford's renewal can help the Central Coast optimise its potential and improve people's quality of life, ensuring that the region is a desirable place to live, work and play.

Gosford will be characterised by its well-connected revitalised places, its civic heart, desirable waterfront and great amenity. Under strong and clear stewardship, Gosford can realise its potential as a renewed urban centre, reinvigorating citizens with a shared sense of place.

The natural setting and distinctive character of communities provides a strong local identity that is catalytic to well-designed places with vibrant streets, attractive lifestyles, safe neighbourhoods, greener places, and better-connected transport systems and communities.

Gosford will be a destination for commerce, culture and recreation, with great places for everyone.

The statements on the following pages indicate the potential of the revitalisation. The photographs and visualisations identify the strengths and weaknesses, and represent the opportunities for Gosford.

1.

It's time for Gosford to fulfill its role as the capital of the Central Coast.

Gosford brings together all the best characteristics of the region: water, natural beauty and an urban centre.

2.

This framework connects the people to its city and the city to its landscape.

3.

This framework unlocks the potential to create great places for people.

An approach that puts the public domain and its people first.

4.

Revitalising Gosford is not just one place or building, it is a network of well-connected places that create an attractive capital city with lively streets.

An attractive city is a platform for investment with short-term returns and long-term growth.

5.

Kibble Park is the civic heart of Gosford.

It's a place for celebration, learning and exchange. It is a central meeting place that brings the character of the bushland hillsides into the heart of the city.

6.

A place to arrive and to spend time, where the landscape, water and city centre come together.

7.

The expansion of the hospital is a major investment in the future of Gosford.

By connecting the hospital to the main street, it provides greater opportunities for innovation and learning.

8.

A network of engaging places and connections ensures Gosford is resilient, diverse and adaptive as it develops.

9.

Effective leadership, coordination and integration enables a common outlook and a clear understanding of the roles and responsibilities of key disciplines, agencies and consultants.

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
2. A FUTURE FOCUS

1.

It's time for Gosford to fulfill its role as the capital of the Central Coast.

Gosford brings together all the best characteristics of the region: water, natural beauty and an urban centre.

Gosford will be a safe and vibrant hub of employment and services that attracts people to spend time and money, during the day, in the evening, and on weekends.

The city's people can help preserve the environment, to produce and sustain it for now and in the future.

It makes sense to invest in Gosford as an urban centre. It is the historic heart of the Central Coast, replete with natural beauty. The city is well-equipped to locate new homes and investment served by effective public transport.

Gosford can balance quality of life and commercial activity.



Image Network

2.

This framework connects the people to its city and the city to its landscape.

Gosford is suitably placed as a strategic location that can capture the beauty of its natural setting to entice inward investment, urban migration and announce Gosford as the social and cultural focal point of the region. This needs a place-based approach.

The natural environment is one of the city's greatest assets. It is optimised by considered, well-designed upgrades and expansion to the public domain, and the connection of open spaces and National Park to create a network of great places and green spaces across the city and extending into the region.



KSBB+Partic. ImageJenny Wood

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
2. A future focus

3.

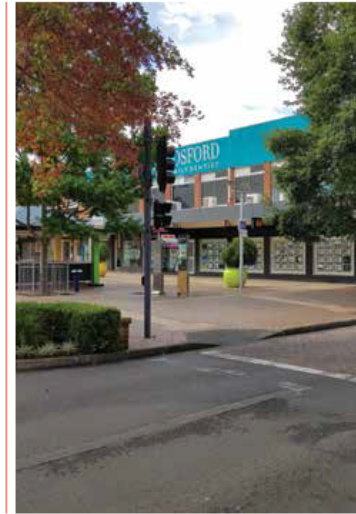
This framework unlocks the potential to create great places for people.

An approach that puts the public domain and its people first.

It demands the optimal use of potential, under-utilised spaces and connections. Gosford will evolve as a cultural, civic and commercial hub where established uses and places can be complemented by new programs, buildings and spaces to attract and engage locals and visitors.

Great places are considered, cohesive, well-designed and planned – they put people very much at the centre.

Residents, visitors or workers will want to come and spend time with family and friends. The city must be a dynamic destination for culture, to learn and play, open day and night.



Merrill Street. Top left image: Courtesy of Central Coast Library Service, top right image: James Woodall Visual Artists: CHROH

4.

Revitalising Gosford is not just one place or building, it is a network of well-connected places that create an attractive capital city with lively streets.

An attractive city involves the connection of great places, good design and a vibrant street life. An attractive city is a platform for investment with short term returns and long-term growth. With a focus on people and places, it's an opportunity to benefit from growth if managed well.

By optimising connectivity in all directions, Gosford has the potential to greatly improve the amenity for everyone. Daytime and night-time activities encourage economic prosperity and longevity. Gosford must provide well-managed, diverse, and safe environments for a wide cross-section of the community.

Gosford can become a great connector – between the waterfront, bushland hillsides and the emerging urban centre.



Main Street, Top Image: Courtesy of Central Coast Library Service
Visualisation: CHROFI

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
2. A FUTURE FOCUS

5.

Kibble Park is the civic heart of Gosford.

It is a place for celebration, learning and exchange. It is a central meeting place that brings the character of the bushland hillsides into the heart of the city.

Vibrant streets and active lifestyles help shape places and reinforce interfaces between city, bush and water.

Appealing, safe open spaces and greener places are activated by people. Successful, well-designed places are supported by walkability. If residents could walk or ride a bike to jobs, education opportunities and services, urban spaces are better activated.



Gosford Library. Top left image: Courtesy of Central Coast Library Services, top right image: Jenae Worsell, Manufacture: CHROFI

6.

A place to arrive and to spend time, where the landscape, water and city centre come together.

Imagine how thousands of extra people spending time and money in their local communities each day would impact positively on local businesses and lead to further job growth and revitalisation.

Liveable places are safe, attractive, socially cohesive and inclusive, and environmentally and economically sustainable.

Liveable places can greatly improve people's health, aspirations, and opportunities as well as supporting a range of lifestyles.



Top image: Courtesy of Central Coast Library Service, bottom image: Sally Elger

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
2. A FUTURE FOCUS

7.

The expansion of the hospital is a major investment in the future of Gosford.

By connecting the hospital to the main street, it provides greater opportunities for innovation and learning.

By better connecting the hospital to the main street and envisaging the waterfront as a destination with great civic amenity, after focusing in the Civic Heart the city's uses and range of lifestyles will be clustered around efficient transport. This gives people the choice to live close jobs, top class services and healthcare, schools, shops, and recreation.

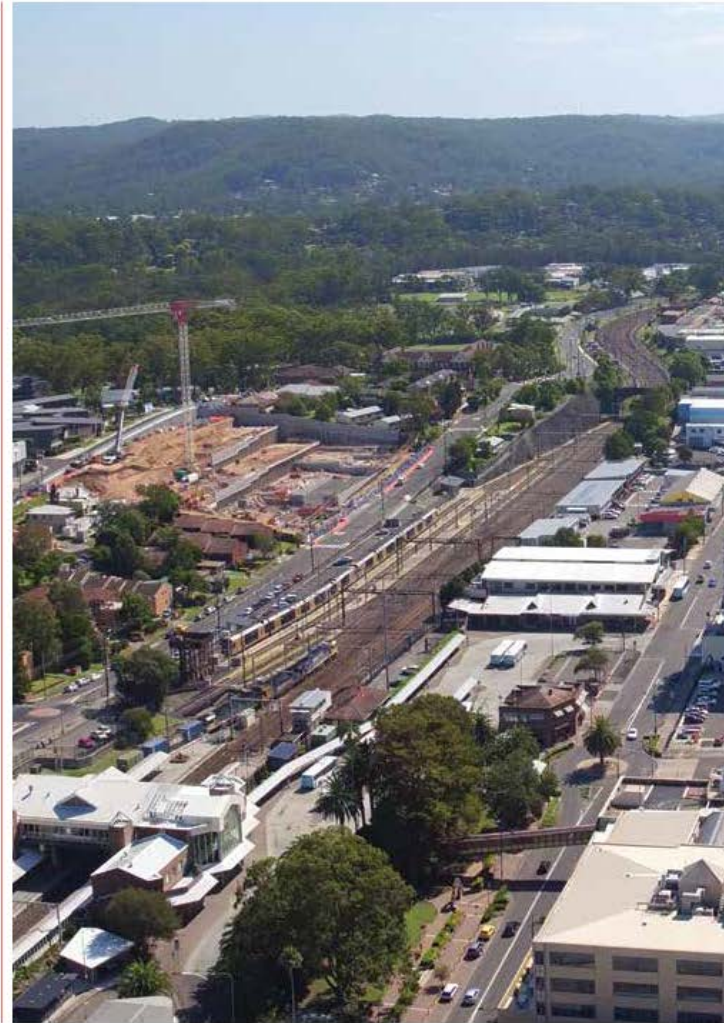


Image: Brian Dwyer

8.

A network of engaging places and connections create a strong public framework which ensures Gosford is resilient, diverse and adaptive as it develops.

Gosford must be future-oriented, catering to the diverse and changing needs and values of local communities and local businesses.

To foster positive social, environmental and economic impact, Gosford must provide further job growth, a burgeoning waterfront experiences, revitalised public domain and a responsive urban centre that is flexible to change.



Image: Sashy Design

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
2. A FUTURE FOCUS

9.

Effective leadership, coordination and integration enables a common outlook and a clear understanding of the roles and responsibilities of key disciplines, agencies and consultants.

For the revitalisation of Gosford to be successful, leadership needs to be established from the outset and visible through all stages of design, change and procurement.

Coordinating the needs of communities and individuals alongside planning and development requirements will ensure well-designed places are delivered as intended.

Understanding and integrating the roles, knowledge and points of view of all involved establishes the framework for long-lasting design solutions. It facilitates a forward-looking, consistent and long-term vision. This approach needs to be based on a clear understanding of the present situation and the factors that will influence future generations.



Image: Central Coast Matters.



GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

03.

THE URBAN LOGIC

Good urban design and planning processes start with research and analysis. This work is then an important part of both developing and informing a shared understanding in any workshop process.

In explaining what makes a regional capital and developing recommendations for Gosford, it is important to analyse the city, its places, setting and history to best inform its future.

Analysis of regional priorities, as well as studying Gosford's scale, size and population, directly informs the development of meaningful urban principles.

The analysis of the city, comprising the city form, its natural and built assets, heritage places and landmarks, parking, and key projects informs the urban design recommendations and the place-based framework to revitalise Gosford.

3.1 What makes a regional capital?

A regional city offers a range of functions and attractions including civic, cultural, shopping, professional services, health, education, transport and events beyond that of a local centre. The clustering of these offers results in a synergy of increased visitation and investment in the city, amplified by the diversity and scale of the investments.

What makes Gosford a regional capital?

Gosford has been shaped by a rich history of Aboriginal culture and urban settlement. Originally home to the Darkinjung and the Guringai indigenous people, Gosford was colonised late in 1823 and expanded rapidly with the development of transport connections to greater NSW combined with the allure of the Central Coast lifestyle.

Defined by its established functions and attractions, including a train station, hospital, tafe and local, State and Government agencies, it has the foundations of a regional capital, with great improvement potential. Gosford has become home to a population of 20,552*, with in the Central Coast catchment of 339,600** people and is part of the fastest growing area in NSW.

Increased investment in the city centre is presenting Gosford with the critical opportunity to invest in its built environment, with a peak in recent development applications for new buildings.

Health, Education and Professional services are the three major industries that will generate employment within the city centre. Increased residential offerings are required due to recent developments of the ATO, Jobs Finance Building and the Gosford Hospital development. Population growth, the ageing of the local population and a diverging family 'type' creating demand for a greater product mix of residential development will all impact the built environment.

*SOURC: 2016 Australian Bureau of Statistics



Gosford Waterfront Image: Brian Dwyer

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

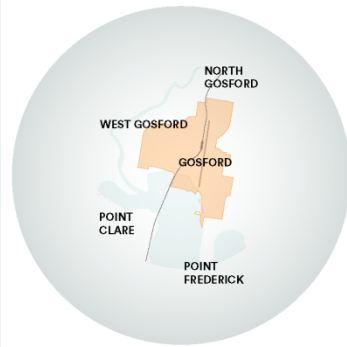
Key Statistics



PEOPLE WHO LIVE IN THE CITY

4,975 PEOPLE *
9,745 by 2036 *

* Study area + neighbouring area - Gosford/West Gosford area. Source: Forecast ID/Census data 2016 Australian Bureau of Statistics



POTENTIAL VISITOR CATCHMENT FROM NEIGHBOURING COMMUNITIES

172,000 PEOPLE *

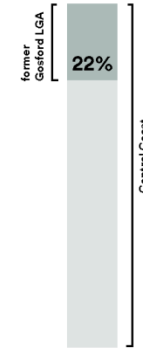
* Catchment: Former Gosford LGA. Forecast ID/Census data 2016 Australian Bureau of Statistics



POTENTIAL VISITOR CATCHMENT FROM THE BROADER CENTRAL COAST REGION

339,600 PEOPLE *

* Source: DPE 2016 population projections and Census data 2016 Australian Bureau of Statistics

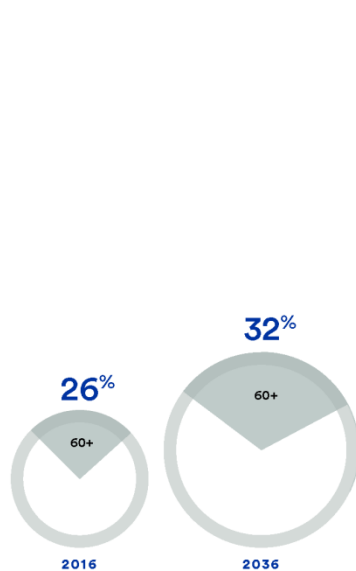


POPULATION GROWTH IN THE CENTRAL COAST

As per the DPE projections for Central Coast LGA, growth in the former Gosford City LGA population represents 22% of the Central Coast population increase to 2036. The remaining 78% is projected to occur in the former Wyong LGA with recent land releases contributing to the significant growth.

The increase in demand for residences in and around Gosford will also drive demand in the city centre for employment.

* Source: Forecast ID/Census data 2016 Australian Bureau of Statistics

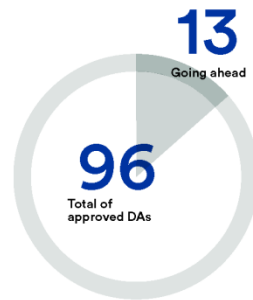


AGEING POPULATION

The proportion of ageing population is expected to grow over the next 20 years. This is evident with 60% of the projected population growth in residents being 60 years and over.

As a result, housing for the downsizer market will be required. Demand for co-located housing and services will also be required.

* Catchments: former Gosford LGA. Source: Forecast ID/Census data 2016 Australian Bureau of Statistics

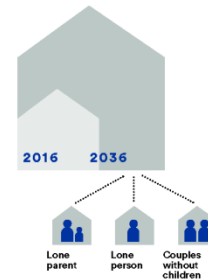


LIVING IN THE CITY

Recent incentive schemes have seen a steep increase in DAs, many of which have lapsed.

There are more apartments projected in Gosford in the next four years than demand for housing.

* Source: DA Submissions, Central Coast Council



PRODUCT MIX

Apartments are expected to represent 44% of the product mix in the Central Coast, with 40% being semi-detached dwellings. This combined with an ageing population will drive the demand for a mix of smaller products.

There will be a significant growth in one parent, lone person and couples without children over the next 20 years (90% of projected growth in households).

Demand for unit development could double as the effects of the hospital expansion and government investment stimulate demand. Longer term, there is further growth with catchments of affordable housing demand from the Sydney Metropolitan market.

* Household projections in former Gosford LGA. Source: Forecast ID/Census data 2016 Australian Bureau of Statistics



WORKING IN THE CITY

Government has offered significant investment to bring high quality jobs to the area, starting with the ATO.

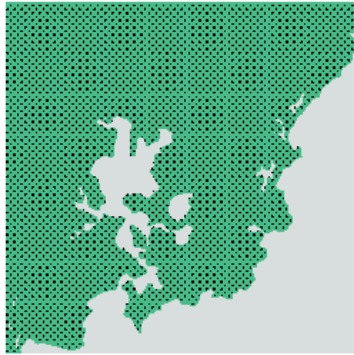
The three major industries that will generate employment in the CBD are Health, Education and Professional services.

Proposed development should be concentrated in this area. Employment projections will stimulate demand for a further 15,000 to 20,000sqm of office space in Gosford CBD over the next 20 years. This would appear to be a very conservative forecast if Government stimulus is added to this demand.

* Household projections in former Gosford LGA. Source: Forecast ID/Census data 2016 Australian Bureau of Statistics

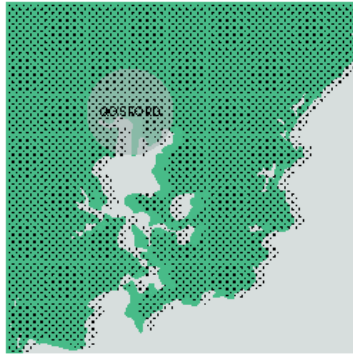
GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

Settlement history



PRE-SETTLEMENT

The Darkinjung and the Guringai indigenous people lived here.



1885 GOSFORD BECAME A TOWN

Constrained access to the site delayed convict settlement in the region until 1823. Gosford became the first centre of settlement of the Central Coast, with an economic centre comprising a court house, hotel, post office and general store located near the steamer wharf in Mann St South. Gosford became a town in 1885.



1930 GOSFORD CONNECTED

Rail links (1887) and highway (1930) to Sydney accelerated Gosford's growth and the town centre began to gravitate toward the railway station. Land reclamation redefined the foreshore, including industrial land in West Gosford and the present-day Gosford Stadium.



GOSFORD 2018: IN A RAPIDLY EXPANDING AREA

Growth in Gosford has accelerated since the 1990s with the addition of numerous medium and high density dwellings. 1.1 million people now live in the corridor of which it is part. Gosford links and is central to numerous satellite centres, that include Erina, Woy Woy, Wyong, Tuggerah, Warnervale.

What has changed?

Mann Street South



Early view of Mann Street South. Image: Central Coast Council Library Service



Mann Street South 2018. Image: Evaro Drones

Mann Street



Mann Street Scooter Race 1936. Image: Central Coast Council Library Service



Mann Street 2018. Image: James Wordell

William Street Mall



William Street Mall in construction c. 1970. Image: Central Coast Council Library Service



William Street Mall 2018. Image: James Wordell

Gosford Library



Kibble Estate carpark - Gosford Library. Image: Central Coast Council Library Service



Gosford Library 2018. Image: James Wordell

3.2 Central Coast Region

GANSW ongoing work on the Urban Design Guide for Regions identifies the key issues for the Central Coast built environment based on the Central Coast Regional Plan 2036 Goals and Directions and in consultation with the Central Coast Council.

The issues identified form 11 Design Priorities to be addressed across regional NSW.

The Urban Design Guide for Regions outlines specific urban design actions and urban design tools to address the 11 Priorities; with the common objective of achieving well-designed built environments that are healthy, responsive, resilient, integrated and equitable.

The following is a summary from the Guide.

Urban Design Guide for Regions:

CENTRAL COAST REGION

With an ideal climate between temperate and tropical, the Central Coast offers an attractive lifestyle, with increasing investment in construction and employment. A high-value environmental and cultural landscape, with Australia's fastest-growing indigenous population.

SETTLEMENT HISTORY

- Constrained access delayed convict settlement until 1823
- Gosford, the first centre of settlement became a town in 1885, followed by rail (1887) and highway links (1930)
- Central coast development accelerated along the coast, with satellite centres.



KEY STATISTICS

POPULATION 2016	339 550	TRANSPORT INFRASTRUCTURE	Airport: access to Sydney (West Wyalong Airport)
POPULATION 2036	415 050 (22%)		Road: Pacific, Central Coast Highway, Pacific Motorway
URBAN CONCENTRATION	96%		Rail: Main Northern Line
REGIONAL CITIES	Gosford		Port: access to Botany Bay
STRATEGIC CENTRES	Erina, Woy Woy, Warnervale Town Centre, Wyong, Tuggerah	NATURAL ENVIRONMENT	Coastal lakes and water ways including the Hawkesbury River, Broken Bay, Tuggerah Lake, Brisbane Water, Soudi, Dharug, Popran and Wyrabalong National Parks, Wet sclerophyll forest, rainforest, dry sclerophyll forest, freshwater and saline wetlands. Ecological sub-tropical to temperate transition zone
LOCAL CENTRES	Toukley, The Entrance, Long Jetty, Terrigal, Umina, Ourimbah, Ettalong, Point Clare, Kincumber, Killarney Vale	CLIMATE ZONES	Warm Temperate
GROSS REGIONAL PRODUCT (GRP)	\$15.1 billion	REGIONAL ATTRACTIONS	Recreational water activities along the beaches, rivers and lake systems
MAIN INDUSTRIES (BY GRP)	Health Care and Social Assistance Manufacturing Construction	COUNCILS	Central Coast
MAIN INDUSTRIES (BY EMPLOYMENT)	Health Care and Social Assistance Retail Trade Manufacturing		
ECONOMIC GROWTH OPPORTUNITIES	Construction, manufacturing, tourism		

KEY ISSUES

Protecting valuable environmental and cultural lands whilst supporting the productive lands west of the M1.

Planning for a resilient built environment considering the natural hazards of bushfire, flooding and coastal hazards associated with climate change.

Developing region-specific guidelines for major centres, middle-ring and neighbourhood-wide scales, aligning land use and infrastructure planning and accounting for the growth of Gosford City Centre.

Confronting long-travel distances, car-dependence and obesity with well-planned settlement patterns and active infrastructure.

Planning support for central business district revitalisation.

Reducing youth unemployment, limited job opportunities and poor regional job containment at all scales of the built environment while strengthening cross-regional economic connections.

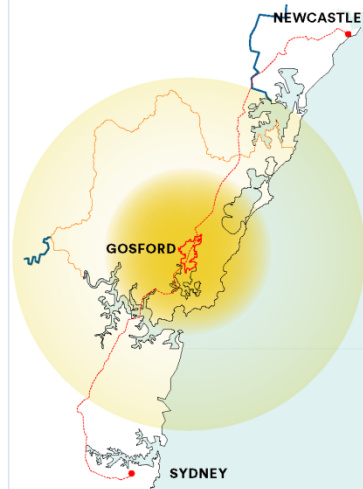
Creating places that are inclusive and well-designed, defining and protecting the 'Central Coast lifestyle'.

Gosford key considerations

The Central Coast Regional Plan 2036 aims to “grow Gosford as the region’s capital.”

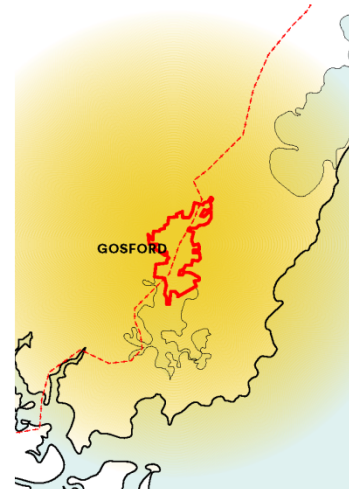
The priorities, outlined in the Regional Plan specific to Gosford, which have informed the UDIF are:

- Growing Gosford City Centre as the region’s capital and focus of professional, civic and health services for the region’s population.
- Undertaking and integrating precinct planning for the Waterfront, Arts and Entertainment, City Core, Railway and Hospital precincts – to grow jobs and coordinate the delivery of improved transport infrastructure.
- Attracting and facilitating greater commercial development in Gosford City Centre by improving the public domain and providing opportunities for development through local planning controls.
- Promoting Gosford City Centre as an attractive place to live, work and play through local planning controls that support vibrant and safe cultural, entertainment and visitor activities.
- Enhancing the growth potential of the health precinct around the Gosford Hospital and allied health facilities in Gosford City Centre to drive the growth of services and specialisation in the region.
- Integrating the railway station with other activities and seeking opportunities to improve east–west connectivity across the railway line.
- Identifying opportunities for the consolidation and relocation of government services and agencies to Gosford City Centre.
- Ensuring that development in Gosford City Centre responds to its natural setting and complements the public domain.
- Improving access to Gosford City Centre from the west and north.



A region is defined by its capital.

The revitalisation of Gosford lies at the heart of regeneration of the Central Coast.



Gosford’s built environment is a good base to work from. Focusing growth in Gosford’s City Centre will reinforce the opportunity to bring social and economic benefits to the city.



Image: Central Coast Regional Plan

The Central Coast Regional Plan identified Gosford as a priority.

Within Gosford the areas to be prioritised are:

- 1. The Waterfront**
- 2. The Arts and Entertainment Precinct**
- 3. The City Core**
- 4. The Railway Precinct**
- 5. The Hospital Precinct**

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

How great places will support a regional capital

Quality places suit a variety of people and activities. As a regional capital Gosford needs to offer places that attract not only the locals, but also business, specialised services, regional visitors, commuters and tourists.

Diverse and rich places offer variety, quality and choice in a concentrated area. This will make Gosford a desirable place to visit and the best option to access key businesses, jobs and services.



High Street and Retail



Commuters



Arts and Music



Playgrounds



Students



Workers



Hospital



Entertainment



Occupation of the waterfront



Open Green Spaces



Office



Tourists

A regional capital needs a variety of users, programmes, activities and businesses to make a successful regional capital. They also provide a comprehensive service to the people of the Region.

"The regional cities of NSW are defined as providing a "...full range of business, government, retail, cultural, entertainment and recreational activities. They are a focal point where large, growing regions can access good jobs, shopping, health, education, recreation and other services and not have to travel more than one hour per day" *

Gosford in comparison

This table compares Gosford with other Regional Cities of NSW by particular building and infrastructure type and illustrates its relative deficiency of facilities.

It highlights the need to reinforce Gosford, in turn serving the people of the Central Coast. It is the sum of the facilities working together to complement each other that will incite development and inspire visitors.

The lack of specific building and infrastructure facilities has been strongly echoed throughout the public participation process. Through community consultation, the three elements most frequently identified as needed in Gosford were a regional performing arts facility/cultural centre, a University and a city playground.

* SOURCE: Centre Types in the Metropolitan Strategy, NSW DP&E 2007

Regional NSW cities in 2010: A checklist of elements

Note: Changes have occurred in other centres since this data was prepared.

		GOSFORD	NEWCASTLE	PARRAMATTA	PENRITH	LIVERPOOL	WOLLONGONG
REGION	CENTRAL COAST	HUNTER	GREATER WEST. SYD	NORTH WEST	SOUTH WEST	ILLAWARRA	
DEMOGRAPHIC PROFILE							
POPULATION REGION	2006-08	304 700	529 200	668 200	760 000	409 700	282 300
	2031	400 000	675 000	780 300	1 024 317	742 272	327 600
EMPLOYMENT OF REGION	2006-08	125 254	228 334	308 500	240 000	119 500	100 765
	2031	170 500	294 334	369 500	367 000	208 500	130 765
FACILITIES (IN CENTRE)							
HOSPITAL		✓	✓	✓	✓	✓	✓
UNIVERSITY CAMPUS		✓	✓	✓	✗	✗	✓
TAFE REGIONAL		✗	✗	✗	✗	✓	✓
TAFE		✓	✓	✓	✓	✓	✓
CONVENTION & EXHIBITION CENTRES		✗	✓	✓	✓	✗	✓
MUSEUMS		✗	✓	✓	✓	✓	✓
CONCERT HALL/ENTERTAINMENT CENTRE/ THEATRES/PLAYHOUSE		✗	✓	✓	✓	✗	✓
STADIUMS		✓	✗	✓	✓	✗	✓
TOWN HALL		✗	✓	✓	✗	✗	✓
LIBRARY		✓	✓	✓	✓	✓	✓
ART GALLERIES CULTURE CENTRE		✗	✓	✗	✓	✓	✗
CIVIL SQUARE/PRECINCT		✗	✓	✓	✓	✓	✓
SCHOOLS		✓	✓	✓	✓	✓	✓
REGIONAL SHOPPING CENTRE		✗	✓	✓	✓	✓	✗
CONSERVATORIUM		✓	✓	✓	✓	✗	✓
SPORTING FACILITIES INDOOR/OUTDOOR		✓	✓	✓	✓	✓	✓
COMMUNITY MEETING CENTRE		✗	✓	✓	✓	✓	✓
REGIONAL PARK/RECREATIONAL FACILITIES		✗	✗	✓	✓	✓	✓
TRANSPORT (IN CENTRE)							
STATIONS		✓	✓	✓	✓	✓	✓
INTERCHANGE		✓	✓	✓	✓	✓	✗
SHUTTLE BUS		✗	✓	✓	✗	✗	✓
CBD BY-PASS/RING ROAD		✗	✓	✓	✗	✓	✓

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

The table adjacent comprises a shortlist of desirable facilities for future Gosford and outlines where future investment in Gosford could be concentrated. Importantly, the combination of new development and existing facilities intelligently used will boost visitor numbers and activate the city centre.

New developments in Gosford will need to acknowledge existing facilities and address the future vision of Gosford; including questions such as:

- What will attract the biggest crowd of regional visitors?
- What will create a visible change?
- What will help to attract people and stimulate investment?
- How will the project be sequenced and developed in relation to neighbouring development?

How will future developments impact the city of Gosford?

Considered investment in new facilities can become catalysts for change and have a long-term, positive impact on the city centre. To project the impact of new developments on the city, benchmarks of a similar scale should be sought.

- ALREADY IN GOSFORD
- THINGS WE CAN ADD TO GOSFORD

FACILITIES	
Hospital	in development
University	bringing a research hub
Tafe regional	
Tafe	Gosford Tafe Campus
Convention and exhibition centre	
Museums	Gosford Classic Car Museum (in city surrounds)
Concert hall and entertainment	
Theatre / playhouse	Laycock Theatre (North Gosford)
Stadium	Central Coast Stadium
Town hall	Council administration building
Library	Relocating
Art gallery and cultural centre	Gosford Regional Gallery
Civic square	Kibble park
Schools	Numerous
Regional shopping centre	Imperial Centre
Conservatorium of music	
Sporting facilities – indoor and outdoor	
Community meeting centre	Reuse existing library
Regional park	
Station	Upgrade
Interchange	To be considered – future
Shuttle bus	

- ALREADY IN GOSFORD
- THINGS WE CAN ADD TO GOSFORD

PLACES/ FUNCTIONS THAT ATTRACT PEOPLE	
Eat street	
Night time activities	
Play ground/ water play	
Promenade	Improve on waterfront
Markets	
Gardens	
Water activities	
Sustainability focus	Greengrid, power, water
Shade / thermal comfort	Street planting
Parking and easy access	Needs consideration
Animals and native environments	Greengrid
Good quality hotel accommodation	Gov subsidy like Newcastle?
Shopping high street	

3.3 Drawing on the work of previous documents & master plans ‘Our City our Destiny’

The result of extensive community consultation, ‘Our City, Our Destiny’ was a high level master plan which articulated the community vision for the Gosford City Centre with five key precincts.

‘Our City Our Destiny’ forms the most comprehensive master plan to date for the city centre and was the basis for further detailed urban studies on transport, movement and street upgrades. Strategies and precincts identified in the master plan have built a strong base for the UDIF.

Most of the ‘Our City Our Destiny’ principles and city framework remain applicable today. The UDIF distills and updates the OCOD recommendations to key principles to aid in the assessment of any future proposal or project. This allows the 2010 master plan ideals to remain relevant, adaptable and resilient. Further, it recognises the physical scale of the city, market demand, Gosford’s specific role as a regional capital and how this impacts on recommendations for implementation.

See Appendix 6.2 for a detailed analysis.



Gosford Urban Design and Implementation Framework

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
3. The Urban Logic

3.4 Gosford city analysis

The City Form

Gosford city centre is surrounded by a picturesque natural environment. Sitting in a valley between the bush covered hills to the east and west and the harbour to the south, Gosford's built environment has been formed by an orthogonal grid that runs north to south.

The south of the city grid is split by the Central Coast Highway before meeting at the water's edge. The city's main street, Mann Street, runs from Gosford Showground in the north, to Baker Street in the south, where it meets the water at the same level.

The city is formed by the elements of the natural and built environment. Waterview Park (Presidents Hill) rises west of the station whilst Rumbalara Reserve rises to the east, forming a natural valley for the city. Gosford is formed by an orthogonal grid that runs north to south. In the south the city grid meets the water's edge, split by the Central Coast Highway. Mann Street terminates at Leagues Club Field, whilst Baker Street meets the water at the same level.

The City Centre of Gosford comprises Gosford Hospital, Gosford train Station and the Gosford CBD, consisting of a mix of retail and commercial uses. It is surrounded by residential properties to the north east, west and Brisbane Water foreshore to the south.

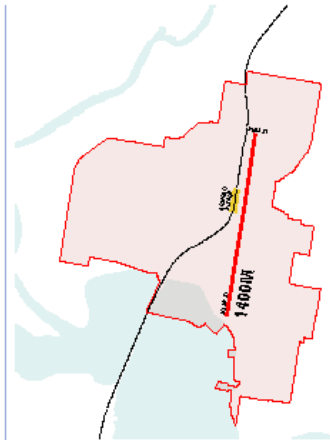


Gosford City Image Bank Drive

The scale of Gosford

By comparing Gosford to Sydney and Newcastle, its two closest neighbouring cities, Gosford's large regional city footprint is evident. The high street of Mann St between the hospital and the waterfront is 1.4km long which is similar to the length of Sydney's George St between Townhall and Circular Quay. It is not a walk done every day.

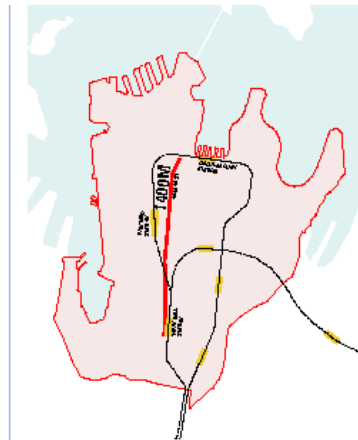
This scale comparison is contrasted by a relatively small population of people living in the centre. A city of this scale needs a greater density of visitors, residents and businesses to feel active as a regional capital.



GOSFORD
4,975
 PEOPLE *



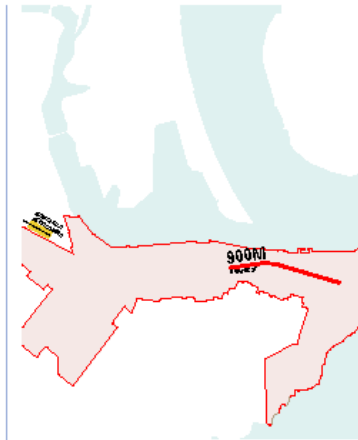
Image:Si:36pp, NSW Land & Property Information



SYDNEY
21,901
 PEOPLE *



Image:Si:36pp, NSW Land & Property Information



NEWCASTLE
4,498
 PEOPLE *



Image:Si:36pp, NSW Land & Property Information

* SOURCE: FORCAST 10/Census data 2016 Australian Bureau of Statistics

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

Natural Assets

Nature and green spaces in an urban environment can improve the social wellbeing and health of the people living, working in, and using the city centre. Green spaces including natural parks and waterways are a critical part of Gosford's civic and cultural identity- it is a city nestled between two hills. Waterview park rises west from the train station and the eastern hill; and Rumbalara Reserve forms part of a longer natural system. Fronted by a significant harbour and areas of natural bush, Gosford offers an attractive lifestyle with a close relationship between the urban centre and the natural environment.

The issues of climate change and sea level rise necessitate a vision for the sustainable future of Gosford. In addition to existing sustainability projects including the Management of the Coastal Open Space System and the Gosford City water mining dual reticulation scheme; the redevelopment of Gosford creates an opportunity to reinvent the city as smart and green. The unique relationship between Gosford's natural features and its built environment forms the basis of a future vision: the integration of urban fabric and sustainable initiatives.

An example Climate Change Strategy would include and address the following:

- Reducing water consumption
- Reducing energy consumption
- Reducing automobile travel
- Cutting greenhouse gas emissions
- Making land use decisions that create a more efficient and walkable city
- Improving building energy performance
- Increasing biodiversity
- Mitigating the impact of sea level rise.

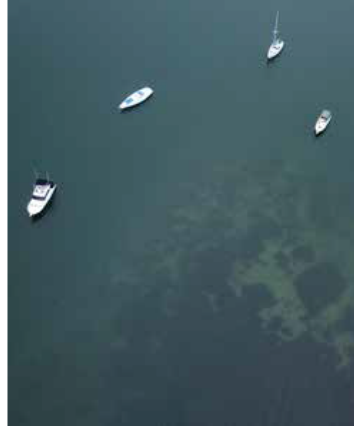


Image | Jenae Wood

Water

The south of the city is formed by Brisbane Water; an estuary ecosystem with mangroves and calm waters. The beaches, bays and inlets of this waterway provide pristine water views for many communities including Gosford.



Image | Jenae Wood

Bushland Reserves

The bushland reserves sit on either side of the city on President's Hill and Rumbalara Reserve. The hilltops frame the city and align Gosford centrally in the broader Narara Valley.



Image | Jenae Wood

Parks

Within the city, Kibble Park and Leagues Club Field are well-loved as welcomed open green spaces within the built environment.

Built Form Assets

Gosford's existing built environment forms a strong base for the city to grow. Gosford is layered with unique urban assets including heritage buildings, rail infrastructure and a high street with a strong north-south axis through the centre of the city. Key building assets include the first Gosford Courthouse (1833) and the first Post Office (1835) to the south of Mann Street and St Mary's Church (1858) to the east of Gosford.

The train station and railway service which commenced operation in 1887, enabled development to grow around it. Retail and commercial sites followed, along with provisions for parking. Despite being visually obscured by the natural landscape, Gosford train station forms an important economic, social and physical connection to greater NSW and is a part of the city's historical and cultural identity.

The future potential of Gosford needs to grow from its existing built form assets and urban structure. It needs to utilise Mann Street as the heritage heart and connector, to enable a new life as a vibrant main street, with a new diversity of built form, fine urban grain and multi-programmed space. Similarly, the rail line should connect areas through the city, utilising new crossing points through the city.



Gosford Historical Image City Design

Historical Buildings

Throughout the city heritage buildings offer some diversity to the built form, and form a vital connection to the city's past. Unlike recently formed communities or centres, Gosford has a rich cultural and built history.



Gosford Railway Image Jenae Woodall

Rail Infrastructure

The railway is embedded in the evolution of Gosford. The tracks, power lines, and remnants from the steam trains are a part of the identity of the city, and an important link between the past and future of the city.



Gosford Railway Image City Design

Mann Street – a High Street

From past to present day Mann St functions as the civic main street for the city. The important structural element of the city can be strengthened as a place for the people.

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

Places and landmarks

As part of the engagement process, workshop attendees comprising members of the community identified places of significance and landmarks of the selected area in Gosford.

A key finding of the process was that community-chosen 'places' tended to be privately owned, implying a deficiency or need for quality public space in the urban environment. Some of the built environment facilities were considered interchangeable in 'places' and 'landmarks' categories; Gosford Station, Central Coast Stadium and the Waterfront precinct. This gives an indication of their importance, for both local use and tourism potential.

Places and destinations

- Gosford High School
- Henry Kendall High School
- Gosford Public School
- Gosford Hospital precinct
- Gosford Private Hospital
- Tafe
- Kibble Park
- Leagues Club Field
- Gosford Olympic Swimming Pool
- Gosford Golf Club
- Imperial Shopping Centre
- Leagues Club
- Tommy's Cafe

Landmarks

- Gosford Station
- Central Coast Stadium
- The Waterfront precinct
- President Hill Lookout
- Rumbalara Reserve
- Sea wall and marina
- Train Station (hidden landmark)



Photo with permission: Kibble Park. Image: James Wrenell
Central Coast Stadium. Image: Central Coast Council
Chris Peck. Image: James Wrenell



Regional Assets

Sporting, health and transport infrastructure assets contribute to the identity and definition of Gosford as a regional capital. The Central Coast Stadium, the Hospital Precinct and Gosford train station were identified in community consultations as three of Gosford's major landmarks. These major facilities are activity epicentres: forming and defining Gosford's point of difference in contrast to the low-density built environment distribution of the majority of the Central Coast.

Gosford is a local connector for the Central Coast community and region to access Newcastle and Sydney. It is in a prime location for future development, facilities and infrastructure to service the Central Coast region.

New large-scale investment in health, education and commerce will bring people from the region to the city centre, reinforcing the gravitas of the city and creating a demand for future facilities including retail, business, community, recreation and transport.



Central Coast Stadium. Image: Central Coast Mirror

Stadium

The iconic Central Coast Stadium is an important landmark for the city. The Stadium is highly visible at the entry to the city, and has an important link to the water views beyond.



Gosford Hospital Precinct. Image: Bruce Downer

Hospital

The hospital is an important investment in the future of Gosford. The site will provide a major source of employment in the city and will perform a critical regional function for the Central Coast.



Gosford train station. Image: Sally Ding

Rail Network

The rail network and Gosford Station are a critical regional asset for the area, drawing in commuters daily and linking Gosford to Sydney and Newcastle. The challenge will be to make this hidden asset and key connector a vital place for the community.

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

ACTIVE MOVEMENT: Key Issues

Gosford City is disconnected from its neighbouring communities.

ACTIVE MOVEMENT: Proposed

Develop an active movement network that is integrated with green infrastructure connecting the city with its landscape. Connect Gosford City with its neighbouring areas for recreation and everyday access.

Movement and Access

This series of diagrams explain the existing structure of Gosford, identifying key elements of access, public infrastructure and environment which can support positive change.

Active movement refers to the possibility to walk and cycle in an urban environment. Prioritising active movement serves to connect the city, integrate the community and promote individuals' health and well-being.



VEHICLE MOVEMENT: Key Issues

- Station Congestion
- Crossing Congestion
- Large Scale Parking Generators
- Existing Commuter Parking

Mann Street can become congested with vehicular traffic including buses that need to access the bus interchange, as well as cars, which use Mann Street as a regional through-link. This predominantly vehicular area limits the opportunity for pedestrian amenity and cycling. The concentration of schools and the hospital, together with regional vehicular movement to North Gosford and Wyong is also causing congestion near Etna Bridge.



VEHICLE MOVEMENT: Proposed

- Proposed Roads
- Ring Road Arrangement

Regional through movement should be re-routed out of Mann Street, protecting the city core for public amenity via a ring road arrangement. East-west connections should also be improved and increased, and Mann Street prioritised for short term access. Public transport options should be promoted, including exploring point to point rideshare systems that suit the dispersed settlement pattern of the region.



GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

Parking

The provision of adequate car parking in Gosford has historically been a contentious issue with an inherent conflict between the requirements for long-term parking for commuters and workers and short-term parking for retail and commercial activities. Particular parking issues include:

- the loss of Kibble short-term parking
- medium-term – State and Local Government parking stations
- long-term adaptive-reuse of buildings for parking
- state car park versus local government
- potential new car parking scheme.

The Gosford City Council recently commissioned a Parking Study which will be used to identify the needs and solutions to parking.

The long term goal under the NSW State Plan is to reduce the demand for parking and better manage existing parking infrastructure. The principle method for reduction of parking demand will be via an improved public transport system and alternate methods of access to the city.

Additional public parking sites have been identified to provide assistance in the development of a car parking strategy.

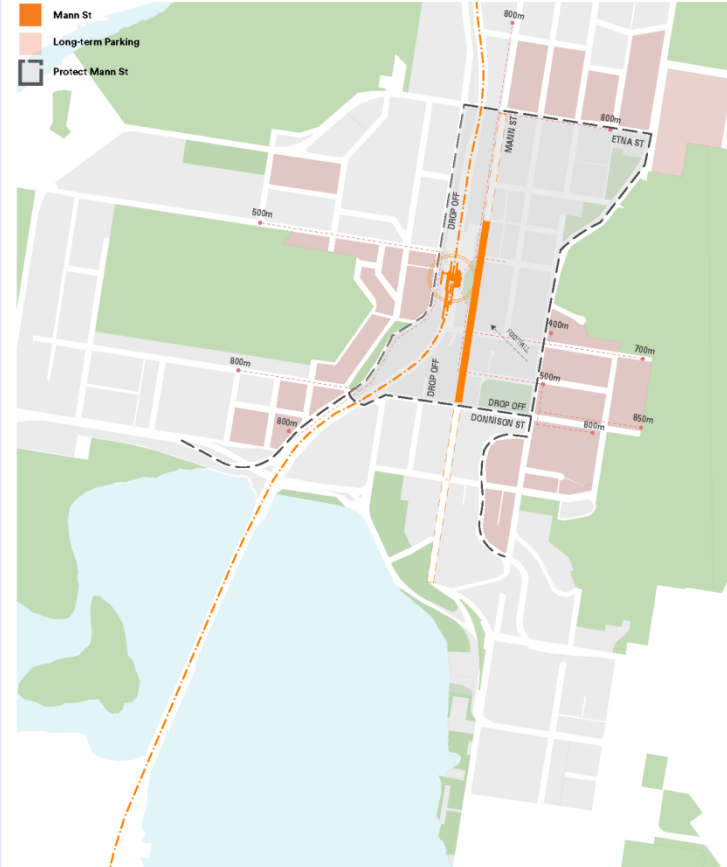
WATERFRONT PARKING: Proposed

The waterfront currently has limited land for parking and should be retained as a natural feature. Below depicts a parking solution with sites one block back from the water as possible, however undesirable, parking sites. Also key sites within a 800m walk from the waters edge offer alternative opportunities.



LONG-TERM PARKING: Proposed

Commuter parking offers the least contribution to an active CBD. Commuter parking should be moved out of the central city core, to sites within a 800-1000m or 10 minute walk to the station.



SHORT-TERM PARKING: Proposed

This frees the inner core of the city for short term parking for people who need to access the services of the city. However it is still important to protect Mann St as the high street for the people. As such no parking entries should be off Mann St or the key places such as Kibble Park.



Key Projects underway: Overview

Council's statutory controls for height and FSR allowed for a variety of building typologies to be constructed. The development industry as well as the former Gosford Council recognised that the controls did little to promote development. As a result, the Council formed a strategy of 'bonus scheme': development-stimulating incentives.

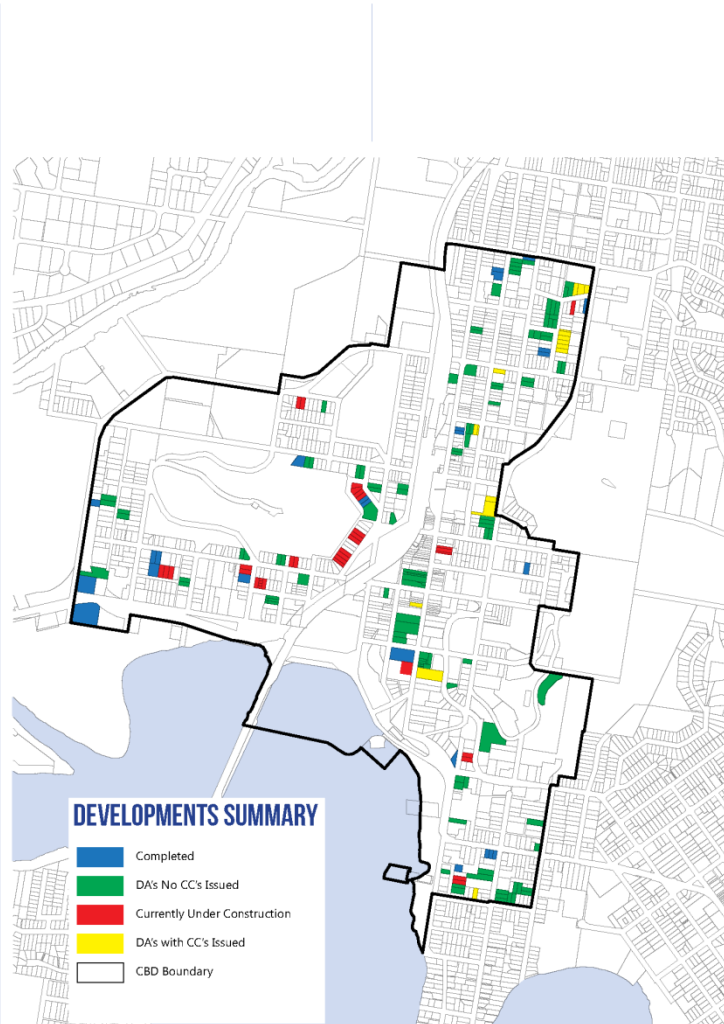
The principal scheme (2012-2016) encouraged development in the short-term; with 30% bonus for height and floorspace and reduced developer contributions from 4% to 1%, under Section 94A.

The scheme was extended for a further year with 30% bonus for height and floorspace and reduced developer contributions from 4% to 2%. The bonus expired on 2. April 2016.

Since the introduction of the 30% Bonus in 2012 there have been:

- 83 Development Approvals in Gosford City Centre
- 17 Developments Completed
- 13 Currently Under Construction (out of ground)
- 7 DA's with Construction Certificate Issued
- 46 DA's with No Construction Certificate Issued – Developers not Acting on Development Consents

*Data Source: Central Coast Council March 2018



*Source: Central Coast Council March 2018

Existing LEP controls include the following:

B3 commercial core + B4 Mixed use

predominantly land zoning

9m to 72m

maximum building height in proposed commercial core

2.5:1 to 6:1

Floor Space Ratio (FSR) range

Heritage items

General and archaeological



Image Source: Central Coast Council March 2018

*Data Source: Central Coast Council March 2018

Comparing the completed developments and developments under construction, there is no direct relationship to contribution rates and development completion/construction. Of 31 developments either completed or currently under construction; there were 14 with contributions of 1%, 3 with 2% and 14 with 4%. Council must make up the shortfall of any discounted contributions.

The symptoms of reduced contribution incentives become apparent in the lack of investment in the built environment. This affects the quality of public open space, community facilities, the public road network, drainage, water quality, environmental protection and public car parking.

Council has delivered the framework for change. Since the 30% bonus scheme was introduced, 4709 residential units have been approved in Gosford City Centre.

In summary:

- 4709 residential units have been approved
- 516 units are under construction
- 600 units have construction certificates issued
- 3055 units have not had construction certificates issued
- 269 units are complete.

The Central Coast Regional Plan identified that targets for Gosford were 10000 more people and 6000 more jobs. The new approved developments provide for 8429 people to be accommodated, according to the average household size for Gosford.

*Data Source: Central Coast Council March 2018

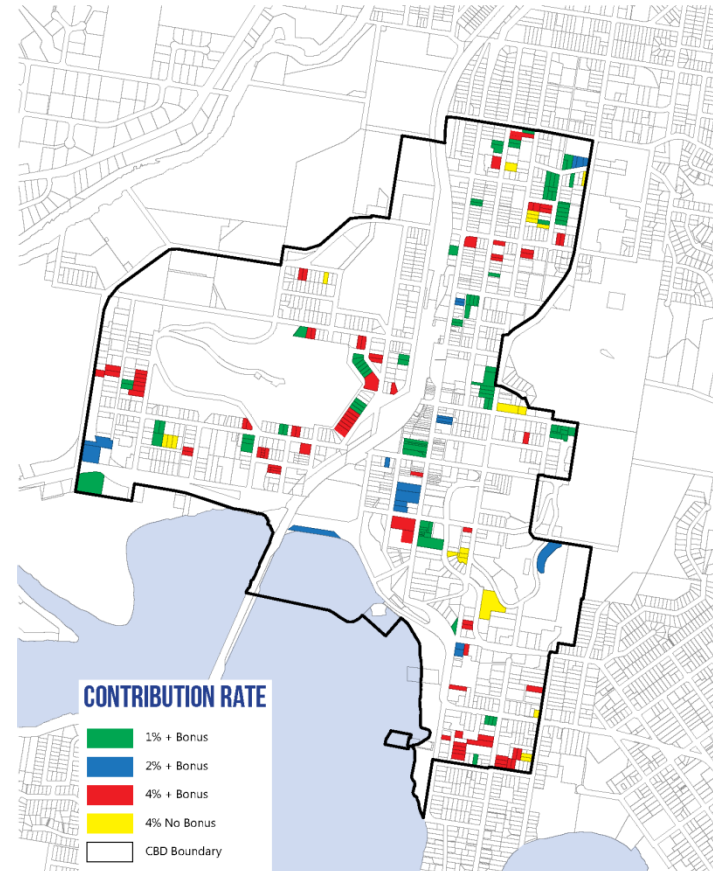


Image Source: Central Coast Council March 2018

**Key Projects underway:
Private Development**

In the last two years, the number of development applications have increased, concentrated parallel to Mann Street east and west and running south to Brisbane Water.

These private developments, as well as public investment signify massive change for Gosford. The city horizon is punctuated by cranes.

Adjacent are examples of the tallest buildings currently approved in Gosford.

Existing LEP controls in the UDIF include:

- 9m to 72m maximum building height in proposed commercial core
- B3 commercial core and mixed use land zoning
- 2.5:1 to 6:1 floor space ratio range
- General and archaeological heritage items



Image Source: CKDS Architecture

Bonython Towers

14-storey residential/retail/commercial development
 One tower to a height of 43m
 155-161 Mann Street
 CKDS Architecture
 56 apartment units
 62 parking spaces
 Granted development consent in December 2016
 Due for completion in 2018
 Under construction



Image Source: Dickson Rothschild Architects

Mariners Plaza

29-storey hotel/residential/retail development
 Twin towers to a height of 98m
 108-118 Mann Street, at the corner of Donnison Street
 Dickson Rothschild Architects
 342 apartments, 167 hotel rooms
 507 parking spaces
 Granted development consent in December 2016, due for completion in 2018



Image Source: Conybeare Morrison Architects

Gosford Alive

23-storey residential development stage 1 and
 20-storey commercial development stage 2
 Kibbleplex car park and imperial shopping centre
 Conybeare Morrison Architects
 730 apartments
 300+ car parks
 New shopping, food and entertainment
 Proposed renewal of the Kibble Park
 Application withdrawn early 2018



Image Source: CKDS Architecture

Waterside

25-storey hotel/residential/commercial development
 Three towers to a maximum height of 87m
 50-70 Mann Street, access to Georgiana and Baker Sts
 CKDS Architecture
 150 apartments
 no. carparks unknown
 Granted development consent in December 2015, due for stage 1 completion in 2020



Image Source: CKDS Architecture

Oracle

15-storey residential/commercial development
 Two towers and podium
 120-124 Erina St, intersection of Henry Parry Drive
 CKDS Architecture
 180 units
 no. carparks unknown
 Granted development consent in December 2016



Image Source: CKDS Architecture

Pinnacle

15-storey residential/commercial development
 Single tower and podium, height unknown
 21-23 Mann St
 CKDS Architecture
 140 apartments
 193 car parks
 Development status unknown



Image Source: Nettleton Architects

280 Mann St

24-storey residential development
 Single tower up to 78m in height
 280-300 Mann St
 Nettleton Architects
 no. apartments unknown
 340 car parks
 Granted development consent September 2017

Key Projects: State and Local Government



Image Source: Central Coast Local Health District

Gosford hospital

Located in the north west of the city centre, the hospital is set to be built by late 2019, with an overall investment of \$348M. The hospital will include 800 car spaces, expanded intensive care and emergency department, operating theatres and 680 beds, amongst other services. As part of an 'innovation precinct' the fully operational hospital estimates to bring another 200 equivalent full time jobs. It aims to join with Newcastle University to deliver a true innovation precinct built on health and education.



Top proposal Image Source: Central Coast Council

Regional library

Currently open to community feedback, this local government \$15M project aims to create a truly regional library located on Donnison St opposite Kibble Park. The concept design currently under review and costing includes high quality commercial and retail space, parking and best practice community library facilities. While the design of contemporary libraries are changing with technology, their role is pivotal in community life, offering services to all ages beyond the traditional role of it as a depository of books.



Image Source: Group GSA

Finance building

Developed by the State Government, the Finance Building will be located on southern Mann St near the ATO. It is scheduled to be built by July 2018 and is currently under construction. The building will include spaces for the Department of Finance, Services and Innovation and further A-grade commercial spaces, retail and dining. It will bring over 300 high-quality government jobs to Gosford.



Image Source: DP&L Library

Regional Performing Arts Centre (RPAC)

This local government project has time limited financial backing to create a 1000 seat theatre and regional performing arts centre. The centre may include conference facilities, cafe, and supporting performance spaces. The community is eager to see this project fulfilled, however a location for the centre is yet to be determined.



Image source B7N

ATO offices

This building, completed in late 2016 will centralise government jobs in Gosford. The building is located near Mann St on Georgiana Terrace, on part of the former Gosford Public School site. It will be home to 300 ATO staff members. The building has limited parking on-site.



Image S4ty Dingo

Olympic pool

Currently reaching its end of building life, the Gosford Olympic Pool is lacking in the vital amenities and services to suit local visitors. The pool currently generates 200 000 visitors per year, but has the potential to double this number. The location on the water's edge has great potential, but currently makes no connection to its views and location. Council is considering redevelopment or possible relocation of this pool.



Image S4ty Dingo

Reuse of existing library

With the formation of a new library, the existing library has the potential to be readapted for other public uses. The Council has been considering community uses, or the function of a Town Hall. The building is centrally located in the heart of Kibble Park on Donnison St. The building was finished 1973 and designed by Alan Williams and Associates. It is considered to be architecture of significance by the Australian Institute of Architects.



Image S4ty Dingo

Gosford Stadium

Gosford Stadium is situated on land reclaimed from Brisbane Water and marsh area near Gosford foreshore, formerly Grahame Park. Constructed in 1999, it has become a local landmark and a celebrated part of the identity of Gosford. The stadium is home to the Central Coast Mariners association football club and captures a spectacular view of the sports arena against a background of palm trees and Brisbane Water beyond.

3.5 Why a Place Based Approach

Based on what we've learnt from past reports and masterplans, as well as insights gained through the UDIF work, a targeted place based approach is recommended:

- Places are in strategic locations where there is a confluence of existing amenity, public space, public investment plans and private development opportunity. They build upon what's already happening.
- They are in locations that can drive footfall between them.
- It concentrates and 'gathers' investment around it. In turn, there is an opportunity for one investment to feed off another.
- It also concentrates activity on-ground.
- Low risk of implementation. Centres around a public space that can be delivered quickly by government to lead the renewal. The first move doesn't need private sector partnership, multi-agency agreements, etc. Relative to buildings, public space can create a greater visible transformation per dollar invested. In addition, approval path and construction times are likely to be quicker.
- The public space is to be a high quality regional attractor to strengthen Gosford's role in the region. This will also boost private investment viability around it by improving visual amenity and bringing people to the focus area.
- This provides government to lead conversation with developers who benefit from this public investment on VPAs – whether this is cash contribution, accommodation of key public functions such as TAFE or University, or encouraging land-use that strengthen Gosford as a regional centre.

By clustering investment around a public place, there's an opportunity to create focus, synergy and momentum – increasing visitation, strengthening viability for investment and provide government greater power to lead the market.

The large scale of the study area encompassing Gosford city centre necessitates a targeted approach to development and investment.

The diagrams adjacent depict the scale of Gosford city centre relative to Sydney, where a comparatively tight urban structure, short distances between transport modes and vibrant activated streetscapes are already established. Dealing with this scale of city centre requires key opportunities to be identified for a focused approach to activity and investment. Whilst activation of the waterfront is desirable, it will not necessarily help to activate the heart of the city. Gosford has the potential to have a series of activated and interesting places along a connected network. Gosford has many important ingredients including the northern anchor of a hospital and education region, a central station, Kibble Park in the heart of the city, and a beautiful waterfront with public open space.

Connected, with a network of pedestrian and cycle friendly routes, the city can focus on making these ingredients work to their full potential. As a network they are stronger than any single project or place.



Image: James Woodall

Creating three places

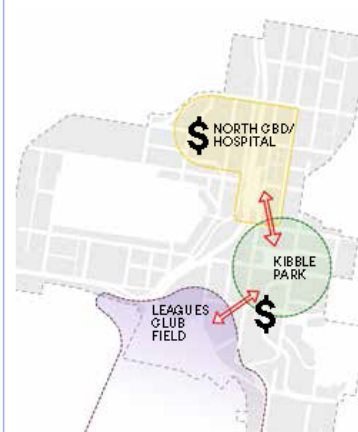
When we examine the scale of Gosford, we understand that the city is large and spread out relative to regional cities of similar population catchment. By creating key places within walking distance to each other, they become stepping stones to navigate across the city.

By starting at the centre of the city with Kibble Park, we can ensure all three places identified can be a connected network which encourage footfall between each other.

Creating a network of connected places allows us to cluster public domain improvements and investment in key areas of the city.



The hospital and waterfront are too far apart & disconnected to enable consolidation of the centre, foster movement and drive footfall. Visitors to each aren't likely to go to the other.



We need to create more attractors in the city to draw people in. Public investment where it can stimulate rich mix of private investment and city usage.



By focusing on places and not singular projects, we can create clusters of intensity that feed off one another and align to what is already happening.

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
3. The Urban Logic

The three places

The Civic Heart

Geographically in the centre of the city, this study area focuses around the open space of Kibble Park. This open space is adjacent to the existing Gosford Library and its proposed re-location, multiple government and administrative functions, and significant private developments like the Imperial Centre and Bonython Tower.

Kibble Park is a central meeting place that can bring the character of the bushland reserves into the heart of the city. The focus on this area is to create a centrally located, attractive and connected open space and town square.

North City

The North City area includes the expanded Gosford hospital site, the schools west of the station, the station park and the station precinct. This latter comprises the crossing of the railway line and includes the eastern side of the station.

The hospital is a major investment in the future of Gosford. By connecting the hospital to a re-imagined rail corridor and main street, its potential is expanded to start a new generation of innovation in the northern end of Gosford.

The focus on this area is to provide a gateway precinct with pedestrian connections, improved walkability and an attractive public domain.

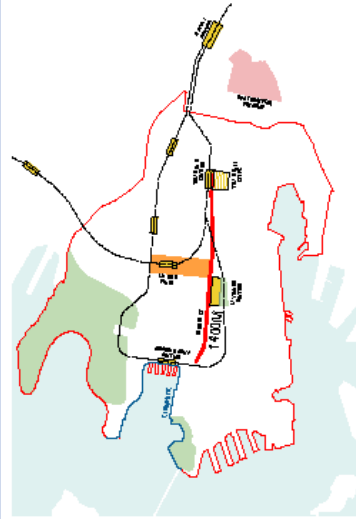
South City

The waterfront parklands bring the locals, the landscape and the water together. This area looks at the waterfront and its relation to Gosford City Park, the Leagues Club Park, Gosford stadium, the Australian Tax Office building, the Finance building and a potential Baker Street extension.

This is focused on providing a sense of place and a strong identity for Gosford. It will guide the opportunities to maximise the contribution of the next stage of development while ensuring public access and amenity. A place to arrive but also to spend time.

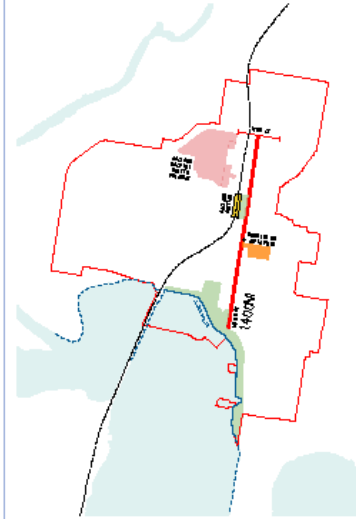


Three connected places for Gosford



Sydney

The walk from Town Hall to Circular Quay in Sydney, the nearest capital city, is 1.4km. Along the spine of George St there are anchoring districts such as the UTS education precinct and key public open spaces like Martin Place and Circular Quay.



Gosford

Gosford interestingly has many of the same potential ingredients along the 1.4km spine of Mann St. In the North there is a cluster of education and the hospital. Along the spine there is Kibble park, and the walk from the rail bridge to the end of the public waterfront at the Scoot hall is a similar distance to the walk between the Opera House and Harbour Bridge.



GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

04.
**THE
RECOMMENDATIONS
& IMPLEMENTATION
FRAMEWORK**



GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
4. Recommendations and implementation

4.1 State and regional level strategies and actions

4.1.1 Foster a place based approach and focus on public domain

4.1.1.1 Place based strategy

Develop place-based strategies to focus activity and investment in an around the priority places identified in the UDIF and along the routes that connect them.

Phase 1 – The Civic Heart

Phase 2 – North City

Phase 3 – South City

Noting detailed recommendations will be provided for discussion in the place base reports being prepared by GANSW for each of these places.

4.1.1.2 Place managers

Establish place managers for Gosford's key places to make the connection between people and places with a focus on use and activation and to support ongoing evolution over time.

Place managers would champion the implementation of the UDIF principles place-based reports and recommendations for their place in all decision making and have oversight of activity that has an impact on the place.

The Project for Public Spaces website provides more detail of the importance and opportunities of a placemaking approach.
<https://www.pps.org/category/placemaking>



Image: Jenae Woodall

4.1.2
Improve regional
access and movement

4.1.2.1 Prioritise and focus transport investment to improve connections for people across the Central Coast into Gosford to support its role as a regional capital.

4.1.2.2 Prioritise a joint place based integrated transport plan and car parking strategy.

Plan should coordinate existing workstreams including car parking strategy (Central Coast Council), Traffic Study (RMS/Central Coast Council), review of current development controls and s94A plan (Central Coast Council) and provide advice and recommendations including:

Options to fast track investigation of Central Coast rapid bus program including the Southern Growth Corridor to extend the 30-minute catchment for the regional capital (30-minute catchment referred to in Transport Futures/ Central Coast Council investigating options)

Extension of Shuttle bus services linked to new car parking options outside the city centre core

Reviewing and prioritising options for connecting people and places across the railway line

Addressing pinch points on the transport network – including congestion and safety issues at key locations for pedestrians, cars and buses

City Centre bypass options for through traffic including investigating linking north Gosford to Mann Road, providing an additional crossing of the rail line

Active transport priorities (building on work by Central Coast Council)

Movement and Place (TfNSW)

Prioritisation of projects and timing (all)

4.1.2.3 Fast-track joint DPE/Central Coast Council car parking review and strategy (to be developed jointly with transport plan above).

Identify short term opportunities for new all-day car parking outside the city centre core

Consider impact of increase the efficient use of existing car parking in the city centre to support the revitalisation of Gosford including differentiating the needs of commuters, residents, visitors and businesses and testing options for charging and options for increasing short stay parking

Review car parking standards for new development to feed into revising planning controls

Provide short, medium and long-term options for commuter and short stay parking.

Prepare short (0-3yrs), medium (3-5 yrs) and long-term (5+years) options for different users of the city (for example commuters, workers and visitors)

4.1.2.4 Continue to support and plan for reduce journey times by rail to Sydney from the Central Coast and particularly Gosford (as outlined in Transport Futures).

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK
4. Recommendations and implementation

4.2 Leadership, coordination and integration

4.2.1 Leadership and coordination

4.2.1.1 Build on the existing role of the Coordinator General to maintain momentum across all levels of government and ensure a whole of government focus on Gosford as the regional capital of the Central Coast.

4.2.1.2 Manage development
Establish a case management system appointing development managers with case, planning and design experience to manage major applications through the pre-application and development application process including agency referrals, discussion on VPAs and opportunities identified in the UDIF and subsequent works streams. The focus of the development managers would be on delivering development opportunities that support the principles of the UDIF and key places.

4.2.1.3 Coordinate and support delivery, including the activity around Government land and projects (e.g. CCRDC) and provide the appropriate resources and powers to enable it to champion, manage and deliver this ambitious program.



Image: Jaxxon '16.mxd

4.2.2 Streamlining and de-risking the planning process

4.2.2.1 Provide statutory weight to the urban design principles promoted in the UDIF in planning controls for Gosford to focus and guide public and private sector investment, help shape development projects and reduce uncertainty, provide a framework to inform more detailed guidelines and developer contribution/ VPA process, raise aspirations.

4.2.2.2 Establish a Government Architect led or endorsed Design Review Panel (e.g. sub State Design Review Panel) that will work to the principles of the UDIF and provide advice early (pre-application) to developers and consent authorities on development outcomes in the city centre.

4.2.2.3 Work with relevant agencies such as RMS, utility providers, Sydney Trains and others to develop fast-track referral processes for development in the city centre.

4.2.2.4 Implement simplified flexible and streamlined planning controls for Gosford central business district. Detailed and spatial recommendation are provided in the Place Based Urban Design Reports.

Implement a new, simplified streamlined set of planning controls for Gosford central business district to permit mixed use precincts whilst also protecting key areas of the commercial core to ensure the core functions of a regional capital have sufficient space to grow.

Provide flexibility in controls to enable negotiated development outcomes and promote more people working and living in the City Centre and enable flexible development outcomes that meet the principles of the UDIF/design principles.

Develop a range of strategies and incentives to attract additional commercial development/ activity. This could include development provisions to ensure a minimum percentage of core commercial/retail uses in appropriate areas and on key sites which can support large floor plates.

Create and incentivise through the planning system innovative models for employment including smart work hubs, public Wi-Fi access, alternative workspace models.

Facilitate the development of landmark buildings as a catalyst for growth in certain locations which will be identified through the detailed Urban Design study process.

Require public benefits/ amenity improvements as part of all development applications.

Support the provision of a mix of housing including low cost rental to support the function and role of the City Centre, particularly at the northern end of Mann Street near the station, schools and hospital.

Provide incentives for the provision of cultural facilities and uses of public benefit in development proposals by excluding in FSR calculations (merit based assessment based on urban design principles identified in the UDIF and future guidelines would still apply).

Protect solar access to public places – 50% of total area to receive sunlight for 4 hours between 9am and 3pm on 21st June.

Require the activation of street frontages and encourage the use of footpath for outdoor seating and display of goods (where the footpath depth allows it).

Require design excellence competitions for major developments in accordance with state policy.

Introduce on-site maximum standards for car parking and plan for the provision of car share spaces, increased bicycle and motorbike parking.

Remove the requirement for a minimum 24m frontage for permissible development to allow smaller and fine grain development to occur.

Identify the bulk and scale that reflect the intentions of the urban study areas and which can translate into relevant planning controls.

Develop place and precinct specific principles and controls.

Expand B4 zone to allow flexibility but still provide for commercial development.

**4.2.3
Infrastructure requirements and developer contributions**

4.2.3.1 Prepare an updated schedule of infrastructure projects and public domain works to increase certainty about the provision of infrastructure and the cost to developers included updated public realm works, green infrastructure requirements, new infrastructure identified through the detailed place based urban to meet ambition for delivery of public domain improvements and the like.

4.2.3.2 Consider a prepare a mix of state and local infrastructure contributions based on the updated schedule of infrastructure above.

4.2.3.3 Finalise the Central Coast Council review of the performance of the existing S94A plan and other approaches to developer contribution.

Identify:
— what funding has been collected
— what funding is available, and
— what it can be allocated towards.

Forecast what funds will be potentially collected with current approvals pipeline and provide transparency on where contributions are being spent.

4.2.3.4 Develop a plan to spend current funding on projects identified in the UDIF that will have a high impact and build confidence.



GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

05. PROCESS

Good design processes are dependent on establishing: a clear understanding of the need or problem to be addressed; good analysis and research; and testing through a process of open ended enquiry in order to identify opportunities.

Good design processes bring together a diverse group of people with different points of view. Through identifying and articulating differences of opinion, good processes foster a common understanding to establish a shared future vision.

Stakeholder and community engagement, at the earliest stages of projects embeds good process in the UDIF. This enables more considered and inclusive proposals, better suited to place and people.

5.1 Background to the process

“Design thinking is a creative skill where designers make new connections and test and retest ideas in order to find a better outcome for the problem at hand and anticipate future problems.”
 — GANSW

The revitalisation of Gosford City Centre has been initiated in the past through plans and strategies undertaken by the NSW Government and former Gosford City Council. This included the 2010 Our City, Our Destiny Masterplan and the 2007 Revitalising Gosford City Centre Plan. These previous plans also included significant engagement, focused on a well-considered vision for Gosford City Centre.

However, the plans have not led to a step change in the quality of the public domain or a sense of place.

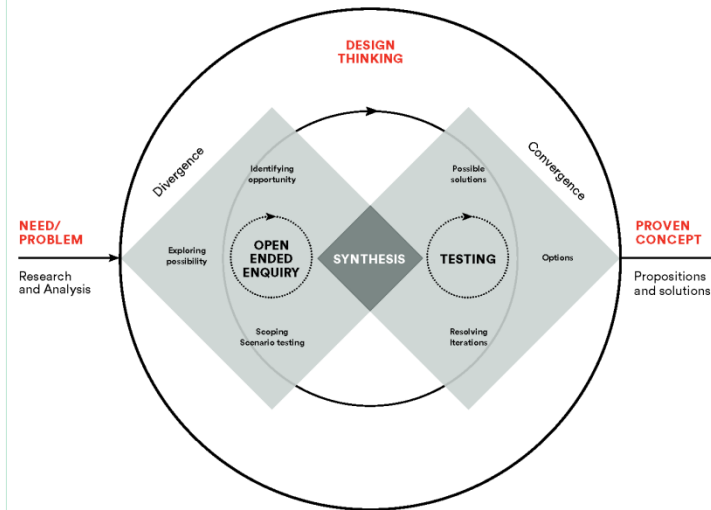
There remains a run-down appearance in the buildings and vacant shops, poor public domain and way finding, poor pedestrian amenity and connectivity with surrounding areas. A critical feature missing from previous plans was delivery ‘buy-in’ for projects identified through the process.

The need for a new approach to the revitalisation of Gosford City Centre has arisen from the growing consensus that a vision without a framework for implementation will not lead to success and that great places and cities do not happen by chance.

They are designed and continued to be designed to manage their transformation over time. Engaging with stakeholders and communities, undertaking urban design analysis and developing strategies and recommendations for collaborative implementation fosters confidence, development and assist in aligning investment.

The scale and complexity of both the barriers and opportunities in Gosford has required a focus on divergent, open-ended enquiry. Workshops were held throughout this project with the following objectives:

- To hear and learn from a diverse set of stakeholders including state and local government and the wider community in a ‘safe’ space.
- To collect detailed information and evidence from all stakeholders.
- To engage all stakeholders in active, participatory design enquiry exercises to challenge their thinking and broaden their exposure and acceptance of many different points of view.
- To break down silos of thinking and organisational structures, to find common goals and synergistic projects and approaches that can have a broader impact.
- To inform the community of this work, the role of the Coordinator General and GANSW.
- To share best practice, raise aspirations and empower stakeholders with new ideas.
- To test out and challenge preliminary findings and recommendations of this work with stakeholders.
- To focus attention on the key places in Gosford.
- To empower stakeholders with the idea that everyone has the ability to cause change and have a positive impact in their community.
- To reframe and change Gosford by looking at it from a different standpoint. This involved working outside the ‘rules’ or the ‘impossible’ and putting aside some of the extensive past work in Gosford.
- Establish champions for the project and Gosford.



Design thinking:
 Design thinking offers a holistic way of finding opportunities and resolving process problems. It is the method in which designers seek to integrate possibilities and to generate new ideas. It is a creative process that seeks answers through collaboration and synthesis of multiple inputs. Design thinking is a creative skill where designers make new connections, test and retest ideas, and anticipate future challenges in order to find a better outcome for the problem at hand.

Open Enquiry Workshops

5.2 The approach to Open Enquiry Workshops

Workshop format

All workshops followed a similar format yet were reactive to the people and outcomes of each day.

Design activities aimed to gain ideas and thinking from a broad group of participants. Each day was mediated by the Deputy Government Architect and facilitated by a team of design consultants from GANSW, CHROFI, and Tyrrell Studio. Workshops sought to identify commonly held aspirations for Gosford and the strategic steps the community will need to take to achieve them.

Each workshop included a subset of the following agenda items and topics for discussion, depending on the participants and the stage of the process:

1. General introduction by Central Coast Coordinator General and or the Director of Operations, Central Coast.
2. General introduction to the process by the Deputy Government Architect.
3. Presentation by CHROFI on what we already know and early thinking.
4. Participatory Design Exercises and Mapping:
 - Places and Landmarks in Gosford
 - How can we improve the places in Gosford?
 - How do people access and use Gosford?
 - How can we make Gosford an attractive regional Capital?
 - Barriers and opportunities for development and business in Gosford.
5. Presentations and Precedents:
 - Walkable cities and green infrastructure – Tyrrell Studio and GANSW
 - Changing regional Centres (Turramurra and Maitland) CHROFI
 - Market overview – Hill PDA

“Cities have the capability of providing something for everybody, only because, and only when, they are created by everybody.”

— Jane Jacobs

The workshop process has benefited from, and been informed by extensive previous work, including numerous studies and extensive consultation. Work was undertaken with stakeholders to identify what has changed. New ideas and programs of work have confirmed existing opportunities and constraints. As projects developed, ideas were tested simultaneously.

The workshops have been a key part of understanding how to proceed. The process facilitated the sharing of ideas, expertise and knowledge where key businesses, government agencies, representatives from Central Coast Council (both councillors and staff) and community groups were represented.

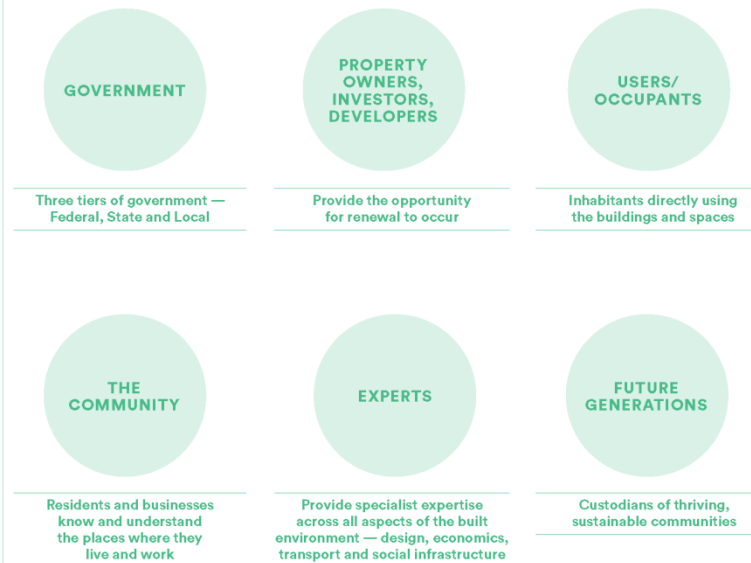
Wider community engagement through the Gosford surveys were able to gauge support for and relevance of initiatives towards the revitalisation of Gosford.

Key issues raised consistently across both the workshops and surveys included inconsistent planning and development, car parking, travel to the city centre, quality of public domain as well as activities available once at the centre.

Over 230 targeted key stakeholders participated across 7 workshops (between November 8, 2017 and April 3, 2018) including:

- Workshop 1: State Government
- Workshop 2: Local Government
- Workshop 3: Local and State Government
- Workshop 4: The Development Industry
- Workshop 5: Community Representatives
- Workshop 6: Council, Transport for New South Wales, Roads and Maritime services
- Workshop 7: Central Coast Councillors

Who is involved in creating place:



The process engaged a range of people involved in creating place

**Workshop 1
What We Heard From
State Agencies**

Participating agencies

The first workshop brought together representatives from the following government agencies:

- NSW Police
- Property NSW
- Office of Environment and Heritage
- Transport NSW
- Roads and Maritime Services
- Crown Lands
- Department of Premier and Cabinet: Centre for Economic and Regional Development
- TAFE NSW
- NSW Health – Local Health District
- NSW Health District Redevelopment (Gosford Hospital)
- Department of Planning and Environment
- School Infrastructure NSW

Findings from Places and Landmarks Mapping

This workshop mapped places and landmarks that were considered important by the participants. Those identified were:

Places and destinations:

- Kibble Park (unique green space with potential)
- Stadium
- Imperial Shopping Centre
- Burns Park (not a desirable place at the moment)
- Leagues Club Park
- Gosford Public School, Henry Kendall High School and Gosford High School
- Hospital – future potential place
- Tommy's cafe – an example of small scale speciality and high quality retail starting to emerge
- Gosford Olympic Pool.

Landmarks:

- Stadium
- Sea wall and marina (not a place as little to do there)
- The water
- Imperial shopping centre as a meeting place and landmark
- Rumbalara Reserve and Presidents Hill Lookout
- Train station, hidden landmark

KEY FINDINGS:

Uncertainty regarding development approval time frames and the delivery of projects is a detrimental investment decisions.

The general sentiment of the participants was wariness of the numerous plans completed for Gosford. The consensus is that it is time for action.

There is an overall positive and optimistic outlook for the future of Gosford, particularly given the number of cranes now in the cityscape.

Significant public investment is underway with strong employment opportunities with the development of the hospital and surrounding medical precinct, Australian Tax Office site and Finance Building at 32 Mann St.

There is potential for further government agencies to relocate to Gosford.

There is a lack of high quality housing options and lifestyle offering (hospitality and entertainment), limiting the attraction of high quality staff to hospital and supporting medical services.

Medical-related businesses are looking for quality premises in the vicinity of the Hospital.

There is a lack of high quality commercial space, limiting options for government agencies and other private businesses to be located in Gosford.

A perceived lack of leadership and cross-departmental collaboration might be limiting some unique opportunities.

There is great potential for Gosford to offer a high quality lifestyle with good links to Sydney, good transport, and a strong sense of community.

Potential for TAFE to tailor their offering to suit the medical and educational focus of new jobs in the area.

Public transport and road connectivity is a key concern, requiring further investigation.

Strong opportunities exist to engage with the selective high school, an important community and key asset in the area.

Government have had unsuccessful relationships with existing commercial landlords, with poor quality building stock and limited supply creating issues for provision of high quality government services.

Larger development plans for the waterfront have faced opposition from the public, with a strong focus on keeping this asset public in nature and use.

Education including schools, TAFE and university are real opportunities for Gosford.

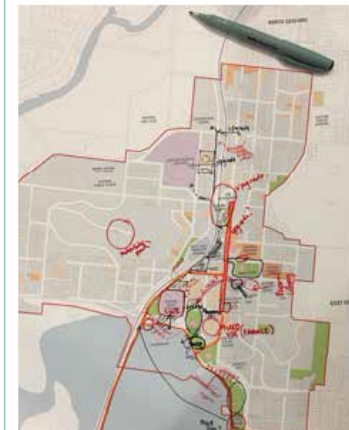
Currently, students at both TAFE and schools have a limited impact on activating the city centre because of the location of these. A university is interested in city centre presence and establishing a facility linked to the expanded hospital.

There is potential conflict between vehicular traffic and pedestrian movement between schools, station and hospital.

The Office of Regional Development has received enquiries from large private sector companies wanting to relocate to Gosford or the Central Coast. These companies are often unable to find existing space in the region or wait the 2-3 years for new space to be created.

An inconsistent approach to planning and development over time.

Image: Sully Diego



Workshop 2
What we heard from
Central Coast Council

Participating agencies

The following attendees from Local Government, as well as local residents, brought perspective to this discussion.

Representatives from the Executive Team from:

- Central Coast Council
- Planning and Environment
- Assets Infrastructure and Business
- Connected Communities

Relevant teams including:

- Strategic Planning
- Leisure and Lifestyle
- Community Engagement and Partnership
- Libraries
- Water and Sewer
- Roads, Transport and Drainage
- Business and Economic Development
- Environmental Strategies
- Land Use and Policy
- Urban Growth Strategies

KEY FINDINGS:

There is significant optimism for the future of Gosford.

Cranes in the sky are a symbol of change.

The Central Coast region is 3/4 the size of Sydney, and the 6th largest council region in Australia.

There are key significant projects coming soon with council support. Many are at preliminary concept stage, and some have funding and delivery mechanisms underway. Not everyone was aware of all the projects on the drawing board. The opportunity to coordinate and build on existing work was recognised with council but also with NSW Government.

The regional library is a significant project underway yet is not widely known about. Other key projects raised included the Olympic Pool and cycle way to Point Clare.

There was a sense that council has been waiting for the DAs submitted to be built to collect on the \$94 contributions to start the public domain upgrades. There was recognition that an upgraded public domain was of high priority to attract people and investment.

Homelessness and domestic violence are significant issues for the area.

Strong community support for growth in tourism but limited support for new development.

Council events in Kibble Park and the waterfront are limited by a lack of infrastructure including, power, lighting, water and amenities.

OPPORTUNITIES:

To provide an experience unlike anywhere else on the Central Coast – an urban hub.

To spark market confidence.

Increase population.

Create more dwellings/short stay accommodation.

Activate streets (like Mann St) and shopfronts.

Art or historic walk utilising empty shopfronts.

Include community to inspire public investment.

Design excellence required.

Link to Point Clare – bike and pedestrian pathway.

Connect to the reserves, and make destinations for tourists.

To rethink Gosford Station Interchange.

To upgrade Kibble Park and William St Plaza.

To connect the station to the Hospital.

To link the stadium to the water.

SUCCESSES:

DAs being approved.

Signs of new businesses, cafes and start-ups, etc,

Fast broadband in the area.

The smartwork hubs are working well.

The new Aqua Park is drawing in visitors. Some events are working well, like Carols by Candlelight, the Food truck festival, and plant based Food festival.

CHALLENGES:

Lack of community pride.

Lack of connection (CBD to waterfront/cultural precinct).

Lack of visitor amenities and accommodation.

Safety / night time activation.

Land / shopfront tenancy owners with empty properties not releasing their holdings for rent.

Poor building stock.

Feasibility of getting things built.

Lack of \$94 funding.



Image: Bruce Dinn

**Workshop 3
Bringing State and Local
Government agencies together.**

The key State and Local Government attendees from workshop 1 and 2 were invited to return and participate in this combined workshop. Overall, 40 people attended an event hosted in the Gosford Smart Work Hub. This workshop aimed to breakdown silos of activity to find common goals and projects that could benefit from collaboration.

This workshop introduced and tested 4 key places:
— The hospital and station
— Kibble Park and Mann St
— Gosford City Park and surrounding developments
— The waterfront

One of the key tasks of the workshop was to identify priority projects including those that could be delivered quickly.

The workshop also focused on:

1. Integrated access and movement
2. Green grid opportunities for Gosford
3. Stewardship, identifying who maintains projects and supporting action

KEY FINDINGS:

Past failures of the prior master plan implementation can be attributed to there being no shared vision, a lack of leadership and the voice of all the key stakeholders not being represented adequately.

Identity and brand, a clear image of what the city could be, all year round, linked to a sense of history, including indigenous history.

Gosford is the place of choice for business as the regional centre of the Central Coast.

There needs to be a network of attractions and things to do.

A conference centre with accommodation is needed.

Activities in the natural environment must be accessible.

Test concepts and ideas with short term pop-ups and small scale developments to help the activation 'snowball'.

Consider opportunities for young people.

Gather quality food offering in a primary 'Eat Street'.

Incentives to help fill vacant shops for vibrant high streets.

A short, medium, and long-term parking strategy for commuters and short visits.

The need for collaboration, respect, and transparency.

Audit of existing assets necessary.

An access and movement study necessary.

24 / 7 activation.

Connections and networks of places, businesses and events needed.

Quality, premier destinations to be clustered to create desirable destination precincts such as: a conference centre and hotel; regional playground, water play or creek project; stadium; markets; pop-up; health; all levels of education; and innovation or start-ups.



GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
5. Process



What will make Gosford a liveable city?

- Strong image and branding
- Range of opportunities for young people
- New development – good quality building stock for residential, commercial and retail.
- Reasons to visit: programming and events
- 24hr – safe city
- Range of education opportunities
- Arts and culture
- Celebrating history and tradition
- Harnessing natural beauty and surrounds – including water, Presidents Lookout and Rumbalara.
- Better services and amenity – retail, hotels and food and beverage.
- Intergenerational community
- Creating a “playground” – a network of activities and things to do.
- “Pop-ups” – testing ground for new ideas while limiting risk.
- Consider big ideas like swimming in Brisbane Waters



What do we need to do?

- Establish a shared vision
- Agree on key messages
- Promote identity and what Gosford has to offer

Major public and private projects that need to be capitalised on and promoted:

- ATO / Finance Building
- Residential projects
- RPACC / regional library
- Hospital

Small scale public and private investment opportunities to be promoted:

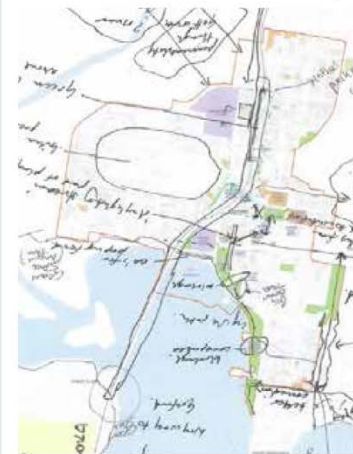
- Pop ups / testing ideas
- Local business success stories
- Public works upgrades
- Private residential and mixed use developments have started



Missing links / key blockages

- Connection between Hospital and city centre
- Presidents Hill is hard to locate, needs to connect to the city
- Congestion at the stadium on event days – poor connection to city centre
- Unsafe road network
- City is not walkable
- Bike and footpath

Examples of the key maps produced in workshop 03.



Creating a connected city centre – future opportunities:

- Developing walking and cycling plans
- Improved public domain
- Mountain bike track
- Land bridge over rail corridor
- Attract local and greater Sydney tourists – for example a ‘Bay to Bay’ run
- Etna St and Donnison St Bridge upgrades
- Amenities, food, beverage at the Leagues Club Field – opportunities for local businesses.
- Connecting the hospital to Point Claire along the rail corridor
- Connect shoreline from Elizabeth Ross Park to Gosford waterfront
- Regional walk from Rumbalara Reserve to Katandra

Workshop 4
What we heard from the property and development industry.

- Participating businesses:**
 The fourth workshop brought together representatives from the following design, development and property businesses with attendees organised by the local study group of the Australian Property Industry.
- MUDO
 - Robertson and Robertson
 - R+H Commercial Central Coast
 - Chapman and Frazer
 - CBRE
 - Matrix
 - Commercial HQ
 - ADG Architects
 - CKDS Architecture
 - Baker Ryan Stewart
 - ADW Johnson
 - Central Real
 - Central Element
 - Bonython Property and Investments
 - EC Property
 - Pinnacle Construction Groups
 - St Hilliers
 - Ledener Group
 - Stevens Group
 - NAB
 - WW And Associates
 - Foundation President UDIA

KEY FINDINGS:

The market would respond positively to improvements to the city centre's public domain to make Gosford a place where people want to live, work and invest.

Attractions, events, cafes, parks and the waterfront are all needed to make Gosford desirable.

Development margins are tight and the bonus floor space and s94 reduction incentive scheme is needed to make projects economically viable.

Approvals are not all land banking, and developers are keen to build. It was also noted that a number of DA approved properties were being sold shortly after approval was granted.

The development industry needs pre-commitment from business to provide new A grade office space. Generally they need an area of 5000-10000m² with a minimum 10 year commitment.

Prospective tenants and buyers often want to see the space already built to consider Gosford as an alternative to Sydney, and don't have the lead times to wait for the new space to be built.

Many opportunity sites in Gosford and many good developers ready to work in the city. New land release areas across the Central Coast are adding competition to the residential market in Gosford.

Some DAs have been lodged and approved with a focus on maximising yield but lack quality and require redesign before development commences. The developers need time and a mechanism to improve the design before the approvals lapse to allow for redesign and to maintain the incentive bonus floor space.

Developers are happy to contribute to public realm upgrades through s94 contributions – but want transparency about how and when the contributions are spent.

There is a shortage of experienced builders on the Central Coast due to the demands of Sydney.

Developers highlighted the fact that many developments need to provide their own power substation. This increases costs for the developer and has a poor public domain outcome at the street level.

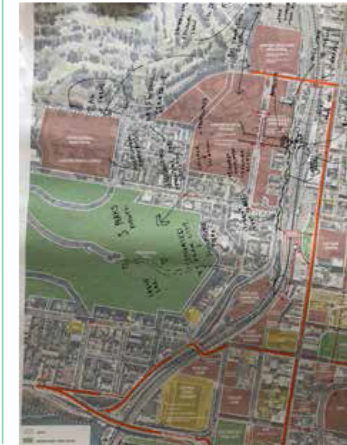
City living in Gosford can be popular as it offers convenience for services, good public transport connections, a coastal lifestyle, water views and the opportunity to downsize out of busy Sydney.

The development industry is keen to better understand the property demands that may come with a health and education innovation precinct.

Currently, the market demands two parking spaces per apartment due to the heavy reliance on cars in Gosford. This adds cost and further congestion in the city. An improved public transport network is needed. An alternative to underground car parking could include central car parking and car share spaces.

A one hour train journey to Sydney would greatly improve demand for public transport – noting that Gosford and Penrith have the same journey times currently.

An inconsistent approach to planning and development over time.



GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
5. Process

Workshop 5
What we heard from community representatives.

Over 50 key community stakeholders attended this workshop, representing diverse interest groups including business, media, community groups, students, not for profits organisations, education, traditional owners and the Mayor (Central Coast Council).

Workshop 5 heard from a cross section of leaders in the community. Attendees represented key businesses, community groups, land owners and cultural groups and was one of the largest workshops held.

These community representatives shared valuable insights and detailed information on their understanding of the city and provided feedback on the suggested place based approach.

Representatives were asked to consider what would make a successful regional capital, and what Gosford needed to be a more reactivated and lively city. The key places of Kibble Park, the Leagues Club Field, the waterfront, and the hospital/school area were analysed in detail for opportunities and constraints and to capture projects or initiatives that were underway that might help make great places.

KEY FINDINGS:

Overall the city needs to focus on its connectivity and inclusiveness. Accessibility across the city was a key theme with many missing links and connections mapped by participants.

Upgrading the public domain is critical to make a place people want to visit, stay and live.

Concern was raised, that many of Gosford's former uses had moved out to Erina, Tuggerah, Terrigal and other centres. It was important to bring the services and specialised businesses back to the capital.

Links to the existing bushland reserves were missing. There was an opportunity for a clear identity and events programme focused on the natural assets and the connection to the city centre.

The school students and hospital workers needed a safe walking route to the station.

Overall wayfinding strategies were needed across the city.

The future potential of the racecourse and showground needed consideration.

Generally participants recognised traffic as an issue. Despite living close to the city centre many participants drove to work in the CBD. Public transport and active transport links were needed to provide an alternative.

The community understood the importance of Kibble Park, and again identified it as the heart of the community.

Waterfront suggestions focused on improved connectivity, amenities, and places to stay by the water; with either a playground, cycle and walking path, or areas for food and beverage.



Inspire Sally Ding

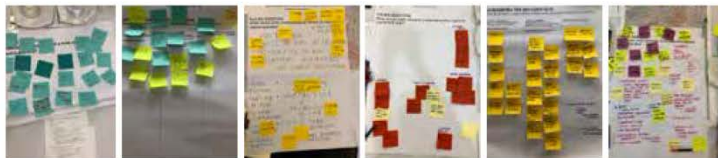


Priorities – the summary

This list summarises the top 25 ideas from the workshop attendees in answer to the questions:

What would make Gosford a more attractive regional capital and why? What might make people stay longer or even live in the city centre?

These comprise the items mentioned four times or more, with the top five ideas marked red.



GREEN LINKS

Trees (marked red) | More parks and gardens, better quality existing parks, botanical gardens (marked grey)

PARKING

More short term parking (currently used by commuters) (marked grey)

TRANSPORT OPTIONS AND ACCESSIBILITY

Better public transport options, other than the car, more direct bus routes (marked red)
 Ferry Service to Woy Woy and Sydney (marked grey)
 Better pedestrian access and accessible footpaths for all (will help things feel closer) (marked red)
 Reduce through traffic on Mann St, including buses. Make Mann St for people (marked grey)
 Better linkages over the station, and over the rail line (marked grey)
 Better bike access and bike paths, bike hire, Point Clare cycle link (marked grey)

SAFETY

Improved safety – particular at night, and for women (marked grey)

DESIGN

Improved public domain and streetscape, public art, better footpaths, bins (marked red)
 Quality first, not low budget, which will entice further investment (marked grey)
 A welcoming entrance, better arrival for kids (marked grey)

SOCIAL

Diversity of users (youth, aging population) (marked grey)

PROGRAMME AND ATTRACTORS

Active usages at all hours, 7 days (marked grey)
 Night life (after 5pm), including small bars (marked red)
 Support Gosford as a cultural capital with diverse arts and music (marked grey)
 Space for sports and outdoor recreation (marked grey)
 Family playground with BBQs, shelter, preferably on the water (marked grey)

IDENTITY

A big clear idea, not band-aid solutions (marked grey)

ECONOMY

Provide quality commercial centre strengthen to compete with other centres (marked grey)
 More variety of retail and jobs (marked grey)
 More high quality shops, culture and arts (marked grey)
 Options for food and beverage after events, e.g.: a Saturday Game at the stadium, on the waterfront (marked grey)

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
5. Process

Priorities – the Long list

This list records the key priorities identified by all workshop attendees on:

- What would make Gosford a more attractive regional capital and why?
- What might make people stay longer or even live in the city centre?

GREEN LINKS

Link reserves to the city	
Trees	
More parks and gardens, better quality existing parks, botanical gardens	
Fountains that work	
Retail the natural ecology	
Better connections to Rambulara Reserve and Presidents Hill	
Improved look out at Rambulara Reserve	
Bay to Bay type run that is links recreation and fitness to the environment, treks	
Consider rising sea levels	
Greenscape on the showground	

PARKING

Parking at night (noting council car park at Baker St shuts at 9pm)	
Parking (for events on the waterfront)	
More short term parking (currently used by commuters)	
Consider charging for parking (debated)	
More commuter parking, parking for workers	

TRANSPORT OPTIONS & ACCESSIBILITY

Better public transport options, other than the car, more direct bus routes	
Better connections to the north and Laycock Street Theatre	
Ferry Service to Woy Woy and Sydney	
Marina for boats, may require dredging	
Better pedestrian access and accessible footpaths for all (will help things feel closer)	
Water access is too distant improve the connection	
Bus interchange at Baker St car park	
Connecting Hospital to the city and city to the water	
Reduce through traffic on Mann St, including buses. Make Mann St for people	
Better signage and wayfinding	
Improved train services	
Better linkages over the station, and over the rail line	

Each grey square represents the relative number of mentions each issue had during the workshop. The top 25 Ideas are shown in red and is summarised of the on the previous page.

Pedestrian access around stadium and CC Leagues club sites	
Better bike access and bike paths, bike hire, point dare cycle link	
A better station – consider Chatswood as a place	
Light rail	
Infrastructure to feed in and be coordinated	

SAFETY

Improved safety – particular at night, and for women	
Passive surveillance	
Lighting	

DESIGN

A distinct design style, not cookie cutter	
Improved public domain and streetscape, public art, better footpaths, bins,	
Make use of the Gosford Sandstone	
Integration of arts and design, sculptural trail	
Quality first, not low budget, which will entice further investment	
A welcoming entrance, better arrival for kids	
Wider footpaths	
Link people to specific quality of place	
Improve empty development sites	
Protect visual access to the water	

SOCIAL

Diversity of users (youth, aging population)	
A place that cares for the least empowered in our region and supports all levels of society, including homeless	
Improved social agenda, mental health, bring everyone along	
Social services can cause issues, consider solutions with the community	
Leadership at the local level	
More people will attract more people	
Indigenous history of President's Hill and other locations respectfully displayed	
Free Wi-Fi across the city for leisure and study – to improve public spaces	

PROGRAMME & ATTRACTORS

Active usages at all hours, 7 days	
Night life (after 5pm), including small bars	
Recreation hub / upgrade the pool	
Drawcard for tourists	
Support Gosford as a cultural capital with diverse arts and music	
Cultural Centre for the region, appropriately funded	
A cultural hub linked to the library	
Tertiary Institutions – Smart Economy / Smart City Strategy	
Library	
Startup/ Entrepreneurial Hubs	
Bigger school	
A town hall / community centre	
Conference facilities	
A network of art spaces to connect the art community, link to education	
Accommodation for visitors	
Youth place to occupy, youth entertainment	
Space for sports and outdoor recreation	
Water play / lagoon	
Improve Kibble Park – only half functional, including the fountain, shade	
An iconic performance space on the water, and smaller 300 seat venue for local artists	
Boat museum	
Power to help support pop-ups or events on the harbour or Kibble Park	
Coordinated activities and events	
Family playground with BBQs, shelter, preferably on the water	
Consider a new future for the race course and show ground – residential?	

IDENTITY

Change the mindset that Gosford is the capital	
A big clear idea, not band-aid solutions	
Action, lots of plans that have not come to fruition	
Coordinated events to create a cultural destination	
Improved Reputation and status	
Sense of a civic or cultural heart	
Retain and preserve historic buildings	
A sense that things are changing	
Express the region's history – e.g. as a citrus producer	
Be modern	
Atmosphere	
Consider what Erina can't provide: Waterfront, high street, history and identity, 24 hr	

ECONOMY

Jobs growth and availability, high income jobs	
Address unemployment	
Provide quality commercial centre strengthen to compete with other centres	
More variety of retail and jobs	
Make Gosford the head quarters for business and culture	
Attract more professional businesses back from places like Erina, e.g. legal and banking	
More high quality shops, culture and arts	
Options for food and beverage after events, e.g. a Saturday Game at the stadium, on the waterfront	
High quality office space	
Make it easier to do business – council fees, charges and levies make it expensive	
Improve the stadium – address event mode pedestrian access, improved members facilities, sponsors areas	

HOUSING

Located in the city centre, more residents living in the city centre	
More affordable housing, address rental stress, social housing	
Diversity in housing options including smaller houses	

Workshop 6

What we heard from Council, Transport for NSW and Roads and Maritime Services.

An integrated access and movement workshop was held; attended by staff with responsibility for Gosford and the Central Coast from TfNSW, RMS, Sydney Trains, Central Coast Council, Department of Planning and the GANSW to discuss particular transport and movement issues to Gosford, current and future.

- The conversation covered the following priorities:
- Integrated Movement and Access Strategy
 - Car parking
 - Short, medium and long-term parking strategy
 - Green infrastructure and active transport as part of public domain
 - Connecting Gosford to Sydney and Newcastle – 1 hour service desirable
 - Connecting Gosford to the region
 - Opportunities for connecting the people and places across the railway line
 - Etna Street Bridge
 - Pedestrian connections between the hospital, schools and station
 - The station – opportunity with changes to platforms/ services
 - Walkability and public domain
 - Improving access between the CBD and the waterfront
 - Through traffic versus local traffic – reducing the impact of traffic on key places
 - Conflict between traffic and pedestrians Northern Precinct
 - Improvement of interface with the water
 - Funding and governance

KEY FINDINGS:

An *integrated place based access and movement plan* should be fast-tracked and incorporate existing work program underway for traffic and car parking.

The car will continue to be important in the short term and needs to be accommodated, but we need to plan for a scenario where car parking can be designed to be repurposed.

Need to ensure the efficient use of existing car parking and identify sites both in centre and outside for future car parking.

Crossing of the railway line – not all crossing need to cater for every mode – opportunities to prioritise modes at different locations

Junctions on either side of Etna Bridge are important – not just the bridge

New Inter-City Fleet will improve services – longer trains, tables, Wi-Fi

New trains require extensions to platforms – explore opportunity to better connect to City Centre

Station upgrades also planned

Opportunity to extend shuttle bus route to serve new parking locations and key attractors

95% of workers who live within 5km of Gosford currently drive-in – there could be an opportunity to target this group for other transport modes

Investigate opportunities for on-demand services operating elsewhere on the Central Coast

Reinforce that the city operates in different ‘modes’ – peak rail commutes, local journeys to work and event mode.



GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
5. Process

Workshop 7
What we heard from Central Coast Councillors.

The workshop was attended by a number of Central Coast Councillors including the Mayor and members of the Executive and Urban Growth Strategies Team.

The workshop focused on what is required to make Gosford an attractive regional capital and the three key places.

The following is a summary of the key issues and opportunities raised.

WHAT WOULD MAKE GOSFORD MORE ATTRACTIVE AS A REGIONAL CAPITAL?

- Critical mass and diversity of people and offer
- Diversity of housing from luxury to student and keyworker housing
- More visitor accommodation including 5 star and boutique, to cater to a range of markets
- Better strip shopping /retail offer – different to out-of-town centres
- Night time and weekend activities/ Live music venues/ RPAOC
- More cultural activities
- Conference centre
- Greater diversity of education opportunities – grow TAFE/ University in centre
- Locating additional government services and offices in Gosford
- Pop up shops and cafes
- Eat street and festivals linked to events
- Medical suites
- MBN
- Look-outs, vantage points and vistas
- Safer lighting/ festive lighting
- Street trees and better open space
- Pet friendly spaces
- Public art
- More green spaces
- Green infrastructure including recycled water not just street tree planting

- Safe cycling routes into the city centre/ more cycle parking
- Car parking – competing pressure from commuters and visitors
- Council working on car parking plan to identify short, medium and long-term options
- Transport – further opportunities for ride share, Uber, car pooling
- Coordination of transport for events – particularly public transport and road closures
- Better timetabling of buses – direct routes/ shorter journey times, better interchange
- Increase and extend shuttle bus services
- Improve event parking options
- Establish south exit from station
- Explore ferry/ water transport option – local and regional
- Encourage flexible working
- Support social services – acknowledge that there are people in community that need support – impact on place

KIBBLE PARK

- Important public green space in the centre of the city
- Not currently attractive enough to draw people to Gosford
- Busy at lunchtime – people working and studying nearby use it
- Perceived safety problems particularly at night – it is hidden from the street
- Poor accessibility and connections across the park
- Lack of greenery
- Cars cut off space from the Imperial Centre
- Conflict with loading and servicing
- Issue with maintenance
- Taxis impact on amenity

- Opportunities
- Reuse library for public use – gallery, temporary exhibitions, meeting space
 - Maintain sunlight to public spaces
 - Natural amphitheatre
 - Footpath dining
 - Hedge or something to block view and impact of Henry Parry Drive
 - Temporary road closure on surrounding streets for special events
 - Food trucks

- More outdoor dining
- Outdoor movie theatre
- Markets
- Music
- University/TAFE
- Safe play

NORTHERN CITY CENTRE

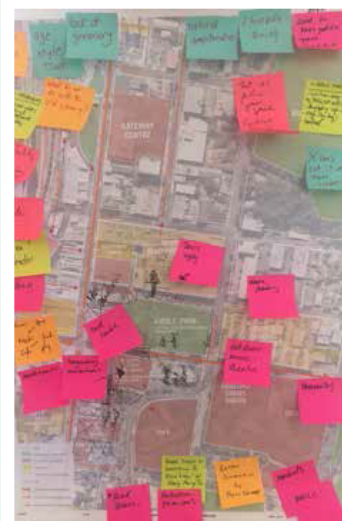
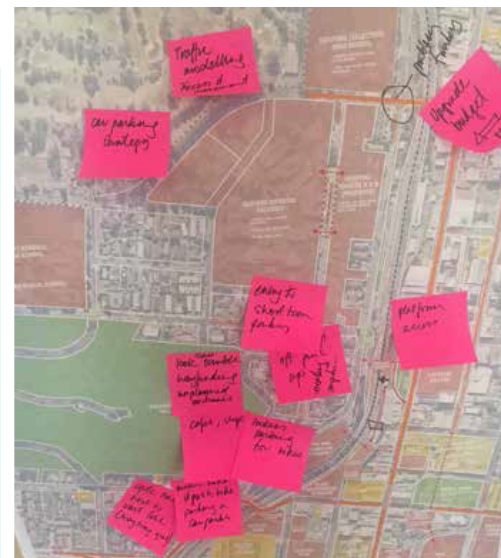
- Congestion problems and pedestrian conflicts at either side of Etna bridge
- Station – problems with wayfinding and connection to city centre
- Entry to short term parking area problem
- Drop off and pick up arrangements causing congestion

- Opportunities
- Need plan to avoid series of unplanned outcomes
 - More shops and retail
 - More motorcycle and bike parking in commuter car parks
 - Better bike parking and lockers
 - Better connecting station into the City centre
 - Improve direct access to platforms

WATERFRONT

- Swimming pool great asset
- Needs better landscaping, play areas
- Poor connectivity to waterfront from city centre, stadium and parks.

- Opportunities
- Better balance between active and passive recreation
 - Improve connectivity to waterfront for pedestrians
 - Consider rerouting Central Coast Highway further north to increase open space on the waterfront
 - Recognition that Council owns a large area of water and could reclaim land without having to interfere with parkland to the north for road
 - Swimming pool – relocate grandstand to open up connection to the waterfront or relocate pool to Leagues Club Field and tennis courts on Racecourse Road
 - Waterpark/ Things to do on the water



5.3 What Council has heard from the community





















The UDIF has benefited from the insights of what matters to the people of the Central Coast. These insights were gathered through the Central Coast Council Community Strategic plan.

Engagement to support the development of the Community Strategic Plan began in April 2017 with the second phase of engagement completed in December 2017. The final plan is due for adoption by Council in June 2018.

The Council has made available the early analysis of the findings to assist with this project. These are the key Gosford CSP outcomes:

- Revitalise Gosford City Centre, Gosford Waterfront and town centres as key destinations and attractors for businesses, as well as local residents, visitors and tourists
- Activate spaces and places to complement activity in town centres
- Target economic development in major centres
- Concentrate development along transport corridors and town centres
- Ensure all new developments are well planned with good access to public transport, green spaces and community facilities

Extract What would make the Central Coast better?: Central Coast Council Community Strategic plan.

	Transport/movement around the LGA (2208 comments)	Built Environment (1612 comments)	Services and Facilities (999 comments)	Activities and Entertainment (973 comments)	Natural Environment (837 comments)
1st	 Improved road maintenance/infrastructure (735 comments)	 Stop/reduce development/high density development (192 comments)	 Options for children and families i.e. playgrounds (219 comments)	 Community events (184 comments)	 Clean streets/waterways (280 comments)
2nd	 Improved public transport (643 comments)	 Improved infrastructure/keeping up with the population (178 comments)	 Better/more sporting facilities (191 comments)	 More/variety of cafes and restaurants (157 comments)	 More trees, green spaces, parks (180 comments)
3rd	 Improved parking availability (259 comments)	 Improved drainage/kerbs and guttering (173 comments)	 Access to quality education (99 comments)	 Public art/promote arts and culture (143 comments)	 Protection of local wildlife/ the environment (131 comments)
4th	 Adequate footpaths (187 comments)	 Develop Gosford, make it the hub of the Central Coast (155 comments)	 Better local services and facilities (80 comments)	 More/variety of entertainment facilities (126 comments)	 Sustainability/recycling initiatives (73 comments)

5.4 Go Gosford Community engagement

The Go Gosford Community Engagement was launched on 27 February 2018 by Department of Planning, Gosford Office. The aim of this engagement was to gauge the level of support and relevance of the revitalisation of Gosford to people living or working in the City Centre and more broadly across the local government area.



Community Surveys:

Community members were surveyed about their impressions of Gosford City Centre and what is needed to make it a thriving regional capital. They were asked questions about how they get to the City Centre and the types of activities they would like to see in the City Centre. The survey also asked respondents whether revitalisation of Gosford City Centre is important to the region.

The survey was conducted using an online engagement website and face to face surveys at shopping centres across the region, Gosford Hospital and Gosford Railway Station.

The online engagement site included the surveys but also included:

- A Mapping Tool where respondents could record places and landmarks of relevance to them in Gosford City Centre
- A story-telling tool where respondents could share memories of Gosford City Centre
- A photo competition where respondents could share pictures of places that are valued in Gosford.

Survey topics:

Over 600 responses were received during the engagement period.

Survey topics included:

- The perceived level of need for revitalisation
- Ease of parking in the city centre
- Mode of travel to the city centre
- Activities that should be available in a regional capital
- Opportunities for flexible working arrangements in Gosford

KEY FINDINGS:

The revitalisation of Gosford City Centre was of very/ most importance to over 80% of respondents, with the revitalisation of Gosford as the region's capital of very/ most importance to over 90% of respondents.

Majority of workers and visitors travel to Gosford currently by car.

A highly-ranked answer to the question "Imagine the Central Coast in 10 years... what would you like it to be?" in Gosford was improved public transport.

The lack of efficient public transport options on the Coast has led to a dependence on travel by private car to the city centre by commuters, workers and shoppers.

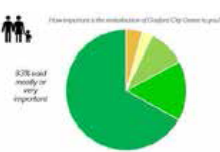
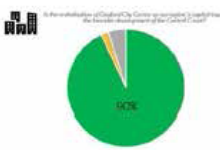
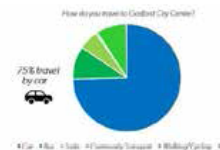
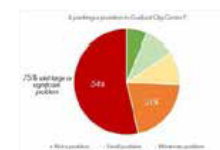
Parking is a significant problem in the city centre.

Survey responses reflected that car parking was a significant problem for a majority of respondents.

Feedback received from the mapping tool and story tool was quite positive with respondents showing how they identify with Gosford and its history. Gosford Pool and Gosford High School and Primary School were specifically mentioned.

Respondents identified public spaces, open space, parks and art needed to be improved.

Main activities respondents wanted for the City Centre included dining/food and entertainment.



5.5 Using Better Placed for the design process of the UDIF

Better Placed, NSW's Integrated Design Policy for the Built Environment, promotes an iterative design process that draws together multiple inputs, resolving and integrating influencing factors through the design process. A key part of this document is emphasising the value of good process. The following table illustrates how the Better Placed design process has been applied to the Gosford city UDIF.

DISCOVER			CREATE				DELIVER		
BETTER PLACED DESIGN PROCESS	Define	Involve	Research	Explore Possibilities	Synthesise	Develop Ideas	Prototype	Evaluate	Implement
<p>THE DESIGN PROCESS FOR THIS UDIF</p> <p>Questions were chosen to establish a clear intent for the problem</p> <p>A clear, targeted brief was set for this plan, identifying clear steps based on the needs of Gosford city centre</p> <p>The long and short term requirements of the plan were considered, anticipating the future needs of Gosford city centre</p> <p>The roles of the Regional Coordinator and GANSW in delivering this report were defined to organise effective work-flows</p>	<p>A broad range of stakeholders and community groups have been identified and engaged</p> <p>By collecting a diverse range of perspectives, the breadth and complexity of issues can be understood and consensus for the recommended actions achieved</p> <p>An expert team of consultants, designers, economists and planners have contributed to this framework</p>	<p>Extensive research has been undertaken in order to identify the context and scope of issues to be addressed by this framework. Existing insights, research and knowledge on Gosford have been identified and incorporated.</p> <p>Precedents and best practice case studies relevant to Gosford city centre have been drawn upon in developing this framework.</p> <p>Existing social, economic and environmental systems and frameworks have been considered in this framework</p>	<p>Multiple strands of data and insights have been collected at design workshops, key stakeholder meetings, through community consultation and surveys</p> <p>In developing this framework, Divergent thinking and concurrent approaches to solving problems have been embraced. Testing a range of ideas in workshops often validated past recommendations.</p> <p>Different scenarios have been framed in order to capture the multitude of possible design outcomes from the framework creation process.</p>	<p>By creating links between different perspectives and sources of information, this framework has been able to synthesise past and present knowledge to create a logical, clear and easily understood way forward</p> <p>Mapping the network of sources, perspectives and idea generators has enabled gaps to be filled and opportunities identified.</p> <p>In developing this framework, links have been formed between stakeholders, resulting in synergy towards change for Gosford city centre.</p>	<p>Ideas generation has been iterative, testing a wide range of options and a diversity of ideas. Rapid testing is equitable, inclusive and supports thinking outside the box</p> <p>Complex ideas and relationships have been distilled into clear and effective visualisations to ensure a common understanding of process and consensus driven framework development.</p> <p>By capturing commonly held vision in diagram and illustration, stakeholders are galvanised and inspired to remain committed to the framework.</p>	<p>A process of rapid prototyping has enabled swift and efficient ideas testing with client and stakeholders.</p> <p>With each round of testing, Prototypes have been built and re-built in response to a range of complex issues and diverse viewpoints.</p> <p>The prototyping process ensures that this framework is an outcome of collaborative effort, is robust, defensible and is the basis of an enduring vision for Gosford city centre.</p>	<p>Continual evaluation and adjustment is key to ensuring the currency and potency of the framework for Gosford city centre.</p> <p>Initial design reviews have been completed by GANSW and the client to test proposals and solutions. Methods and frameworks for ongoing evaluation established and incorporated in the framework.</p> <p>This framework is built upon common objectives and a shared vision for Gosford city centre. Future proposals for Gosford must be evaluated against this vision</p>	<p>This framework is intended to enable consistent and considered resource procurement and project delivery. The framework should be able to unite a range of efforts, initiatives and enterprises under a common set of objectives and aspirations for Gosford city centre.</p> <p>The success of this framework will be measured by how well its vision and intent for Gosford city centre is implemented across sites, by stakeholders and over time. The framework is a narrative, told by the people of Gosford for their future prosperity.</p>	

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

06. APPENDIX

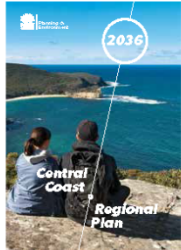
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GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

6.1 DETAILED REPORT SUMMARIES

These following summary pages aim to consolidate federal, state and local plans for the region, with a specific lens of the Gosford City Centre.

6.1.1 NSW Government Policies and Goals



Central Coast Regional Plan, 2016-2036

NSW DEPARTMENT OF PLANNING AND ENVIRONMENT

This document outlines the NSW Government vision for the central coast, including its broad vision for the region and the role the Gosford City Centre will play in the future of this region. The plan outlines regional level economic, jobs, housing, infrastructure and industry targets for the region. The plan outlines 4 goals for the region, and 23 Directions. Goals 1/Direction 1 specifically relates to the growth of the Gosford City Centre as the region's capital.



Economic Profile – Central Coast, 2015

NSW NOW

This document outlines the economic profile of the Central Coast. The Central Coast is "a unique region with an enviable coastal lifestyle, growing business hub, skilled workforce, and strategic access to Sydney and Newcastle."

"In 2013 the Central Coast generated \$10.8 billion in Gross Regional Product (GRP), representing 8% of Regional NSW's total GRP."

Key state investment opportunities include upgrades to the Central Coast Highway West Gosford Intersection, the Northconnex Motorway, train passing loops to improve commuter services, and business investment schemes across the area.

The report includes some useful detailed economic and local population statistics including population, employment rates, and education levels.



Gosford City Centre Transport management and accessibility plan (TMAP), 2010

AECOM FOR TRANSPORT NSW

This document's primary purpose is to increase the public transport share of work trips into the Gosford City Centre. There is recognition that Gosford needs to address its complex transport challenges such as access across the railway line, illegible and poor quality pedestrian facilities, severance of the waterfront, limited cycle networks and end of trip facilities, managing its parking supply and commuter drain. The comprehensive review includes initiatives to tackle these key challenges and interestingly places the potential measures into short term, medium term and long term actions.

It is unclear how many of the recommendations were adopted and implemented. Many recommendations suggested further studies like a detailed parking strategy, bus review, and public domain upgrades.



Central Coast Destination Management Plan – Draft 2, 2017-2021

EARTHCHECK FOR DESTINATION NSW

This document outlines the state level vision for the Central Coast to be the choice for leisure, business and major events. By leveraging the natural assets of the area, the management plan aims to lift the overall visitor experience and use tourism to engage entrepreneurs and the youth in employment opportunities.

It aims to increase the overall market share of regional visitors by providing a clear marketing identity for the Central Coast.

The plan includes strategic priorities for the next scope of work including development and implementing a new Destination Brand and a Regional Signage Strategy for each of the villages in the Central Coast.

Interestingly it is one of the few strategies that outlines key actions for creating a clear, simple identity for the area, improving market confidence, attracting investment and lifting community pride.



Better Placed, 2017

GANSW

This state level policy "has been developed by the Government Architect to deliver the strategic approach needed to ensure that as our cities and towns grow bigger they get even better." It outlines a clear government endorsed design approach to ensure good design that will deliver the architecture, public spaces and environments we want to inhabit now and into the future.

The focus on quality places in Gosford, and strategies for an iterative design process has informed the work of this urban design and implementation framework. In particular the focus on holistic design thinking has ensured cross government agency collaboration through a series of workshops, and the ability to reconsider the connections and synergies that strengthen the individual precincts and places of Gosford.

The seven design objectives for better places form the basis for evaluating the quality of public spaces proposed by this implementation framework.



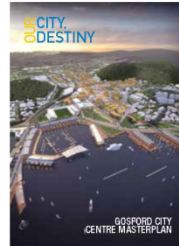
Greener Places, 2017

GANSW

This draft policy document by the Government Architect, 'Greener Places' focuses on the role green infrastructure can play in shaping and improving the liveability of our urban spaces. "It aims to create a healthier, more liveable and sustainable urban environment by improving community access to recreation and exercise, supporting walking and cycling connections, and improving the resilience of urban areas." Crucial to this policy is the concept that green infrastructure is a network of connected places that form a green grid. This infrastructure has environmental, social and economic benefits for the community.

For Gosford the key places identified by the workshops were all interestingly focused on the green open spaces of the city centre. Enhancing the natural green and water setting of Gosford to its connection to recreational spaces and the public transport network is a vital part of this plan.

6.1.2 Gosford City Council Strategic Policies and Plans

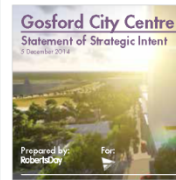


Our City, Our Destiny – Gosford City Centre, 2010

COX ARCHITECTS

The result of extensive community consultation, this extensive high level masterplan articulated the community vision for the Gosford City Centre with 5 key precincts. This masterplan forms the most comprehensive masterplan to date for the city centre and was the basis for further detailed urban studies on transport, movement and street upgrades. Little of the masterplan moved to implementation and most remains at a strategic high level with little detail.

The document remains largely applicable. This report aims to provide an update of this 2010 masterplan and its strategic vision.



Gosford City Centre – Statement of Strategic Intent, 2014

ROBERTS DAY

This document was created in response to renewed interest in the development in the city centre, and the large number of DAs received by Council as a result of the bonus provisions in the Gosford LEP 2014.

It recognises the increase in FSR and height combined with uncertain economic times have created 'land banking'. It aims to 'align council's strategic direction and current market conditions', and aims to provide best practice design for appropriate built form massing and heights that would suit the city centre.

Whilst useful the document is again a high level strategic intent document. It contains recommendations for future actions that would allow for implementation and detailed design. The slender tower typology may not align to current demand projections for the city centre.



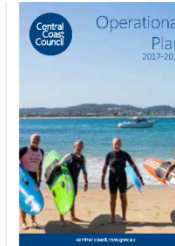
Gosford City Centre – Floorspace Incentive Contribution Rate Study for Gosford City Council, 2016

AECOM

This document provided an economic review of the Gosford LEP Incentive Scheme, and considered the implication of continuing or changing this legislations. Key insights included the impact the incentive scheme has had on land banking in the city centre, and how very few paper DAs resulted in real construction or increased financial contributions to council. Increasing certainty for the development market is crucial. Other key insights include:

"At present price levels, large scale tower development is unlikely to eventuate"

"Council could consider a program of public domain works in the CBD. Public domain works are visible and can have a strong positive impact on investment confidence. Together with ongoing development activity, public and private investment activity can help create an environment conducive to further investment into Gosford."



Operational Plan – Central Coast Council, 2017/ 2018

CENTRAL COAST COUNCIL

This document outlines the Central Coast Council priorities for operational delivery in 2017 and 2018. Key items for the Gosford City Centre include funding for:

- Design and construction of a new regional library and commercial building at the Parkside building by 30.06.20
- Facilitate the establishment of a university presence by 2010
- Stadium upgrades (approx \$2M)
- City centre sewer and water upgrades
- Design of a regional performing art and conference centre.

6.1.3 Gosford City Council Transport and Parking Strategies



Gosford Bike Strategy, 2014

GOSFORD CITY COUNCIL

Based on community consultation, "the Gosford Bike Strategy 2014 is Gosford City's commitment to support initiatives that encourage more people within the local government area to cycle and to promote cycling as an integral part of daily life."

The strategy aims to increase the mode of bicycle trip in the LGA to 5% by 2016 and 8% by 2031 in line with state policies. It also aims to establish a 244km cycle network which is safe, well maintained and conveniently located.

The strategy outlines key network upgrades to the local road system, key projects like the Point Clare Cycleway, the need for end of trip facilities and bike parking.

The strategy also supports sustainable use of designated mountain bike tracks in the natural bush settings of Kincumba Mountain and Rumbalara and Katandra Reserves.

Funding partners are being pursued.



Gosford Parking Study, 2010

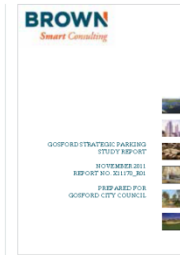
GTA CONSULTING FOR GOSFORD CITY COUNCIL

This is a detailed supply and demand study for parking in the Gosford city centre. It reviews opportunities to create a walkable urban city with parking on the periphery and high levels of activity in the core. The review considers parking provision in new developments, parking in strata titles, parking discounts, the transition of long term parking to short term, contribution methods and future opportunities from large developments.

In general the issue of commuter parking is addressed by transitioning to other modes of transport, to allow long term parking to transition to short term parking to support retail and commercial development. It suggests developers can give discretionary VAP contributions to offset a 10% reduction in parking provisions.

Additional car parks, on the periphery of the city centre are recommended, within suitable walking distance and supported by a shuttle bus.

Stronger restrictions and increased fees are recommended for current parking stations. Parking should be provided at the waterfront, new library and to the north.



Gosford Strategic Parking Study, 2011

BROWN SMART CONSULTING FOR GOSFORD CITY COUNCIL

This is a detailed supply and demand study for parking in the Gosford city centre. It reviews opportunities to create a walkable urban city with parking on the periphery and high levels of activity in the core.

In general the issue of commuter parking is addressed by transitioning to other modes of transport, to allow long term parking to transition to short term parking to support retail and commercial development. It suggests developers can give discretionary VAP contributions to offset a 10% reduction in parking provisions. Additional car parks, on the periphery of the city centre are recommended, within suitable walking distance and supported by a shuttle bus. Alternatively partnerships with the shopping centre and Leagues club could be considered.

Stronger restrictions and increased fees are recommended for current parking stations. Parking should be provided at the waterfront, new library and to the north.

A detailed schedule of actions are provided.

6.1.4 Gosford City Council Public Domain Strategies



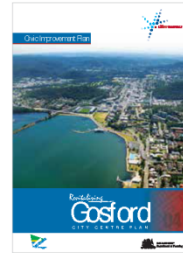
Gosford Streetscape Guidelines, 2011

OCULUS FOR GOSFORD CITY COUNCIL

The primary objective of these design guidelines is to 'provide a clear vision and framework for future streetscape upgrades within the City's six urban and waterfront precincts. The guidelines provide a consistent and locally responsive palette of elements and materials which...facilitate the creation of great streets.' "They aim to help Council to create streets with a high level of amenity...and as a result act as a catalyst for retail and urban development in the City."

This document takes into account the 2010 masterplans and were developed in association with Council officers.

Key issues include a local of consistent theme, poor pedestrian amenity and way finding, a need to express the unique qualities of Gosford, poor visual quality and the dominance of the vehicle.



Gosford Civic Improvement Plan, 2007

NSW DEPT PLANNING REGIONAL CITIES
TASKFORCE FOR GOSFORD CITY COUNCIL

This plan "provides a description of the city infrastructure needed to support the growth and development of the city centre, and outlines the framework for contributions to be made from development towards funding and provision of the infrastructure." This document was prepared prior to the "Our City Our Destiny" masterplan and was accompanied by a Vision document, LEP and DCP. It appears to be the first mention of waterfront precinct and cultural centre.

The public domain framework plan includes 'green fingers' of street planting to link the streets with the surrounding bushland, the creation of an 'urban stream' to interpret the original creek that could link the hospital and waterfront, and the provision on amenities to support public open space. All of these plans remain relevant today. The upgrade of Kibble Plaza to its current design may have come from this plan.

These strategies are proposed to be funded by a 4% S94A levy.



Gosford Waterfront Strategy, 2007

TAYLOR CULLITY LETHLEAN

This early report, prior to the Our Vision Our Destiny masterplan of 2010 outlines possible options for the design and development of the Waterfront. This concise strategy suggests the Waterfront is considered as a staged development of nodes along a cycle and pedestrian route. Whilst development sites are possible, like a medium sized marina, the focus is on community driven recreation spaces and a journey that links them along the water. The strategy recommends that the optimum width of the recreational land between the water and road should be 50-60 meters to allow for walking, cycling, landscape and playgrounds. For this space to be possible the strategy has considered land reclamation or realignment of Dane Drive.



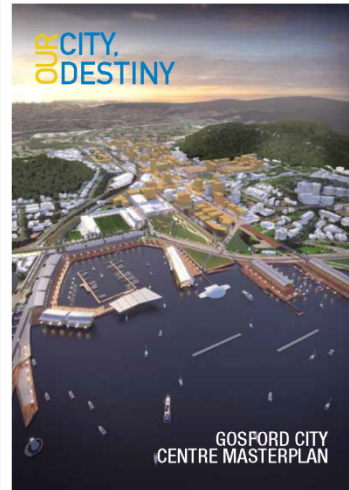
Brisbane Water Foreshore Floodplain Risk Management Plan, 2015

CARDNO FOR GOSFORD CITY COUNCIL

Informed by a Floodplain Risk Management Study (also by Cardno), this report aim to direct and coordinate future management of flood prone land around the Brisbane Water Foreshore, and to educate community about flood risks.

The main concern is coastal tidal flooding, often a result of severe coastal storm events. For Gosford the main impact is at the foreshore, with private properties unlikely to be affected. The impact of rising sea levels is also of significant concerns. Key finding of the study were that existing flood risks can be managed through controls, emergency response measures and minor works. "The impact of wind and swell induced waves were found to have an impact up to 20m from the foreshore edge for the majority of the waterway, with some areas near the entrance to Brisbane Water potentially being impacted up to 40m from the foreshore edge due to the influence of ocean swells..." The report outlines suggested DCP controls. Existing seawalls may require maintenance or raising in some areas.

6.1.5 Our City Our Destiny



Our City, Our Destiny – Gosford City Centre Masterplan, 2010

COX ARCHITECTS

The result of extensive community consultation, this extensive high level masterplan articulated the community vision for the Gosford City Centre with 5 key precincts. This masterplan forms the most comprehensive masterplan to date for the city centre and was the basis for further detailed urban studies on transport, movement and street upgrades. Little of the masterplan moved to implementation and most remains at a strategic high level with little detail. A more comprehensive update of this document is outlined in this section of the UDIF report.

This Masterplan called "Our City, Our Destiny" (also referred to as The Gosford Challenge) is the key masterplan still relevant in large for the Gosford Area.

This masterplan was developed by the former Gosford City Council, the Land and Property Management Authority and an extensive design team including Cox Architects, Arup and Oculus to name a few. The work was also the result of extensive community consultation including a 6 day design charrette in 2009. The work was largely supported by the community. This masterplan, as outlined in this appendix, builds on the recommendations of many previous reports. It remains as the most detailed and comprehensive masterplan since 2010.

As such this work forms the basis of this implementation plan. The key structure and urban strategies of this masterplan remain largely relevant. In this UDIF report, small updates are provided where time has altered a requirement or overall thinking. The focus of this report remains on how to implement this masterplan with meaningful actions and measurable results. It appears that for various reasons, very little of this masterplan has been enacted. There is however consensus and vision with this work that helps drive this implementation plan.

The following summary outlines some of the key thinking of this masterplan.

Key Strategic Aims:

In 2010 the Central Coast was expected to add 100,000 new residents and 55,000 new jobs by 2031. Gosford City Centre was expected to add 10,000 of those new residents and 6000 new jobs.

"... no other regional city or major centre within Sydney has the unique landscape and relationship to water. Through the power of its setting, the lifestyle of the Central Coast and its strategic and transport relationship to Sydney, Gosford clearly has the potential to both make a contribution in the Sydney context and adopt a more dynamic role within the Region."

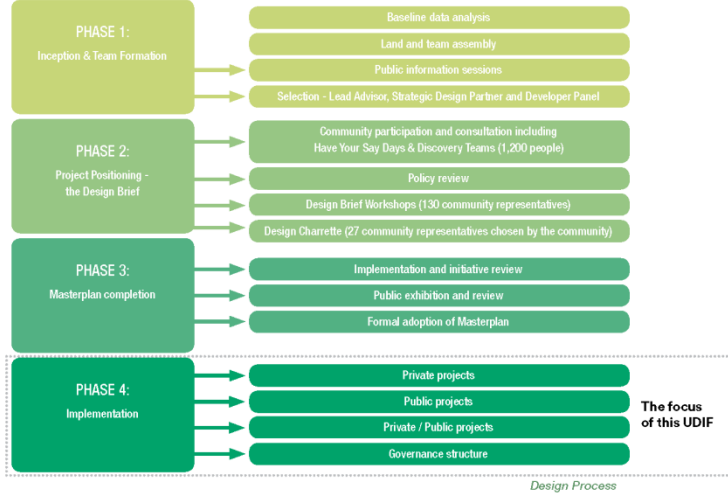
"Gosford is designated to serve the regional catchment extending from Warnervale, Wyong and Tuggerah in the north, east to the coastline at The Entrance, Terrigal and Avoca, Woy Woy, Ettalong and Umina to the south and west to Somersby and the National Parks, a significant area of some 1,800 square kilometers... and a population of over 300,000..."

"It is important that the villages of Gosford are reconnected with its heart, a city centre where people of all ages, incomes and cultural backgrounds feel they belong to a community that is vibrant, diverse, safe, healthy and sustainable. Gosford will become the focal point for the direct catchment and reduce the need to travel outside of the Central Coast by increasing commercial, retail, educational, work and social activities within the City Centre."

"Included in this Masterplan is an implementation section ('Making it Happen') that deals with how the public realm and infrastructure will be developed to act as a catalyst to encourage investment and development by local landowners, government and developers."

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
Appendix 1

The journey of this Masterplan is depicted in the following diagram:



It then breaks down into 5 key precincts. A summary of these precincts and their suggested projects are summarised below:

1. Waterfront Precinct

- Continuous public access to the waterfront
- Retail, entertainment and offices with buildings that minimise view loss
- Dane Drive Upgrade with parking and new crossings, new intersection at Brian McGowan Bridge and Baker St
- Point Clare Cycle Link
- Regional playground of 2500m2 on the water

2. Art and Entertainment Precinct

- Create activity on nights and the weekend
- Extend Baker St to the water as a pedestrian link
- Activate Mann St with cafés and restaurants
- Represent Aboriginal heritage through the creek reinterpretation
- Create a performing arts centre close to the water, possibly linked to the Conservatory of Music
- Relocate sports fields from the Gosford City Park to the schools, allowing this as a recreational place for events, markets and informal play.

3. The City Core:

- Focused on Mann St and Kibble Park
- Activate the façades surrounding Kibble Park
- Mann Street sewer upgrades combined with public domain upgrades and street tree planting
- Create a pedestrian link through the Imperial Shopping Centre to Watt St to link the park to more streets
- Link Erina and Dommison St to Rumbalara Reserve
- Strengthen connections between Dommison St and Mann St

4. The Railway Precinct:

- TMAP (complete)
- Review possible improved interchange
- Mann St north upgrade
- Hills/Watt St Green spine to link northern residents to Kibble Park
- Remove the Gateway Centre Bridge
- Upgrade Burns Park

5. Hospital Precinct:

- Support the expansion of health and wellbeing services
- Etna St Bridge upgrade – priority to service the hospital and schools
- Holden St, Racecourse Rd and Showground Rd pedestrian upgrades
- New development along showground road
- Gosford Public School Relocation (completed)
- Gosford and Henry Kendall High School oval upgrades, in partnership with council to help provide these facilities for the wider community
- Railway pedestrian link upgrades acknowledging the challenges of topography

4 Key Strategic Uses/Relationships

Connectivity

Connect water, hills and suburbs back to the CBD and Kibble Park

Interconnect major open spaces and draw landscape down from the hills with open spaces and tree planting (consider a green grid plan)

Improvement movement through pedestrian paths and cycleways

Use the streets as 'green spaces in their own right'

Work with owners of the Imperial Shopping Centre to provide a link from Watt St to Kibble Park in any future development (confirm if this is in the proposed stage 1 DA)

- Activation
- Access and Movement
- Sustainability

Regional cities' infrastructure table.

The NSW State Government has designated Gosford a Regional City of Sydney along with Parramatta, Liverpool and Penrith. They have also designated Gosford the Regional City of the Central Coast, as Newcastle is to the Hunter and Wollongong is to the Illawarra. Significant Government investment in infrastructure has supported growth in all these Regional Cities, through less so in Gosford.

Regional NSW Cities

		Gosford	Newcastle	Parramatta	Penrith	Liverpool	Wollongong
REGION		Central Coast	Hunter Valley	Greater Western Sydney	North West	South West	Illawara
DEMOGRAPHIC PROFILE							
Population of Region	2006-08	304 700	529 200	668 200	760 000	409 700	282 300
	2031	400 000	675 000	780 300	1 024 317	742 272	327 600
Employment of Region	2006-08	125 254	228 334	308 500	240 000	119 500	100 765
	2031	170 500	294 334	369 500	367 000	208 500	130 765

FACILITIES (In Centre)							
Hospital		✓	✓	✓	✓	✓	✓
University Campus		✓	✓	✓	✗	✗	✓
TAFE Regional		✗	✗	✗	✗	✓	✓
TAFE		✓	✓	✓	✓	✓	✓
Convention & Exhibition Centres		✗	✓	✓	✓	✗	✓
Museums		✗	✓	✓	✓	✓	✓
Concert Hall / Entertainment Centre / Theatres/Playhouse		✗	✓	✓	✓	✗	✓
Stadiums		✓	✗	✓	✓	✗	✓
Town Hall		✗	✓	✓	✗	✗	✓
Library		✓	✓	✓	✓	✓	✓
Art Galleries & Cultural Centre		✗	✓	✗	✓	✓	✗
Civic Square/precinct		✗	✓	✓	✓	✓	✓
Schools		✓	✓	✓	✓	✓	✓
Regional Shopping Centre		✗	✓	✓	✓	✓	✓
Conservatorium		✓	✓	✓	✓	✗	✓
Sporting Facilities Indoor/Outdoor		✓	✓	✓	✓	✓	✓
Community Meeting Centre		✗	✓	✓	✓	✓	✓
Regional Park/Recreational Facilities		✗	✗	✓	✓	✓	✓
TRANSPORT (In Centre)							
Stations		✓	✓	✓	✓	✓	✓
Interchange		✓	✓	✓	✓	✓	✗
Shuttle Bus		✗	✓	✓	✗	✗	✓
CBD B y-Pass/Ring Road		✗	✓	✓	✗	✓	✓

6.1.6 Gosford City Centre Transport Management and Accessibility Plan (TMAP)

Gosford City Centre
Transport Management and Accessibility Plan

Gosford City Centre Transport management and accessibility plan (TMAP), 2010
AECOM FOR TRANSPORT NSW

This document's primary purpose is to increase the public transport share of work trips into the Gosford City Centre. There is recognition that Gosford needs to address its complex transport challenges such as access across the railway line, illegible and poor quality pedestrian facilities, severance of the waterfront, limited cycle networks and end of trip facilities, managing its parking supply and commuter drain. The comprehensive review includes initiatives to tackle these key challenges and interestingly places the potential measures into short term, medium term and long term actions.

"This TMAP supports the renewal process in Gosford with measures to encourage sustainable growth. It addresses existing transport challenges such as access across the railway line, illegible and poor quality pedestrian facilities, severance of the waterfront, a developing cycle network and end-of trip facilities, and managing parking supply."

In 2010 this report stated its intention for a "A 10% target of AM peak period journeys to work into Gosford by non-car modes potentially by 2036."

Most of the challenges and recommendations outlined below are still valid and relevant. It seems like few of the suggested strategies were implemented.

Key Challenges:

"The Challenge process identified a number of key transport and access issues including:

Challenges for improving future access from West Gosford;

Poor legibility and amenity for trips through and within the city centre;

Severance of the waterfront by Dane Drive;

An immature cycle network and end-of trip facilities;

High demand for parking facilities."

The significant commuter work force (more than 25% of the workforce) creates congestion on the major arterial roads in peak periods, creates excessive demand for commuter parking in the city centre and informal parking near the F3 Freeway as people gather for informal car share.

Despite some topographical challenges analysis of work journeys indicates that many local trips from east and west Gosford to the city centre could be made by bus services or by or cycle.

In the PM peak, the east Gosford movement causes traffic, compounded by school and commuter traffic.

Connectivity Issues:

Improve connectivity for cycle and pedestrians across the Etna St Bridge and Donnison Street Bridge.

Pedestrian upgrades to Racecourse Rd

Improve the Railway Pedestrian bridge, in particular the south bound platform on the Mann St side of the bridge could be access on level with the street, rather than going up and down stairs.

Improved pedestrian connectivity to the Waterfront from the city centre

Overcome gradient changes and improve pedestrian connections to the Gosford Hospital

Traffic Strategies:

Mann St to be promoted as a public transport corridor
Consider options to direct heavy vehicles away from Mann St and Henry Parry Drive

Consider a bypass of traffic around the city centre

Improve access from West Gosford by cycle and public transport to reduce traffic demands.

Ensure significant new developments, the hospital and schools have travel plans that encourage alternative modes of transport.

Public Transport Strategies:

Improve a number of key bus corridors to link Gosford to the Central Coast Region

Encourage and promote alternatives to the private car to change travel behaviour.

Improve bus frequency, easily recognisable bus services and route consolidation to improve access and perception.

Improving bus services is suggested as a key way to reduce east/west congestion at Etna St and Donnison St.

Create a city centre loop bus to connect the city centre, station and hospital precincts. This should be linked to possible park and ride facilities.

Consider a "Gosford Interchange transit oriented development" at the station to consolidate and strengthen the public transport core of the city centre. This could help overcome grade level changes between Mann St, the Station and Showground Road. It may also act as a catalyst project for Gosford.

Walking Strategies:

The work of this strategy is supported by a Pedestrian Access and Mobility Plan (PAMP) (To be received)

The Baker Lane pedestrian short cut from the parking station to the station is considered unsafe poor to poor lighting and little passive surveillance. This route should be reconsidered, particularly in light of how this foot traffic could be re-diverted to activate Mann St.

The Draft Masterplan for the Renewal of Gosford City Centre recommends a pedestrian and cycle bridge over the station at Erina St to tie into the parking facilities.

Cycling Strategies:

Existing levels of cycle in 2010 were noted as very low, less than 1% of journeys to work.

Key cycle routes along Mann St recommended

Waterfront Point Clare Cycle Link along the railway causeway.

Identify suitable bike parking and end of trip facilities for workers in Gosford. Consider in particular on the west side of Gosford Station to cater to the population arriving on this side.

Create clear maps and promote the cycle and walking upgrades. Consider training to help improve understanding of possible alternative routes.

Parking Strategies:

"The role of parking should be to enable access to shops and services whilst limiting long term, commuter parking and where feasible, provide park and ride on the city fringe in locations such as Erina and Kariong. Management of long term parking needs to be accompanied by suitable alternative access measures, such as key bus corridors and park and ride."

In 2010 the AM peak most commuters are at the station by 7.30am, before shops would be normally open to capture this value.

In 2010 there was approximately 10, 500 parking spaces including 7000 off street parking. Most is available for longer term parking provided with no restrictions or charge. The stations have a visual impact and bring traffic into the city centre.

Consider shifting this parking to short term, time restricted parking that encourages access to the city centre for local services and business to help strengthen the economy.

Consider park and ride facilities on the periphery, or a Gosford City Council shuttle bus service in peak commuter times.

Consider reducing DCP parking rates for the city centre which will add congestion to the city centre with new developments.

Consider improved signage for the availability of parking supply.

Improved public transport or cycle connections to the station could reduce this parking demand, as more than 60% of the parking near the station is used for commuters.

Delivery:

"All potential measures considered and recommended in the TMAP are grouped into timescales for potential implementation as follows:

Short term measures – which could be delivered within 3-5 years (2010 - 2016).

Medium term measures – which could be delivered within the next 5-10 years (2016 - 2021).

Long term – which could be delivered within the next 10-15 years (2021 - 2036).

Public Domain Upgrades:

Mann St and Donnison St upgrades are suggested.

Generally pedestrian and cycle access through the city centre is limited.

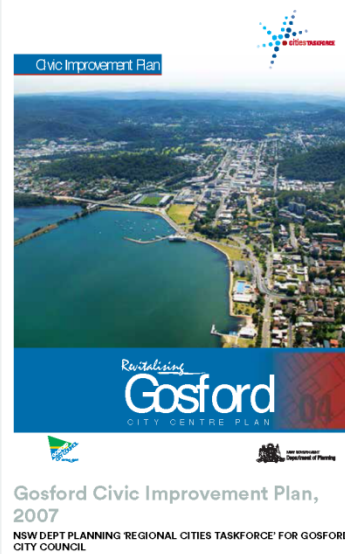
Consider the impact of future sea level rises which may affect Waterfront development and waterfront pedestrian and cycle routes.

Updates since this plan and future considerations:

RMS and Transport for NSW are currently underway with a detail movement plan for the Gosford City Centre and the wider Central Coast Region. At the time of this report the findings of this study were not yet known,

Since this report the state government has invested significant funds towards upgrades to the Central Coast Highway West Gosford Intersection, the Northconnex Motorway, train passing loops to improve commuter services, The impact of these changes would require further analysis. It is not expected that any of these changes will materially change the urban structure and recommendations of this report.

6.1.8 Gosford Civic Improvement Plan



This plan “provides a description of the city infrastructure needed to support the growth and development of the city centre, and outlines the framework for contributions to be made from development towards funding and provision of the infrastructure.” This document was prepared prior to the “Our City Our Destiny” masterplan and was accompanied by a Vision document, LEP and DCP. It appears to be the first mention of waterfront precinct and cultural centre.

This plan, an early document prior to the 2010 *Our City Our Destiny*. The principles and design strategies outlined below are still relevant to this implementation plan.

Principles:
Recover the natural relationship of the city to the sea and bushland.

Respect the city's heritage, reinforcing traditional street patterns and alignments.

Provide recreational and community facilities for community use.

Ensure the continued functioning of transport/ access systems.

Design strategies:
Introduce water and biodiversity sensitive designs that enhance the environment as well as create attractive public amenity.

Reinforce Mann Street as the city's civic spine.

Re-align 'Wharf Road' to re-connect Mann Street to the water.

Respect the permeable nature of the city grid and create a city-wide landscaping plan.

Re-align and landscape Dane Drive to change its highway character to a waterfront boulevard.

The plan seeks to provide strategies and actions for public domain projects including:
An Open Space Network – that unifies the existing fragmented open space.

Urban biodiversity designs

Water sensitive urban design including an 'urban stream' that in an interpretation of the original creek that flowed through the valley to the Broad Water.

Street typology work such as improving the civic spine of Mann St and reinforcing Baker Street's relationship to the original creekline with the urban stream.

Pedestrian Network with a safe and legible cycle and pedestrian network

Furniture and Paving developed with a Technical Manual for a consistent approach

Improved public open spaces with improved facilities, and new soccer, cricket and netball facilities, improved existing groups, more play facilities, joint ventures with the schools, youth oriented play like skate facilities.

Public art strategy

Bushland conservation and rehabilitation

Transport management including a parking strategy, and shuttle bus service

Improved stormwater management

Create a cultural centre to strengthen the arts and cultural environment, encourage innovation and facilitate learning and education.

“The cultural centre will include a number of facilities such as a concert hall, art house, cinema, museum, indigenous cultural centre, community cultural centre, civic centre, youth arts warehouse, performing arts facility, workshops/studios, community arts facility, and outdoor art and cultural facilities. The aim is to establish a centre of excellence for artistic and cultural practice.”

Create a new library and community facility (as identified in the Council's strategic Plan, Community Plan 2001

Create a regional cycleway network.

These works would be funded by a S94A Development Contributions Plan at 4% of the cost of the development.



Legend:
— Bus shuttle route
- - - City centre boundary

Figure 9: Bus shuttle route

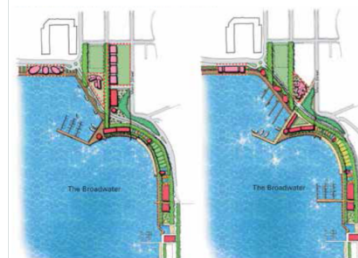


Figure 11: Waterfront precinct improvements

GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

6.2
**OUR CITY,
OUR DESTINY
MASTER PLAN
SUMMARY**

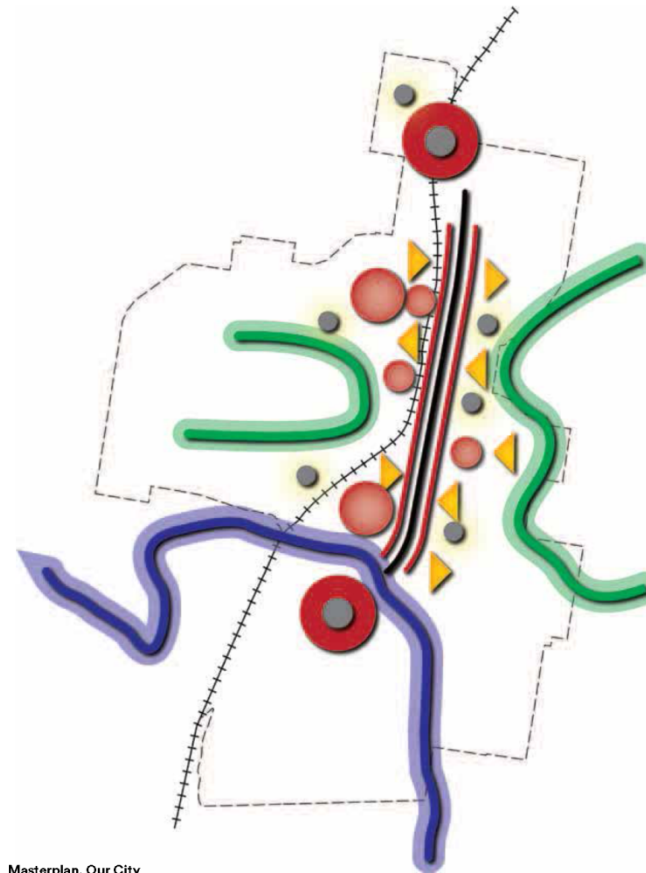
6.2.1 Our City, Our Destiny – Updating the Masterplan

The result of extensive community consultation, 'Our City, Our Destiny' was a high level master plan which articulated the community vision for the Gosford City Centre with five key precincts.

'Our City Our Destiny' forms the most comprehensive master plan to date for the city centre and was the basis for further detailed urban studies on transport, movement and street upgrades. Strategies and precincts identified in the master plan have built a strong base for the UDIF.

Most of the 'Our City Our Destiny' principles and city framework remain applicable today. The UDIF distils and updates the OCOD recommendations to key principles to aid in the assessment of any future proposal or project. This allows the 2010 master plan ideals to remain relevant, adaptable and resilient. Further, it recognises the physical scale of the city, market demand, Gosford's specific role as a regional capital and how this impacts on recommendations for implementation.

The adjacent table comprises a summary of the strategic urban moves, and suggested precincts and projects as originally outlined in 'Our City, Our Destiny'.



Masterplan, Our City
Our Destiny, 2010, p17

6.2.2 Natural Assets

“The structure plan for Gosford is the framework which will underpin the Master plan. It is a result of the natural attributes of Gosford and the potential to anchor attractions and key destinations upon a central axis. It is a robust framework that allows for flexibility of uses whilst maintaining the core dynamics of a growing Regional City...

The structure plan seeks to build upon the outstanding natural attributes of the City, by celebrating the mountains and the waterfront and building them in to the fabric of a revitalised and active heart.

It also reinforces Mann St as the spine upon which the city can grow and as the main artery for activity and movement.” p17

Today these sentiments form the basis for many of the new vision statements including:

1. It's time for Gosford to reclaim its role as the capital of the Central Coast. Gosford brings together all the best characteristics of the region: water, natural beauty and an urban centre.
2. Connect the people to its city and the city to its landscape.
3. Unlock the potential to create great places for people. An approach that puts the public domain and its people first.
4. Revitalising is not just one place or building, it is a network of well-connected places that create an attractive capital city with lively streets. An attractive city is a platform for investment with short-term returns and long term growth.



6.2.3 Comparing the Design Process

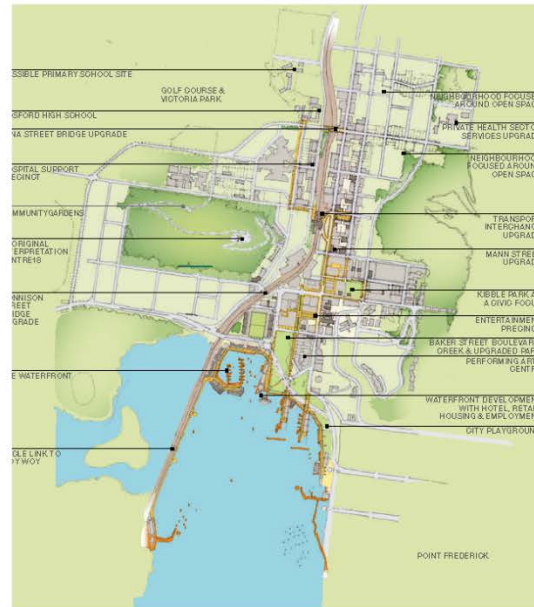
Workshops and Consultation

The OCOD master plan states ‘the spatial framework is a visual story taken from the discussions and drawings during the charrette process, the spatial framework translates written and numerical information into a visual medium to give a holistic view of the masterplan.’ p17

Similarly, utilising the tool and strategies outlined in the GANSW “Better Placed” this 2018 interpretation plan has involved an iterative design process and extensive stakeholder workshops. The result of these workshops have been summarised in detail.

Identifying key places

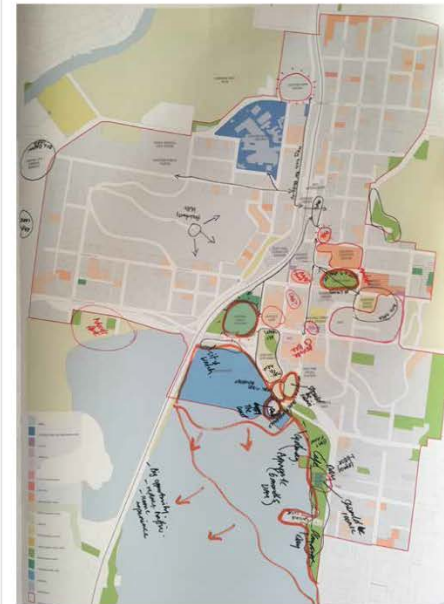
The 2010 as a comprehensive masterplan identifies the key places and landmarks of the city centre



Masterplan, Our City Our Destiny, 2010, p 19

Using workshops to test the validity of places

The workshop mapping image above demonstrates how the 2010 masterplan is still relevant today.

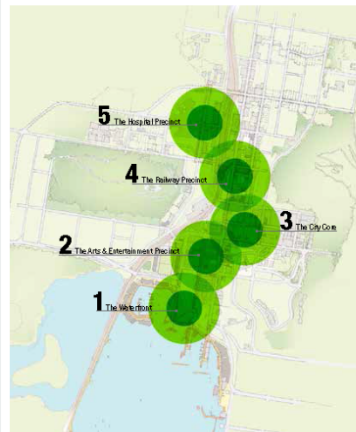


Workshop 1, Go Gosford Updates, 2018

6.2.4 Precinct Strategy

Overall Approach 2010 – Our City Our Destiny

The 2010 master plan identified five precincts as simplistic concentric radial areas. This diagram expresses only the position of the precincts in the city centre however did not define the areas and their connections geographically.

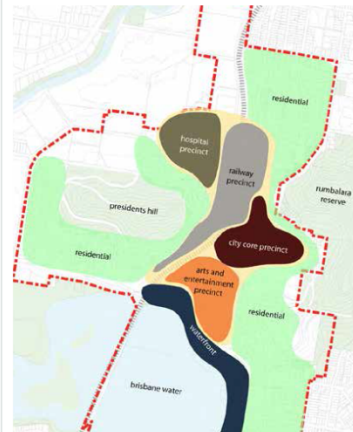


Five Precinct Activities, Our City Our Destiny, 2010, p20

Urban Design Intent 2011 – Oculus Streetscape Design

The Oculus document made an attempt to identify the areas geographically.

The Hospital precinct and the City core is divided by the railway which extends from north to south throughout the city centre. The waterfront precinct is long and narrow, and disconnected from the city core. Many of these challenges remain relevant today. Connections between the precincts have gained greater significance in this implementation plan.



Urban Precinct Diagram, Oculus Gosford Streetscape Guidelines, 2011, p7

UDIF 2018 – Places and their connections

The “Go Gosford” precinct strategy simplifies the five precincts to three places.

By using places rather than precincts to progress the work we can easily understand the places that people use on a daily basis. Places also allow for a diversity of uses and users, emphasising that whilst there may be one dominant use, complimentary symbiotic uses are vital. Each of the 3 places are anchored by a public or green open space. This will facilitate the organization of the city centre using public spaces as social attractors and the focus for redevelopment.



Go Gosford Update, 2018

6.2.5 Landmarks and Places

Places in the City Centre – Gosford City Centre Masterplan 2010

The 2010 masterplan identified many of the key existing and future places that remain relevant today. The network of public places seen in the map remain consistent with the places and landmark identified by the stakeholders in 2018.

Interestingly despite its design flaws Kibble park remains a key place for the community.

The train station (transport interchange) is still the most important form of public transport in and out of the city centre.

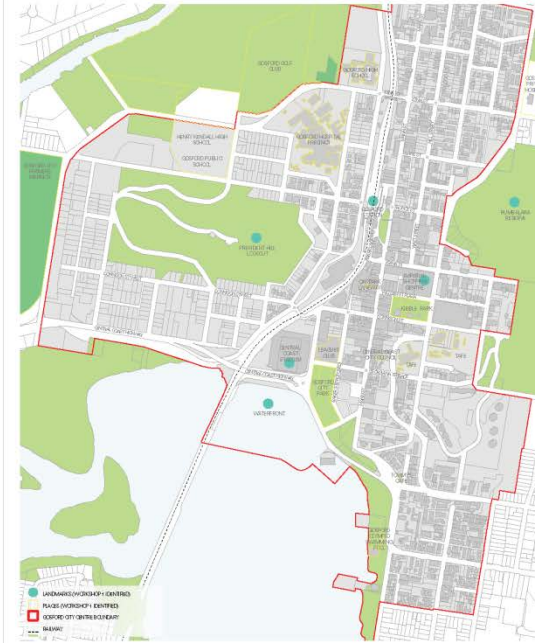
The hospital with its large extension will become even more important within the city centre.



2010 Masterplan Key Places

- The school precinct
- The Hospital precinct
- The Transport Interchange
- Kibble Park
- Gosford Stadium
- Leagues Club Park
- The Waterfront precinct

Public Plan, Our City Our Destiny, 2010, p 51



2018 Key Places

- Gosford High school
- Henry Kendal High school
- Gosford Public school
- Gosford Hospital precinct
- Gosford Private Hospital
- TAFE
- Kibble Park
- Gosford City Park
- Gosford Olympic Swimming pool
- Gosford Golf Club

2018 Key Landmarks

- President Hill Lookout
- Gosford Station
- Rumbalara Reserve
- Central Coast Stadium
- Gosford Stadium
- The Waterfront precinct

Landmarks and Places, Go Gosford Update, 2018

6.2.6 Main Connections

The 2010 OCOD master plan outlines the structure of the key connections that form the city centre. These connections remain relevant today.

Kibble Park is identified by the community as the core of the city. Its relationship to the surrounding business and retail areas is fundamental to the organisation of the centre. The main axis is formed by Mann running north-south, connection the railway to the waterfront. The main east-west axis connects the city core to the hills.

These axial relationships of the master plan are still valid, however some of the scale of the city and the length of Mann St has been challenged. It is unlikely that someone will walk the full 1.4 Km from the hospital to the waterfront along Mann St.

Therefore it is important to create a network of connected destinations, attractors and places to promote good connections and activation along as much of the city centre as possible, primarily along Mann St.

This implementation plan also takes some of the suggested east/west connections further in concept, creating accessible planted boulevards that link the two reserves.

Etna St – East-West Connection City Centre to Regional Rd (Raocourse Rd)

Faunoo St – East-West Connection City Centre to Regional Rd (To Be Implemented)

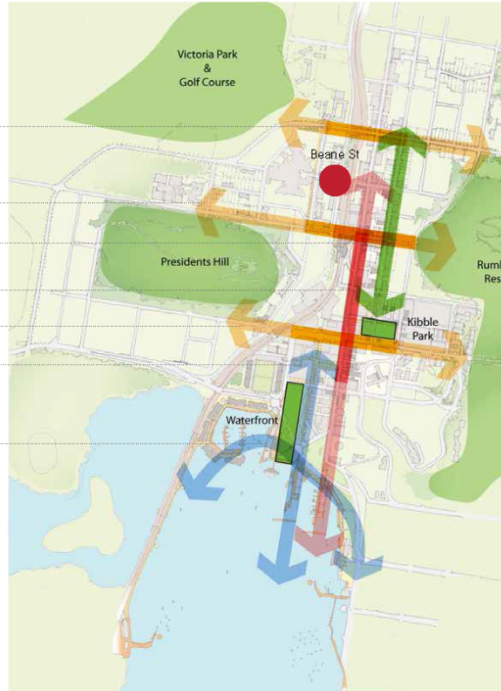
Watt St – North-South Connection Kibble Park to Regional Rd (To Be Implemented)

Mann St – Main North-South Connection

Donnison St – East-West Connection to Regional Rd (Existing)

Baker St – Main City Centre to Waterfront Connection

Waterfront Connection



Movement Axes,
Our City our Destiny, 2010,
p52

6.2.7 Views and Vistas

Urban Vistas Dynamics – Our City, Our Destiny 2010

The 2010 master plan discusses the importance of views connecting the city to the landscape assets.

“The views of the water around the foreshore are valued. View sharing ensures views are retained for all people to enjoy. Studies and an analysis of views and vistas around the city have identified key views:

The principles to protect key vistas are:

Development on the waterfront is concentrated towards the railway

Views out from the Leagues Club Park are preserved

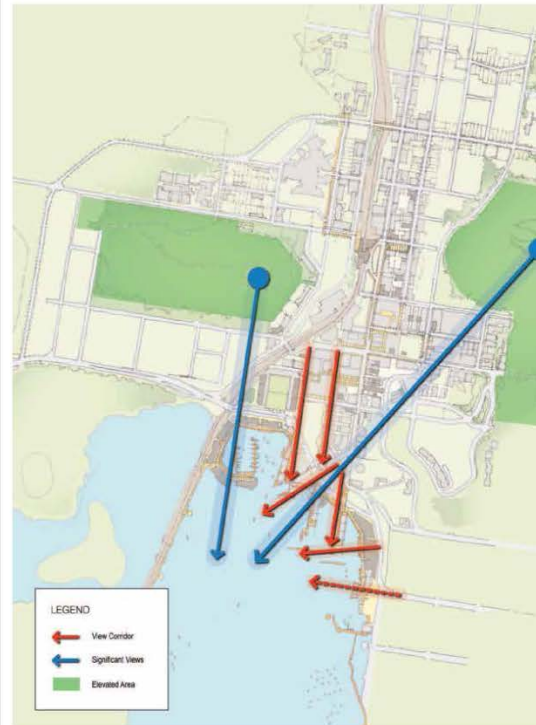
The view down Baker Street is protected

Views to and from Rumbalara and presidents hill are important elements

Possible views from the central coast highway from east Gosford should the pool be relocated or reconfigured.” p67

Most of the views and vistas are still relevant, however the focus of the 2010 masterplan is largely the waterfront. Clear site lines assist with way finding and they are a fundamental part of the identity of a city. Views and vistas also promote physical and visual connections between the three places. In the Go Gosford’s strategy the 3 places physically connect to Mann St.

As identified the distance between waterfront and city centre is large. As such, intermediate places are an additional areas to focus view and vistas.



Key Views

- From the end of Mann street to the water
- Along Baker street
- Down Dane drive past Bluetongue stadium and the leagues club
- Down through the memorial park into the harbour
- From the central coast highway to the water.

Views & Vistas, Our City our Destiny, 2010, p67



Additional Key Views

- Views to Kibble Park
- Views between Kibble park and the hills either side
- Views between the hospital and train station
- Views between the Station to Mann St

Views & Vistas, Go Gosford Updates, 2018

6.2.8 Roads Structure

The 2010 OCOD master plan states: “In revitalising the city centre this plan seeks to encourage those vehicles with a potential destination with Gosford city centre, through the introduction of clear gateway entrances to the city centre.

The challenge is to provide for destination traffic, whilst discouraging unwanted through traffic and managing congestion.” p75

The intent of the 2010 masterplan is to promote the use of the regional roads that loop around the city centre rather than the use of internal urban roads. This will diminish unwanted traffic congestion and will promote for a more pedestrian and bike friendly urban environment especially though Mann St and along the rail corridor.

The “Go Gosford” updates recognises the validity of this structure. It reiterates that the majority of regional traffic will enter the city centre from the south using the Central Coast Highway/Danes Drive. Parking, cycle and pedestrian strategies should promote this structure of roads



Major roads, Our City our Destiny, 2010, p75

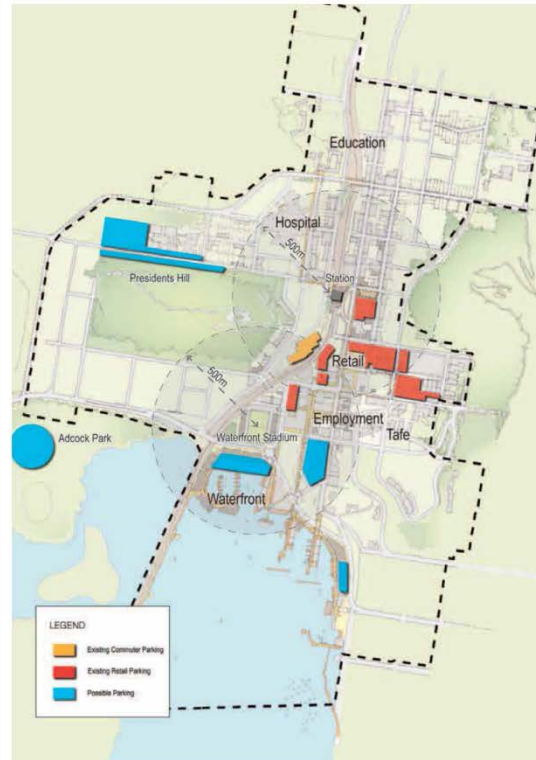


Major roads, Go Gosford Updates, 2018

6.2.9 Parking Strategy

The 2010 OCOD states “The provision of adequate parking within the City Centre has long been a contentious issue. There is an inherent conflict between the requirements for long term parking for commuters and workers and short term parking for retail and commercial activities.

In 2018 the strategy identifies a more comprehensive set of principles that unpacks logical positions for different modes of parking. It also provides implementation suggestions to address these parking issues which are still relevant today.



Parking, Our City our Destiny, 2010, p80



Suggested Long Term Parking Locations, Go Gosford Updates, 2018

6.2.10 Proposed Bicycle Network



Bicycle Network, Our City our Destiny, 2010, p78

Our City Our Destiny 2010 Active Network

"A cycle plan has been prepared by Gosford city council and a number of major initiatives have been proposed. The most ambitious of these is the cycle link to Point Clare beside the railway causeway.

The provision of sheltered cycle parking within the city centre, including the hospital and the waterfront, will be key elements in the cycle masterplan. As seen in European cities, cycle facilities can be effective if combined with commercial and tourism opportunities. Bike centres can offer sales, maintenance and storage." p78



Cycling Design Principles, Oculus Gosford Streetscape Guidelines, 2011, p23

Detailed Urban Design 2011

In 2011 the Oculus Streetscape Guidelines provided a more detailed summary of missing links in this network

In the city north side, the new hospital is suggested to be linked to Racecourse Rd, which could also link to The Narara Creek shore. Mann street connection could extend further towards the north side of the city loop. The waterfront could have an alternative route to Mann St via Baker St.

Works	Description	Time frame
1	Gosford to Point Clare waterfront cycleway	Medium long term
2	Connection between waterfront and Mann Street, along vaughan Avenue, Baker Street Boulevard and Dominion Street	Short-term
3	Cycleway along Mann Street between Dana Drive and Elms Street	Short-term
4	Water cycleway from East Gosford to Gosford Waterfront along Tok Street	Short-term
5	Racecourse Road to Hecken Street via Sinclair Street	Short-term
6	Create cycleway Racecourse Road between Faunce Street West and Dana Drive	Short-term
7	Cycleway across bridge connecting to the West Gosford industrial Estate	Medium-term
8	Cycleway between intersection of Racecourse Road / Faunce Street West, continuing north along Showground Road	Short-term

Table 4.4 Recommended works (Strategic Objective Priority 1)

Sources: Extracted from Gosford City Centre TMAP



Gosford Bike Strategy, Preliminary list of Proposed works, 2014, p35

Implementation Schedule 2014

The 2011 Oculus plan has been developed to: "A preliminary list of proposed works has been prepared for Strategic Objectives and is subject to further evaluation and availability of funds. Recommendations are grouped into proposed short term (to 2016), medium term (to 2021) and long term (to 2036) time frames for development and implementation. Costs are not provided and will be subject to further investigation."p35. These suggestions should be the focus of cycle funding and improvements. If possible, land adjacent to the rail corridor could provide an additional vital link, particularly with its connection to Waterfront under the Central Coast Highway.

6.2.11 Proposed Pedestrian Network



Pedestrian Network,
Our City our Destiny, 2010, p79

Overall Approach 2010

"Redevelopment of the urban streets will improve access to the city centre and the waterfront.

Many of the streets within the city are not fully formed and do not allow safe or easy pedestrian access. A program of street upgrades including lighting will be undertaken, with priority given to high pedestrian areas. There is a need for better and consistent signage including travel times to assist pedestrians to navigate the city."p79

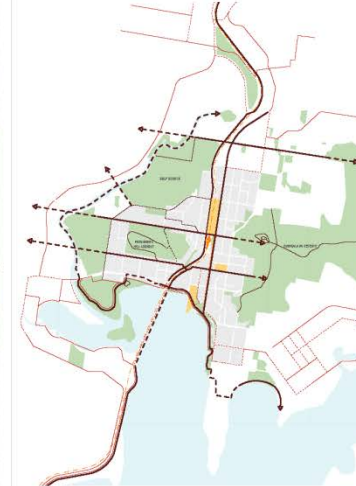
These points all remain relevant.



Pedestrian Access Design Principles,
Oculus Gosford Streetscape Guidelines, 2011, p22

Detailed Urban Design 2011

"The Oculus masterplan provide more detailed analysis of the public domain and the fine grain improvements required. The pedestrian access strategy seeks to increase and improve key pedestrian links and access between destinations." p22



Go Gosford Proposed Active Network.

Go Gosford 2018

The Go Gosford report emphasises the importance of pedestrian connections across the city. Key principles include encouraging walkability, providing pedestrian amenity in terms of shade and weather protection, create active facades along primary roads like Mann St, increase the number of east/west connections across the rail line, and to create a network of connected places.

6.2.12 Sustainability Strategy

The 2010 OCOD master plan identifies the importance of a sustainability strategy which remains current today. In addition to the ideas below, the 2018 strategy has a strengthened a green grid of planted streets that connect the city to the reserves.

"The redevelopment of Gosford creates an opportunity to reinvent the city as smart and green. Unlike most urban sites, the natural beauty of Gosford is seen as the starting point for a strategy that seamlessly integrates sustainable initiatives into the urban fabric with the aim of protecting and enhancing the city's natural legacy. Sustainability initiatives will build upon this legacy.

Green leadership

The challenges facing Gosford are not just structural and political. The spectre of climate change and sea level rise brings with it the need to imagine a new sustainable future and will profoundly change the way our cities evolve.

To be sustainable means that, as an organisation, Gosford City Council is aware of the impact of its actions on the LGA and that Council takes responsibility for these actions. Gosford City Council has demonstrated leadership in the area of environment sustainability for many years." p87

Council should continue to improve the environmental sustainability of Gosford. Some example projects include:

- Management of the Coastal Open Space System (COSS)
- The Gosford City water mining dual reticulation scheme
- Responding to climate change via the development of a draft climate change policy and several climate change mitigation and adaptation projects, sea level rise planning.

The Masterplan will continue to build upon this work by actively pursuing the following areas:

- Reduce water consumption
- Reduction in energy consumption
- Reduce automobile travel
- Cut greenhouse gas emissions
- Make land use decisions that create a more efficient and more walkable city
- Improve building energy performance
- Increase biodiversity
- Mitigate the impact of sea level rise

Sustainability, Our City our Destiny, 2010, p87



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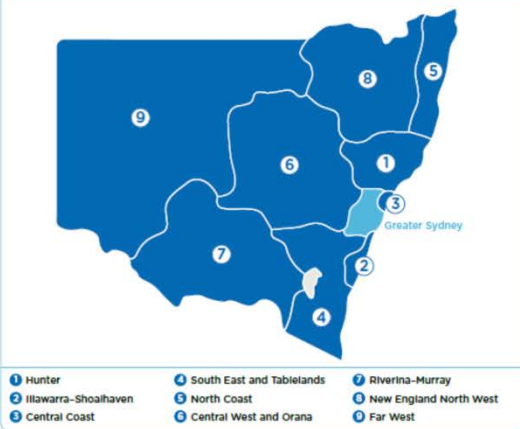
GOSFORD URBAN DESIGN AND
IMPLEMENTATION FRAMEWORK

6.3
TRANSPORT FUTURES
SUMMARY OF
CENTRAL COAST



**REGIONAL NSW
SERVICES AND
INFRASTRUCTURE
PLAN**

The Regions of NSW



Gosford urban design and implementation framework

Location	Population		
	2016	2036	2056
Greater Sydney	4.68M	6.42M	8.26M
Regional NSW Outer Metropolitan	1.48M	1.75M	1.98M
- Hunter	0.73M	0.86M	0.94M
- Illawarra-Shoalhaven	0.40M	0.47M	0.53M
- Central Coast	0.34M	0.45M	0.51M
Regional NSW Remainder	1.59M	1.75M	1.85M
Total (NSW)	7.75M	9.92M	12.09M

Tier	Identified locations	Future service levels
Global Gateway	Newcastle, Canberra, Gold Coast	Provide at a minimum international, interstate, inter-regional, intra-regional and in-town services in conjunction with ACT and Queensland Governments
Satellite cities	Gosford and Wollongong	Provide 30 minute city access within cities, with 60 minute access to Sydney
Regional city transport hubs	Tweed Heads, Lismore, Coffs Harbour, Port Macquarie, Armidale, Tamworth, Dubbo, Orange, Bathurst, Griffith, Wagga Wagga, Albury and Nowra	Provide at a minimum inter-regional, intra-regional and in-town services
Regional Centre transport hubs	Such as Broken Hill, Shellharbour and Queanbeyan	Provide at a minimum intra-regional services

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GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
Appendix 3



Figure 41: Potential Faster Rail improvements Central Coast and Newcastle rail line

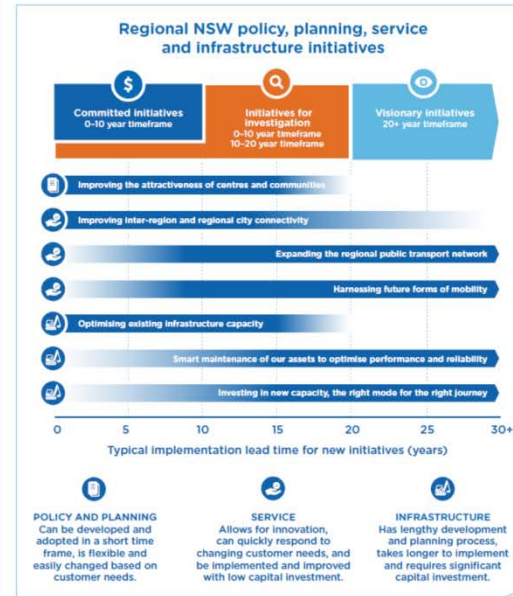


Figure 50: Initiatives to support the customer outcomes

Categorisation of initiatives

Committed Initiatives (0-10yrs)

Initiatives that either have committed funding, are committed/ contractually committed, are for immediate detailed planning, or are part of key maintenance, renewal or safety programs. Some initiatives are subject to final business case and funding.

Initiatives for investigation (0-10, 10-20yrs)

Initiatives intended to be investigated for potential commitment or implementation within the next 20 years. Those listed in 0-10 horizon will be prioritised for more detailed investigation to determine if they are required in the next 20 years.

Visionary Initiatives (20+ years)

Longer term initiatives that may be investigated within the next 10 years, but are unlikely to require implementation within 20 years.

Connecting Greater Sydney and the Central Coast

Gosford has been identified as a Satellite city of Greater Sydney reflecting its status as the key city of the Central Coast region. Strong population growth in the Central Coast over the next 40 years will see the region's relationship with Greater Sydney grow. This is reflected in the increased travel demand by both private vehicle and public transport expected between Greater Sydney and Central Coast each day.

Initiatives for investigation to better connect these regions include:

- › New Intercity Fleet
- › Delivering the Hub and Spoke model
- › Delivering the Regional Rail Fleet Project
- › Faster Rail improvements between Sydney and Newcastle
- › Freight Separation Program (Northern Sydney Freight Corridor)
- › Outer Sydney Orbital
- › Delivery of the NorthConnex link between the M1 and M2
- › M1 Smart Motorway

Outer Metro Initiatives - Connecting Greater Sydney to Regional NSW

KEY

- 1 Committed 0-10 years
- 2 Initiatives for investigation 0-10 years
- 3 Initiatives for investigation 10-20 years
- 4 Visionary Initiatives 20+ years

All initiatives for investigation are subject to business case development.

INITIATIVES

- 1 New Intercity Fleet
- 2 Upgrades to Blue Mountains Line
- 3 Upgrades to Main West Line
- 4 Sydney-Central Coast-Newcastle Faster Rail Improvement
- 5 M1 Motorway Improvements (Stawkesbury River - Mt White)
- 6 Northern Sydney Freight Corridor Stage 2
- 7 Sydney-Wollongong Faster Rail Improvement
- 8 Improved bus connections between South West Sydney and Illawarra
- 9 Bus priority measures on Appin Road
- 10 Picton Rd and Appin Rd Improvements
- 11 Belts Line of Road Improvements
- 12 Great Dividing Range long term solution study
- 13 Great Dividing Range long term solution corridor preservation
- 14 Sydney-Canberra Faster Rail Improvement
- 15 Mt Victoria to Orange road corridor improvements to achieve travel time savings and road safety outcomes
- 16 M1 - Newcastle SMART Motorway
- 17 Corridor Preservation for Higher Speed Connections
- 18 M1 Princes SMART Motorway
- 19 Electrification of intercity to Bomaderry/Nowra
- 20 Completion of Haidon to Dombarton railway line
- 21 M31 Hume SMART motorway
- 22 Electrification of intercity to Goulburn
- 23 Electrification of intercity to Bathurst
- 24 Higher Speed Connections (east coast)
- 25 Illawarra Escarpment long term solution
- 26 Outer Sydney Orbital from Great Western Highway to Central Coast
- 27 Outer Sydney Orbital from Hume Motorway to Illawarra
- 28 Belts Line of Road-Castlereagh Connection
- 29 Delivery of Great Dividing Range long term solution



Figure 55: Connecting regional NSW to Greater Sydney



Central Coast

The Central Coast has a population of around 340,000 people and will grow to approximately 500,000 people over the next 40 years. This strong population growth is a result of its close proximity to the state's two economic powerhouses, Greater Sydney and Hunter as well as the attractive lifestyle it offers. The region is home to the Darkinjung people.

In 2014–15 the region contributed \$8.6 billion to the NSW economy, primarily due to its specialisation in agribusiness and food, professional services, health and aged care and freight, logistics and distribution. Opportunities are available to better connect the region's residents and visitors and in doing so, support the growth of employment within the region.

We are working closely with the Department of Planning and Environment to achieve the vision for the Central Coast region of "a healthy natural environment, a flourishing economy and well-connected communities". Gosford is identified as the capital of the region and its renewal has and will continue to attract new residents, jobs, business and investment to the Central Coast. To support this, two growth corridors between Erina and Somersby as well as Tuggerah to Warnervale have been identified for increased investment in health, education, advanced manufacturing and service industries. We are working with the Department of Planning and Environment on its four goals for the Central Coast region:

- › A prosperous Central Coast with more jobs close to home
- › Protect the natural environment and manage the use of agricultural and resource lands
- › Well-connected communities and attractive lifestyles
- › A variety of housing choice to suit needs and lifestyles

Satellite city

In alignment with Department of Planning and Environment's identification of Gosford as the capital for the region, Gosford has been identified as a Satellite city of Greater Sydney. As a result, there is a need for improved efficiency of the transport network to, from and within Gosford and the Central Coast.

This includes:

- › Development of an integrated public transport network hierarchy, including:
 - A single operator taking multi-modal responsibility across the Central Coast.
 - Improved integration and interchange between modes/services to enable seamless customer experience.
 - Expanding 30 minute catchments for public transport.
 - Improved time of day coverage and service frequency, reduced journey times, and the deployment of on-demand, flexible services.
 - Facilitating car sharing services that are integrated with public transport.
- › Improved access to the northern and southern growth corridors with frequent public transport connections.
- › Improving the accessibility of the Central Coast to Greater Sydney and to the Global Gateway City of Greater Newcastle by public transport and private vehicle. We will work on improving travel times along the Sydney to Newcastle corridor. Seven deviations have been identified along the Central Coast and Newcastle rail line which when combined with the New Intercity Fleet could provide up to 40 minutes travel time savings between Broadmeadow and Central. Additionally, the Outer Sydney Orbital will provide a connection between the Western City and the Central Coast.
- › Supporting urban renewal and increased accessibility and liveability of key centres through improved transport connections.
- › Providing for the diverse travel needs of transport customers, including the large numbers of discretionary trips made throughout the day within the Central Coast as well as early morning and late evening commuters leaving the region.
- › Development of active transport networks.
- › Addressing pinch points in the road network and informing the program of road network optimisation improvements to support the maintenance of 30 minute catchments for car journeys.
- › Undertaking a car parking review to evaluate and prioritise car parking availability and use within centres and at key interchanges.
- › Introducing travel demand management policies and transport optimisation programs to re-balance demand against service and infrastructure provision.
- › To support this, a Central Coast transport network hierarchy has been developed.

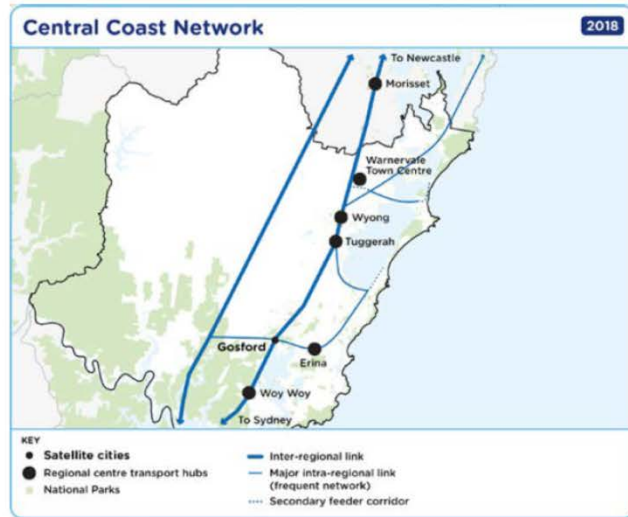


Figure 58: Network hierarchy for Central Coast

Hub & spoke

A key to the future success of the Central Coast is supporting efficient transport connections to, from and within the region. Working with Department of Planning and Environment, we have identified key hubs to support travel in the Central Coast region. These include:

- › Erina, Gosford, Tuggerah, Warnervale town centre, Woy Woy and Wyong.

Connections to these key hubs will be provided by the best mode for the transport task, this may mean improving the existing connections available or working on delivering new services and connections. The existing connections available include:

- › **Air:** No scheduled, passenger services currently operate in the Central Coast. The Central Coast Airport at Warnervale provides opportunities for private flights and general aviation.
- › **Road:** M1 Pacific Motorway, Pacific Highway and Central Coast Highway.
- › **Rail:** Intercity services between Sydney Central and Newcastle Interchange (Central Coast and Newcastle line). NSW TrainLink regional rail services between Sydney and Casino, Grafton, Brisbane, Armidale and Moree.
- › **Bus:** Local bus services operate across the region.
- › **Ferry:** Connections between Woy Woy, Empire Bay, Wagstaff, Ettalong and Palm Beach.
- › **Walking and cycling:** Various levels of infrastructure exist across the region to support walking and cycling.
- › **On demand:** Including community transport and taxis.

Future Transport Planning

A draft Central Coast Future Transport Plan will be developed as a supporting plan, discussing in more detail the transport future for the Central Coast region.

Place-based plans, plans considering the implementation of the movement and place framework will be developed for prioritised key hubs across the Central Coast region. These will be developed in conjunction with key stakeholders such as local government and Department of Planning and Environment.

GOSFORD URBAN DESIGN AND IMPLEMENTATION FRAMEWORK
Appendix 3

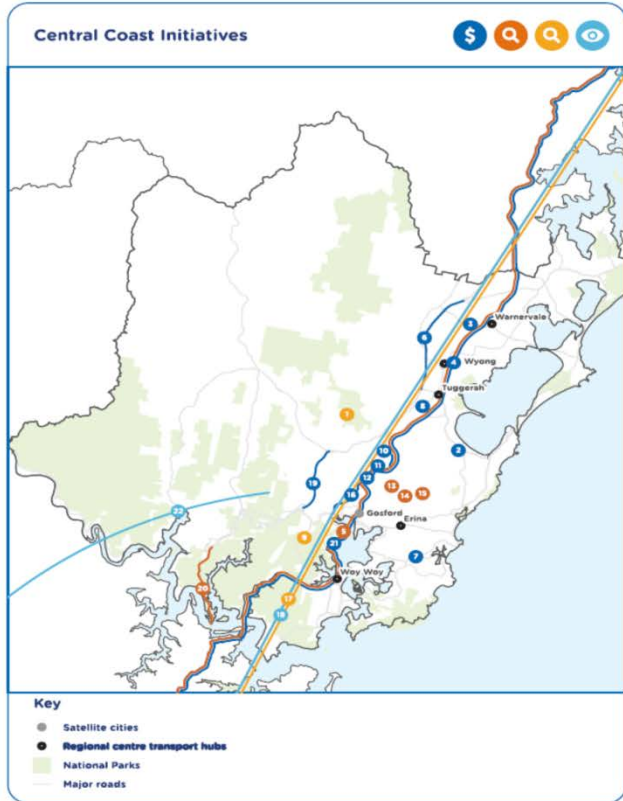


Figure 59: Central Coast infrastructure initiatives

Central Coast Region initiatives

KEY

- 1 Committed 0-10 years
- 2 Initiatives for investigation 0-10 years
- 3 Initiatives for investigation 10-20 years
- 4 Visionary initiatives 20+ years

All initiatives for investigation are subject to business case development.

INITIATIVES

- 2 Wyong Road, Mingara Drive to Tumbi Road Upgrade
- 3 Warnervale Link Road, Albert Warner Drive to Pacific Highway (Planning)
- 4 Pacific Highway, Wyong Town Centre (Planning)
- 6 Pacific Motorway Widening and Reconstruction, Wyong Road to Doyalson Link Road (State and Federal Funded)
- 7 Empire Bay Drive, The Scenic Road and Cochrane Street Intersection Upgrade
- 8 Kangy Angy Train Maintenance Facility for New Intercity Fleet
- 10 Pacific Highway, Ourimbah Street to Glen Road
- 11 Pacific Highway, Parsons Road to Ourimbah Street (Planning)
- 12 Pacific Highway and Manns Road, Narara Creek Road to Parsons Road (Planning)
- 16 Manns Road, Central Coast Highway to Narara Creek Road (Planning)
- 19 Pacific Motorway Widening, Karlong Interchange to Somersby Interchange (State and Federal Funded)
- 21 New Intercity Fleet
- 5 Sydney-Central Coast-Newcastle Faster Rail Improvement
- 13 Bus Headstart for Central Coast
- 14 Central Coast Rapid Bus package
- 15 Central Coast Place Plans
- 20 M1 Motorway Improvements (Hawkesbury River - Mt White)
- 1 Outer Metro Roads Program
- 9 M1 - Newcastle SMART Motorway
- 17 Corridor Preservation for Higher Speed Connections
- 18 Higher Speed Connections (east coast)
- 22 Outer Sydney Orbital from Great Western Highway to Central Coast

In addition to these region specific initiatives, statewide initiatives have been identified and are included in Chapter 4 of the Regional NSW Services and Infrastructure Plan.

Progressing our plans

Establishing customer outcomes and a vision for our transport system, and identifying and prioritising initiatives to deliver on these is just the first step in planning for the future. To ensure we are delivering the best outcomes for customers and the community, we will undertake detailed planning and feasibility studies for specific initiatives.

Our immediate priority will be to develop more detailed regional and place-based plans. As these plans are defined, we will then progress to detailed feasibility assessments of specific initiatives as part of the business case process. The business case process will ensure initiatives that are progressed for funding and delivery deliver value for money for the people of NSW.



Planning for Places - A tiered approach to planning transport for the regions

Guided by the Future Transport 2056 Strategy, Regional NSW Services and Infrastructure Plan and supporting plans, we will develop more detailed regional, corridor and place plans. These plans will focus on more regional and local transport needs and how these fit into the wider regional NSW transport vision. The plans will also enable us to assess in more detail what initiatives are most appropriate for addressing the transport needs of these corridors and places to align with land use planning. Importantly, they will also enable us to address community feedback on specific regional and local initiatives we should investigate.

Regional Plans

To be developed for each region to align with the Department of Planning and Environment's 20 year Regional Plans, the Regional Economic Development Strategies (REDS) and the Regional Development Framework to ensure transport planning supports growing regional economies and improves amenity for regional communities. These will be prepared in consultation with local government and Joint Organisations, other state government agencies and Australian Government bodies.

Place Plans

Plans considering the implementation of the movement and place framework will be developed for prioritised cities and centres within each region. These will be developed in conjunction with key stakeholders such as local government and Department of Planning and Environment.

Corridor plans

Rather than a specific road or train line, a corridor in the context of strategic planning is a broad, geographic linear area. Planning for the needs of a corridor ensures that different transport investments work together as part of an integrated solution. Mode-specific specialists will work alongside local experts to develop these plans.

**GOVERNMENT
ARCHITECT
NEW SOUTH WALES**



FOR DISCUSSION
09 APRIL 2018

GO!SFORD



URBAN DESIGN IMPLEMENTATION FRAMEWORK PLACE REPORT 1 – THE CIVIC HEART

CHROFI

TYRRELLSTUDIO

GOVERNMENT
ARCHITECT
NEW SOUTH WALES



Design objectives for NSW

Seven objectives define the key considerations in the design of the built environment.



Better fit
contextual, local
and of its place



Better performance
sustainable, adaptable
and durable



Better for community
inclusive, connected
and diverse



Better for people
safe, comfortable
and liveable



Better working
functional, efficient
and fit for purpose



Better value
creating and
adding value



Better look and feel
engaging, inviting
and attractive

GN
ASW

Government Architect NSW acknowledges the traditional custodians of the land and pays respect to Elders past, present and future. We honour Australian Aboriginal and Torres Strait Islander peoples' unique cultural and spiritual relationships to place and their rich contribution to our society. To that end, all our work seeks to uphold the idea that if we care for country, it will care for us.

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GOSFORD PLACE REPORT 1

01. INTRODUCTION

Government Architect NSW (GANSW), supporting the Central Coast Coordinator General and in close collaboration with a consultancy team led by CHROFI, has initiated work to support the ongoing revitalisation of the regional city of Gosford.

The Urban Design and Implementation Framework (UDIF) will help to shape the continued development and renewal of the Gosford City Centre and support implementation of the Central Coast Regional Plan 2036 vision for Gosford. Capitalising on government and private sector investment in Gosford will strengthen its role as the regional capital and benefit current and future residents of the entire Central Coast.

This Place based report explores and describes in greater detail the issues, constraints, uses and opportunities associated with Kibble Park. It considers the park in relationship to Mann St, surrounding built form and landscape in order to fulfill its potential as the Civic Heart of Gosford.

1.1 Preamble

Great places and cities don't happen by chance. They are designed and continue to be designed to manage their transformation over time. The need for the UDIF has arisen from the growing consensus that a vision needs a framework for implementation. The implementation of strategies and recommendations will enable the urban design outcomes to be delivered.

When undertaking urban design analysis and developing strategies and recommendations for collaborative implementation engaging with people fosters confidence, development and investment. Done carefully, with focus on natural attributes, amenity, public domain and quality, it has the potential to transform quality of life for people, stimulate the economy, and enhance the environment.

The current context

There have been several ad hoc responses to support revitalisation in the city centre including sporadic development of new buildings, largely in the residential market concentrated around the fringe of the city centre itself.

There remains a run-down appearance in the buildings and vacant shops, with poor quality public domain and way finding, poor pedestrian amenity and poor connectivity with surrounding areas.

Previous plans have:

- not resolved issues relating to investment in public domain and the city's key public places
- led to individual developments but not significantly improved the public domain or overall attractiveness of the city
- not inspired confidence and investment in the Gosford City Centre leaving a run-down appearance, and building stock as well as empty shops
- delivered limited development contributions for public domain infrastructure.

However, there has been progress with the establishment of new government projects and jobs (Hospital, Australian Tax Office) within the city centre and commitments for a regional library and arts centre. This has built both momentum for change and boosted private sector investment confidence.

The UDIF report

The UDIF has identified a different approach to the revitalisation of Gosford City Centre, one that takes a place-based approach. It prioritises and focuses interventions spatially and in terms of implementation and deliverability, with potential catalytic impact. This approach focuses on key public places where there is existing momentum in terms of public and private sector investment, prioritising public focused

initiatives in the overall city centre renewal.

The UDIF will provide a series of recommendations regarding the design of places and spaces to support delivery of the vision for Gosford City Centre. It will also provide recommendations relating to governance, infrastructure and other matters that will be critical to the ongoing urban renewal and quality outcomes for Gosford.

By providing an integrated design approach, we can collaboratively create the framework for the delivery of great places, spaces and buildings that attract and retain residents, jobs, tourists, and further investment.

The UDIF is a design-led methodology, balancing multiple and sometimes competing agendas. It is for all agencies, levels of government, industry and the community to plug in to for action. It builds on the vision of the regional plan, which has consensus. We need to synthesise and ensure action starts now to deliver tangible outcomes for the community.

The UDIF proposes an urban logic to assist with decision making and outlining recommendations. It provides an implementation plan to guide rejuvenation, growth and foster good design and a high quality public realm.

The three places which the UDIF will plan in the most detail are:

- City Centre middle: Kibble park and the civic heart
- City Centre north: the health and education precinct
- City Centre south: the waterfront

This report provides the detailed urban design framework for Kibble Park: the civic heart.

Purpose of this Report

This Place Based Report provides guidance to help shape and inform the development of the next stages of planning and investment decisions in Gosford.

This report will inform:

- Early urban design advice to assist with shaping and the assessment of time sensitive projects ensuring that they contribute positively to the vision for Gosford e.g. Regional Library and RPAC.
- Development of new and emerging planning controls for the city centre and the three places
- Contribute to the schedule of projects to be included in the preparation of state and local infrastructure contribution plans
- Public and private sector investment decisions in the three key places
- Pre-application advice to developers
- Consideration of planning proposals and applications including proposed design review processes
- Negotiation of voluntary planning agreements (VPA) and the potential inclusion of regional and community facilities in proposals
- Potential partnerships opportunities for mixed use projects
- Inform and support bids for funding e.g. upgrades in public realm, active transport, etc.,

1.2 Why a Place Based Approach

Based on what we've learnt from past reports and master plans, as well as insights gained through the UDIF work, a targeted place based approach is recommended.

- Places have been identified as important because they are in strategic locations where there is a confluence of existing amenity, public space, public investment plans and private development opportunity. They build upon what's already happening.
- They are in locations that can drive footfall between them.
- It concentrates and 'gathers' investment around it. In turn, there is an opportunity for one investment to feed off another.
- It also concentrates activity on-ground rather than in buildings or privately-owned places.
- Low risk of implementation. Centres around a public space that can be delivered quickly by government to lead the renewal. The first move doesn't need private sector partnership, multi-agency agreements, etc.
- Relative to buildings, public space can create a greater visible transformation per dollar invested. In addition, approval path and construction times are likely to be quicker.
- The public space is to be a high quality regional attractor to strengthen Gosford's role in the region. This will also boost private investment viability around it by improving visual amenity and bringing people to the focus area.
- This provides government opportunity to lead conversation with developers who benefit from this public investment on VPAs – whether this is contributions, accommodation of key public functions such as cultural facilities, new TAFE or university space, or encouraging land-uses that strengthen Gosford as a regional centre.

By clustering investment around a public place, there's an opportunity to create focus, synergy and momentum – increasing visitation, strengthening viability for investment and provide government greater power to lead the market.

The large scale of the study area encompassing Gosford city centre necessitates a targeted approach to development and investment.

It is important to consider, the scale of Gosford city centre relative to Sydney, which has a comparatively tight urban structure, short distances between transport modes and vibrant activated streetscapes are already established.

Dealing with this scale of city centre requires key opportunities to be identified for a focused approach to activity and investment.

Whilst activation of the waterfront is desirable, it will not necessarily help to activate the heart of the city. Gosford has the potential to have a series of activated and interesting places along a connected network. Gosford has many important ingredients including the northern anchor of a hospital and education region, a central station, Kibble Park in the heart of the city, and a beautiful waterfront with public open space.

Connected, with a network of pedestrian and cycle friendly routes to both the bushland and the water, the city can focus on making these ingredients work to their full potential. As a network they are stronger than any single project or place.

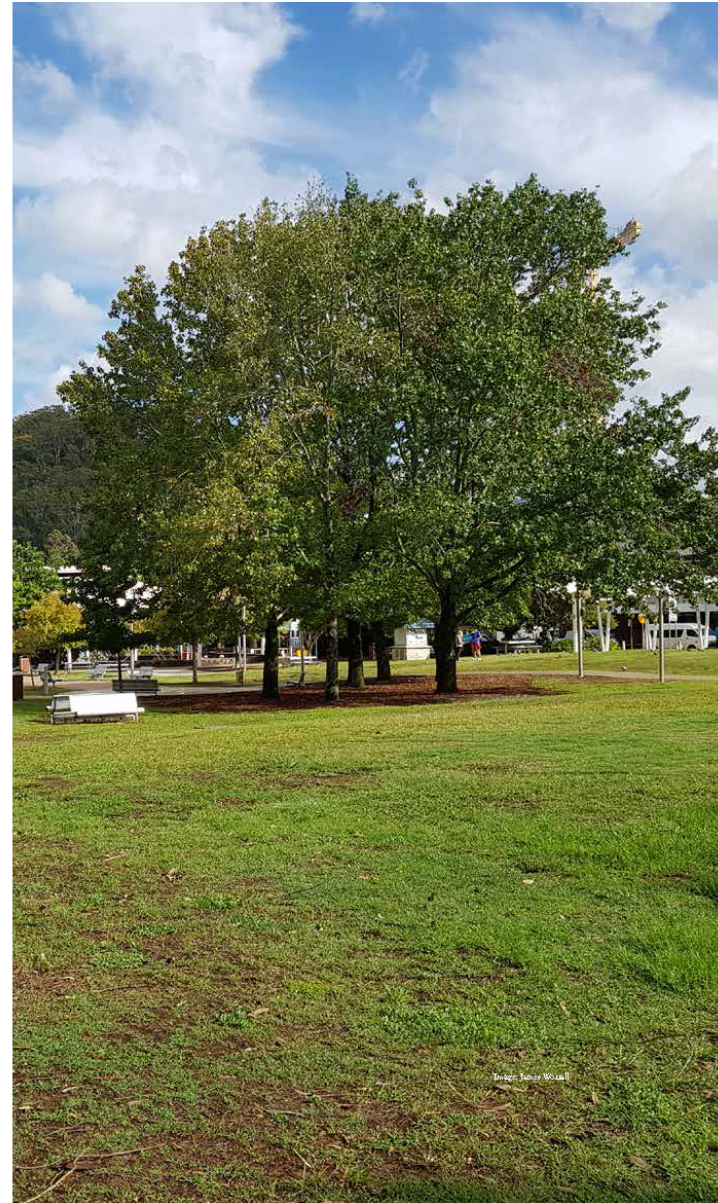


Image: Robert Wilson

The potential of Gosford

The city of Gosford has enormous potential; the structure of the city has all the ingredients to be a successful regional capital. The reality on the ground is not as favourable. Public investment is needed to change the reality of Gosford.

Role of Gosford

Gosford's role extends beyond the immediate towards a local, regional and state context.

Its role as a regional capital brings with it aspirations, including:

- Civic and Cultural role as the heart of city centre with the provision of services for a regional city providing employment and residential
- Prominent commercial core functions – professional services, civic services (such as banks, police, court house, government agencies, Council, or regional library facilities)
- Consolidation of government and professional services
- Accessible city that is the focus of efficient movement networks.



GOSFORD PLACE REPORT 1
The Civic Heart: Introduction

1.3 Green Infrastructure Strategy

The development of a Green Infrastructure Strategy, like Parramatta Ways or Greater Sydney Green Grid, can support the establishment of a network of well-connected places that create an attractive capital city with vibrant street life. An attractive city is a platform for investment with short term returns and long term growth.

Green Infrastructure is the network of green spaces, natural systems and semi-natural systems including parks, rivers, bushland and private gardens that are strategically planned, designed and managed to support a good quality of life in an urban environment.

Green Infrastructure is as crucial to the city as transport, cultural and communications infrastructure. It delivers a range of benefits including:





- Healthy living
- Mitigating flooding
- Improving air and water quality
- Cooling the urban environment
- Encouraging walking and cycling and enhancing biodiversity and ecological resilience
- Absorbing and transforming waste.

A green infrastructure strategy supports improved active movement

Creating a more connected city through active movement paths and networks is a priority for Gosford. Prioritising active movement serves to connect the city, integrate the community and promote individuals' health and well-being.

The UDIF proposes to develop an active movement network that is integrated with green infrastructure connecting the city with its landscape. Connect Gosford City with its neighbouring areas for recreation and everyday access.

LEGEND

-  Cycle Paths
-  Pedestrian Paths – extension of grid
-  Potential Cycle Paths
-  Potential Pedestrian Paths



1.4 Creating three places

When we examine the scale of Gosford, we understand that the city is large and spread out relative to regional cities of similar population catchment. By creating key places within walking distance to each other, they become stepping stones to navigate across the city.

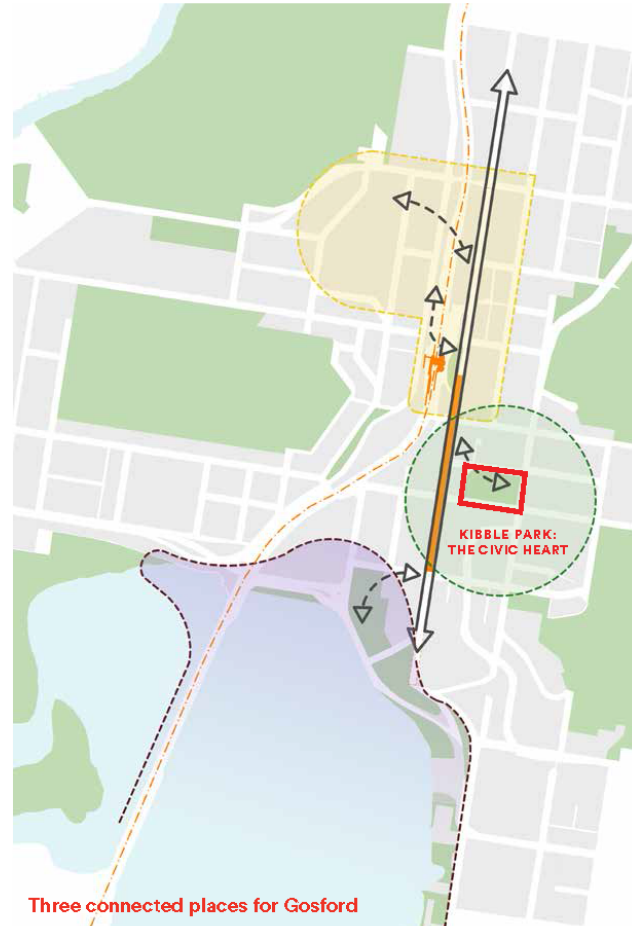
By starting at the centre of the city with Kibble Park, we can ensure all three places identified can be a connected network which encourage footfall between each other.

Enabling a network of connected places allows us to cluster public domain improvements and investment in key areas of the city.

Kibble Park: The Civic Heart

Geographically in the centre of the city, this study area in focus around the open space of Kibble Park. This open space is adjacent to the existing Gosford Library and its proposed re-location, multiple government and administrative functions, and significant private developments like the Imperial Centre and Bonnython Tower.

It is a central meeting place that brings the character of the bushland reserves into the heart of the city. The focus on this area is to create a centrally located, attractive and connected open space and town square.



1.5 The Civic Heart design principles

Improve way finding, pedestrian access, social infrastructure and align with a city centre plan for the Green Grid (place design with identity, social infrastructure design, usable open space, how people use places and how they move between them, improve orientation and navigation).

Building design needs to promote street frontage activity, connectivity, mobility, and visual impact.

Ensure there is an attractive public domain and acknowledge how buildings interact/address the public space. Consider the relationship of buildings to each other and the public domain.

For each of the three study areas, identify what are the significant areas that we need to maintain an important aspect to/from.

Ensure solar access and limit/prevent overshadowing in accordance with specific place based report.

Identify and protect view corridors.

GOSFORD PLACE REPORT 1
The Civic Heart: Introduction

The importance of Mann Street

Mann Street is a fundamental part of the structure of Gosford. It is the primary spine that connects different areas of the city along its length. It provides a clear way finding and organisational line to the city.

As a high street, it offers a historically important civic quality to the city. It has a fine grain streetscape that is human scale, offering diversity of shopfronts and reflecting the city's history as a regional centre.

In similar ways to Sydney's George Street, it changes character along its length. To the north, there are larger sites generally of service functions, which transition to the core retail strip from the railway station to Georgiana Terrace, then to the older heritage streetscape to the south, terminating at Memorial Park which overlooks Brisbane Waters.

The place based approach reinforces the essential structure of the city, strengthening diversity of city experience and use along this spine.



Image source: Central Coast Library Service



GOSFORD PLACE REPORT 1

02. KIBBLE PARK: THE CIVIC HEART

Kibble Park is a unique place in Gosford, identified consistently by the community as the heart of the city. Geographically it is in the centre of Gosford located adjacent to Mann St connected through William St Plaza. It is close to the station and connected in the east to the loop road of Henry Parry Drive.

As the only significant open space in the centre, the area has the potential to operate at a scale that can make it a regional destination. It is a central meeting place that brings the character of the bushland reserves into the heart of the city.

The park has a significant amount of public and private development commencing around it bringing, residents and new commercial space. Many important regional functions are focusing on this civic heart, including the new regional library, local courts and government services.

Significant private developments are also underway in this place, including Bonython Tower under construction, the Imperial Centre development, and other investments along Mann Street. It is also adjacent to the main regional shopping facility of the Imperial Centre.

By concentrating investment in this place, there is the possibility to create both a moment of green relief and a place of civic and commercial intensity in the heart of the city.

Starting with Kibble Park creates a great place in the heart of the community, rather than at its edges. The park, under local government ownership, is a logical and realistic place to start. It doesn't have the complex challenges of the waterfront, and is ready to start whilst the hospital is under construction.

Changes in the heart of the community have the potential to 'ripple' their impact in all directions. Creating a strong public open space with key principles can protect this public asset from future neighbouring developments.

A central civic space working to its full potential can draw on and support the people, businesses, jobs and services that will surround it.

2.1 Why Kibble Park?

Kibble Park is a unique place in Gosford, identified consistently by the community as the heart of the city. Geographically it is in the centre of Gosford located adjacent to Mann St connected through William St Plaza. It is close to the station and connected in the east to the loop road of Henry Parry Drive.

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GOSFORD PLACE REPORT 1
Kibble Park: The Civic Heart

2.2 Kibble Park – Strategic Position in the City Network

Kibble Park draws the sense of the wider bushland landscapes into the heart of the city.

It is the key moment where the north-south urbanity of Mann Street meets the bushland character of the east west connections.

Kibble Park becomes the centre of a wider green circulation network linking the northern city centre, station and waterfront.



2.3 Kibble Park – Strategic Position in relation to regional attractions

Kibble Park and the surrounding area is in a great location to build on existing and future regional attractions with additional commercial, tourist, cultural and educational facilities.

These include:

- Regional Library
- TAFE
- Large Shopping Centre
- Mann St Retail
- Railway Station
- Bonython Tower
- Kibbleplex



GOSFORD PLACE REPORT 1
Kibble Park: The Civic Heart

2.4 About Kibble Park

History

The history of Gosford highlights it has:

- civic importance
- a place for the community
- places of heritage significance
- had recent years of decline
- maintenance issues
- capacity to grow

What do locals say

The UDIF process involved consultation with the local Gosford community. Kibble Park is a community hub; valued by all who live and work in the centre.

"The Imperial Shopping Centre which is recognised as a big attractor with its multiple entrances, as well as the Youth Arts Warehouse and the Employment and Training Australia Secondary College."

"Improvements are needed in the public domain of the city centre to make Gosford a place where people want to live."

"Kibble Park is poorly maintained and is limited in the events it can host. It needs services, amenities, and to function as the civic heart of the city."

"There is lack of provision of office and commercial spaces."

"Overall safety needs to be improved in Gosford, including Kibble Park."

"I walk through the park on my way to the station but rarely stop"

"The library is a great building. It would be great if we could find a new use for it."

"It is great to have somewhere to stop with the kids but I wish the playground was better and the fountain worked."

"We are starting to see a few fun new restaurants and cafes open up but not many stay open in the evening or on weekend!"



Image - Capital Coast Library Range

“We need more Sunday trade, somewhere to have dinner after work, more options to take the family”

Values

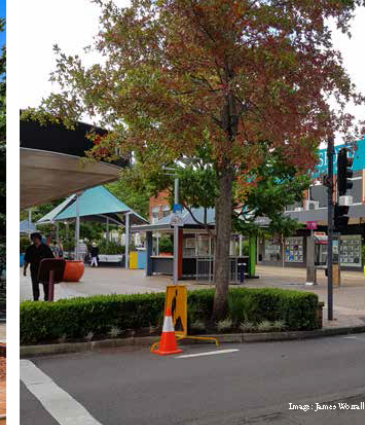
- Important public green space in the centre of the city
- Views to the hills
- Busy at lunchtime – people working and studying nearby use it
- Markets and events in Kibble Park bring the community together
- Connection to the Imperial Centre and car parking
- Close to the station
- History associated with Byblos restaurant and the library

Constraints

- Not attractive enough to draw people to Gosford
- The park is cluttered which reduces its flexibility
- Perceived safety problems particularly at night
- It is hidden from the street
- Poor accessibility and connections across the park
- Lack of greenery
- Cars cut off space from the Imperial Centre and take up space
- Conflict with loading and servicing
- Issue with maintenance
- Fountain never works
- Taxis are ugly
- Location of stage doesn't work for events

Opportunities

- Declutter to create a sense of space
- Ensure design allows for flexible use including performances, markets, outdoor movies and festival
- Visual and physical connections to Presidents Hill and Rumbalara Reserve
- Improve connection to Mann Street
- Maintain sunlight to public spaces
- Reuse library for public use – gallery, temporary exhibitions, meeting space, town hall
- Active uses that attract diversity range of people surrounding the park
- Additional footpath seating for restaurants and cafes
- Night-time and weekend activity
- Something to protect the space from traffic on Henry Parry Drive
- Improve pedestrian and cycle access to the park
- Temporary road closures of surrounding streets for special events
- More outdoor dining opportunities
- Food trucks in the short term to kick start the market
- University/TAFE presence
- Better playground
- Water feature that kids can play in to cool down.



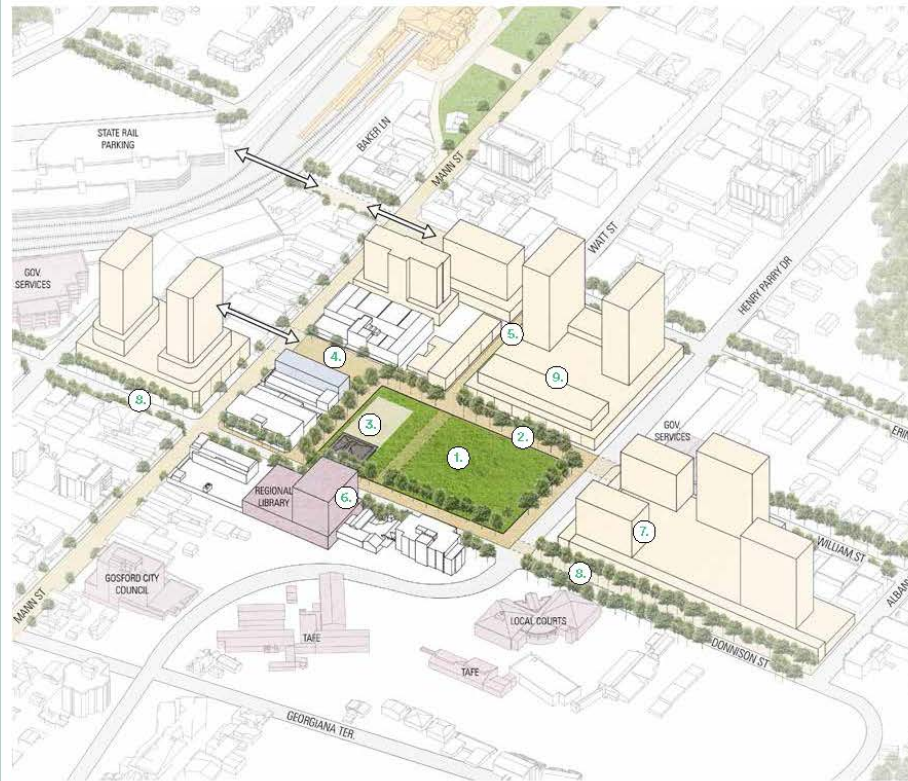
Gosford Place Report to Kibble Park

15

GOSFORD PLAGE REPORT 1
Kibble Park: The Civic Heart

2.5 Civic Heart design principles

- 1.** Create a green room – respite for the growing city
- 2.** Relocate activity to the edge and activate where people move
- 3.** Re-purpose library as a public building – possible Town Hall, meeting place, flexible exhibition space. Investigate upgrade or new cafe integrated with park
- 4.** Create a civic plaza and connect to Mann Street with active edges. Increase visual connection between Mann St and the park. Streetscape upgrade to Mann Street
- 5.** Establish through connection street at Watt Street
- 6.** Align connection to new regional library. Explore secondary connections between regional library, TAFE, Conservatory of Music and Council
- 7.** Investigate opportunity to retain public parking in part of future development behind an active edge to Henry Parry Dr.
- 8.** Connect park to reserves with tree planting along Donnison Street
- 9.** Locate development height to minimise overshadowing of Kibble Park.

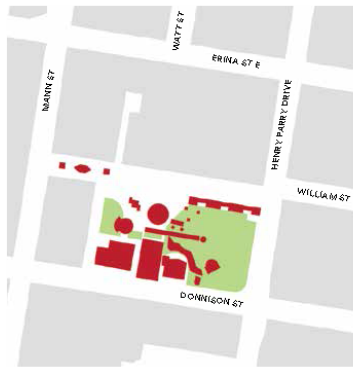




GOSFORD PLAGE REPORT 1
Kibble Park: The Civic Heart

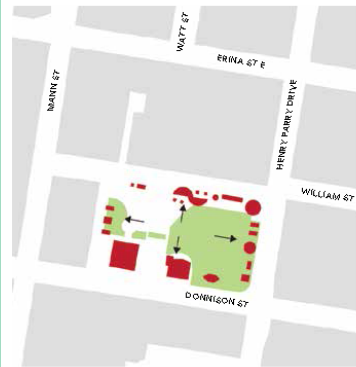
2.6 Detailed Urban Design principles

1. Activity to the edges



CURRENT

The park is cluttered. Scattered park activities, vehicle access and car parking take up much of the space.



PROPOSED

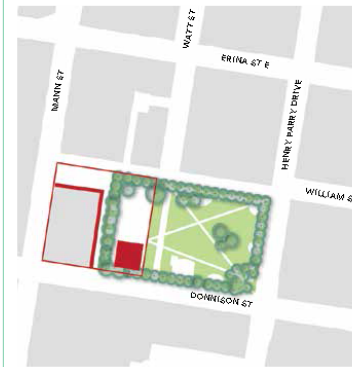
Move the park uses to the edges to create active streets and diverse edges for people to gather. The open space can breathe.

2. An iconic green room



Planting a frame of Eucalyptus trees creates an alternative green room with shaded edges. The park becomes iconic, grounded by a beautiful borrowed view to President's Hill.

3. Great Civic Spaces



Mann St Plaza and the re-purposed library become flexible event spaces for day and night activation.

4.
Ensure sunlight access to the park



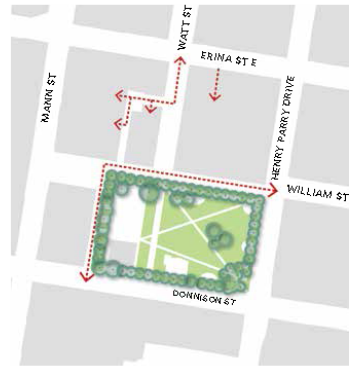
Locate height in surrounding developments to minimise overshadowing of park and public domain.
Ensure solar access for 4 hours to 60% of the park between 9am and 3pm at the winter solstice. Refer to built form principles.

5.
Active street frontages



Protect and promote the fine grain retail of Mann Street.
Ensure active and defined street frontages to all park edges.
Re-purpose library as a public building. Investigate upgrade or new cafe integrated with park.
Curate diverse offerings for different demographic segments and for activation during day and night, weekday and weekends.

6.
Reduce dominance of cars



Vehicle access is retained around the park in the short and medium term, but with reduced road widths and shared surfaces with clear pedestrian priority.
Future servicing or parking entries not to be located on Mortimer Lane or William Street adjacent to the park, or where active street frontages are proposed.

7.
Connect with nature

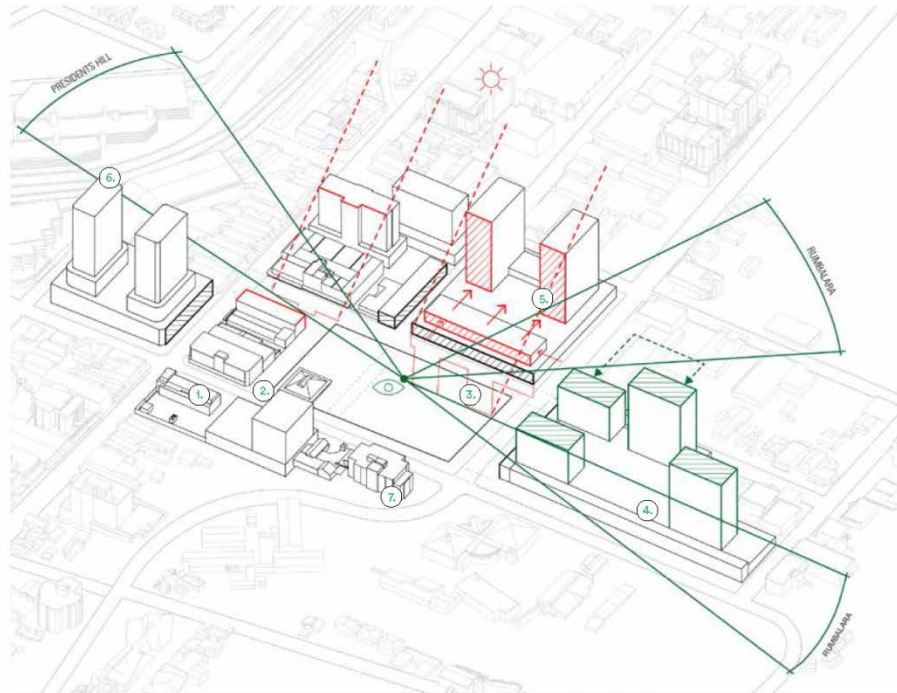


Protect view corridors to President's Hill and Rumbalara Ridgeline. Refer to built form principles.

GOSFORD PLACE REPORT 1
Kibble Park The Civic Heart

2.7 Built form principles

- 1.** 2-3 storey podium height with a fine grain and active frontages to key places and streets
- 2.** Ensure ground floor facade of the plaza buildings achieves sun throughout the day
- 3.** Provide setback to upper levels above podium
- 4.** Slender East/West tower forms, aligned to protect view corridors to Rumbalara ridgeline
- 5.** Setback slender north/south orientated tower forms to allow shadows to move quickly across Kibble Park.
- 6.** Protect view corridors to Presidents Hill from Kibble Park
- 7.** Ensure solar access for 4 hours to 50% of the park between 9am and 3pm at the winter solstice.







2.8 Precedents and character



Emerald Log by McGregor Coxall
Image: Simon Whitehead

A green room that brings the quality of the bush reserves to the city

Improve pedestrian and cycle access to the park



The Good-Line by Aspect Studio, CH2O/FI
Image: Simon Whitehead

Ability to draw community together for functions and events

Trees on the edges

Flexible and adaptable spaces in the middle

A place for all ages to play

Better playground

Water feature that children can play in



The Good-Line by Aspect Studio, CH2O/FI
Image: Simon Whitehead

A moment of quiet in the city for multiple users

Temporary road closure for special events

Food trucks/more outdoor dining



Market Junction by CH2O/FI
Image: Matt Abbott

Connecting a main street to open spaces and pathways along the river

Encouraging the activation of the rear of buildings

Landmarks and icon

GOSFORD PLACE REPORT 1
Kibble Park: The Civic Heart

2.9 What we heard

This table captures a summary of feedback from workshop 5 that relates to Kibble park and the surrounding area - now, opportunities for the future and the role its can play in making Gosford a regional capital and great place.

Each grey square represents the relative number of mentions each issue had during the workshop.

GREEN LINKS

Link reserves to the city	■
Trees	■■■■■
More parks and gardens, better quality existing parks, botanical gardens	■■■■■
Fountains that work	■
Retail the natural ecology	■
Better connections to Rambulara Reserve and Presidents Hill	■■■

PARKING

Parking at night (noting council car park at Baker St shuts at 8pm)	■
More short term parking (currently used by commuters)	■■■
Consider charging for parking (debated)	■
More commuter parking, parking for workers	■

TRANSPORT OPTIONS AND ACCESSIBILITY

Better public transport options, other than the car, more direct bus routes	■■■■■
Better pedestrian access and accessible footpaths for all (will help things feel closer)	■■■■■
Reduce through traffic on Mann St, including buses. Make Mann St for people	■■■■■
Better signage and way finding	■■■
Infrastructure to feed in and be coordinated	■■■

SAFETY

Improved safety – particular at night, and for women	■■■
Passive surveillance	■
Lighting	■

DESIGN

A distinct design style, not cookie cutter	■■■
Improved public domain and streetscape, public art, better footpaths, bins,	■■■■■
Make use of the Gosford Sandstone	■
Integration of arts and design, cultural trail	■■■
Quality first, not low budget, which will entice further investment	■■■
Wider footpaths	■
Link people to specific quality of place	■

SOCIAL

Diversity of users (youth, aging population)	■■■
Improved social agenda, mental health, bring everyone along	■
More people will attract more people	■
Indigenous history of President's Hill and other locations respectfully displayed	■

PROGRAMME AND ATTRACTORS

Active usages at all hours, 7 days	■■■■■
Night life (after 5pm), including small bars	■■■■■
Drawcard for tourists	■
A cultural hub linked to the library	■
Library	■
Start-up/ Entrepreneurial Hubs	■
Youth place to occupy, youth entertainment	■■■
Space for sports and outdoor recreation	■■■
Improve Kibble Park – only half functional, including the fountain, shade	■
Coordinated activities and events	■
Family playground with BBQs, shelter	■■■

IDENTITY

A big clear idea, not band-aid solutions	■■■
Action, lots of plans that have not come to fruition	■
Coordinated events to create a cultural destination	■
Improved Reputation and status	■■■
Sense of a civic or cultural heart	■
A sense that things are changing	■
Express the region's history – e.g. as a citrus producers	■
Be modern	■
Atmosphere	■

ECONOMY

More variety of retail and jobs	■■■
More high quality shops, culture and arts	■■■



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